

INDEX

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STATE OF INDIANA
STATE HIGHWAY COMMISSION

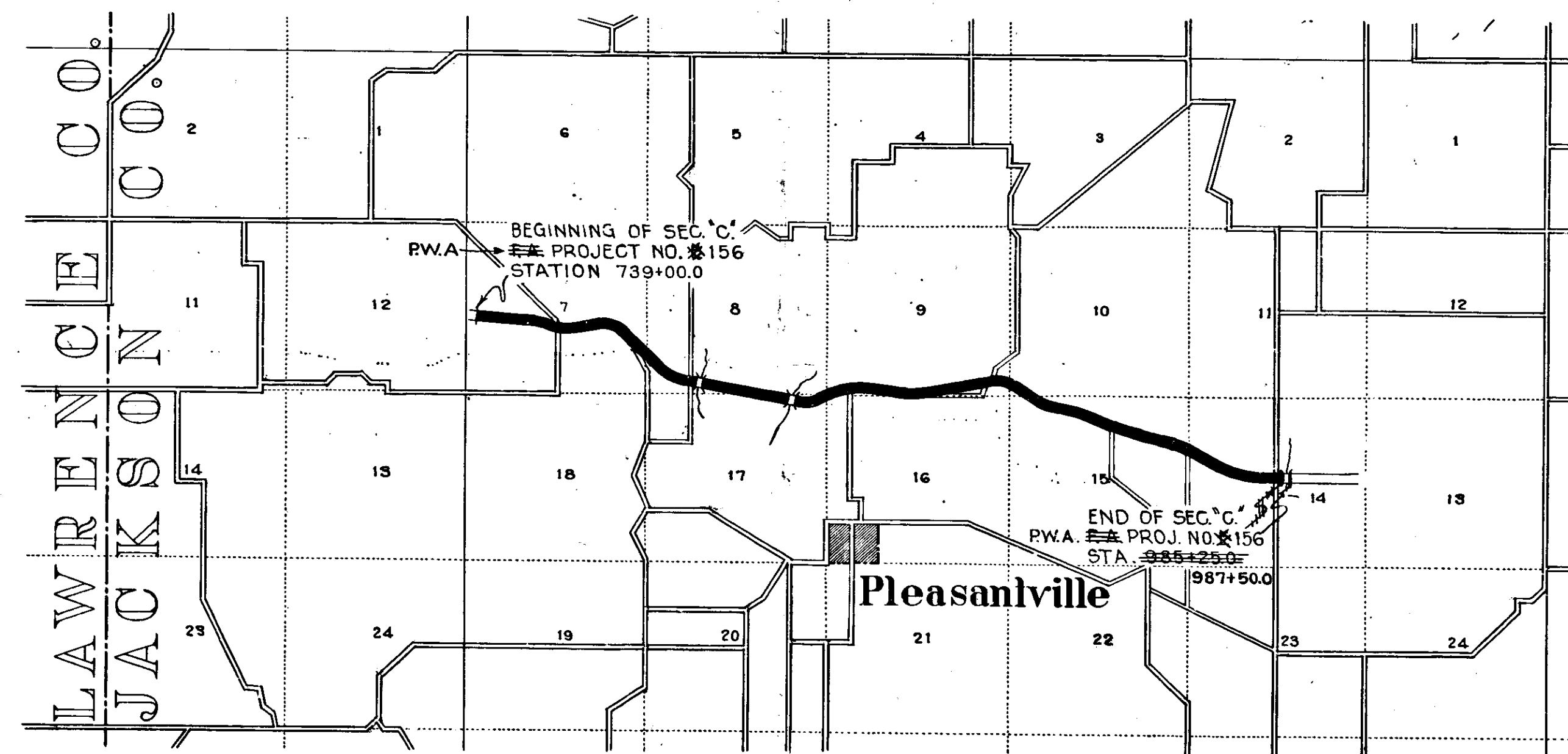
PLAN AND PROFILE OF PROPOSED

STATE HIGHWAY

P.W.A. PROJECT NO. 156 SEC. C PAVING
BEDFORD ~ SEYMOUR ROAD

BEGINNING AT A POINT IN THE SOUTH HALF OF SEC. 7-T5N-R3E APPROX. 320' EAST OF THE WEST LINE OF SAID SEC. 7 AND RUNNING SOUTHEAST TO A POINT AT THE APPROX. CENTER OF SEC. 14-T5N-R3E IN JACKSON COUNTY. 476' EAST OF THE

GROSS LENGTH: 4.664 MI. - 4.706
NET LENGTH: 4.581 MI. - 4.672
SCALES:-
PLAN (LONG.- 1"=100' PROFILE (HORIZ.- 1"=100'
(TRANS.- 1"=100' VERT.- 1"=10'
MAX. GRADE= 6.80%



Scale 1"=3000'

FEDERAL ROAD DISTRICT NO.	STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	1933	1	20

SECTION: C PAVING.

APPROVED & ADOPTED 10-23-33
BY STATE HIGHWAY COMMISSION OF INDIANA

James D. Adams
CHAIRMAN - STATE HIGHWAY COMMISSION OF INDIANA

APPROVED 10-23-33

W. K. Keefe
CHIEF ENGINEER - STATE HIGHWAY COMMISSION OF INDIANA

APPROVED AND ADOPTED 3-28-33.
By STATE HIGHWAY COMMISSION OF INDIANA.

James D. Adams
CHAIRMAN - STATE HIGHWAY COMMISSION

APPROVED 1-25-33

Augusta Barnhart
DIRECTOR - STATE HIGHWAY COMMISSION

APPROVED 12-10-32

J. J. Brown
DIRECTOR - STATE HIGHWAY COMMISSION

APPROVED 12-10-32

J. J. Brown
DIRECTOR - STATE HIGHWAY COMMISSION

RECOMMENDED FOR APPROVAL

DISTRICT ENGINEER - BUREAU OF PUBLIC ROADS

RECOMMENDED FOR APPROVAL

DISTRICT ENGINEER - BUREAU OF PUBLIC ROADS

APPROVED

DIRECTOR - BUREAU OF PUBLIC ROADS

INDIANA STATE HIGHWAY STANDARD SPECIFICATION DATED 1923 AND INDIANA STATE HIGHWAY STANDARD SPECIFICATION B DATED APRIL 1931 ON FILE WITH U. S. BUREAU OF PUBLIC ROADS. TO BE USED WITH THESE PLANS, ALSO SUPPLEMENTAL SPECIFICATION DRAFT OF NOV. 1, 1928 AND NOV. 1, 1930 AND SUPPLEMENTAL SPECIFICATION NO. 1 DRAFT OF MARCH 1, 1932 & STD. SPECIAL PROVISIONS DRAFT OF SEPT. 22, 1933 REV. OCT. 6, 1933

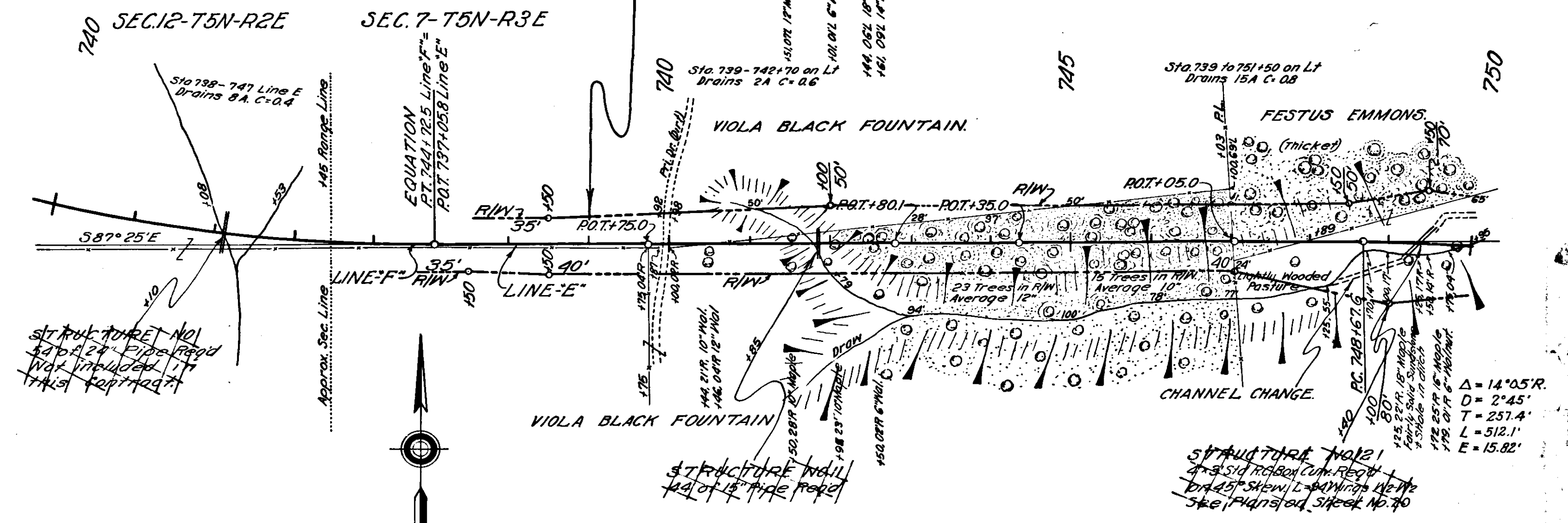
C.C.V. 12-19-32

12-16-32

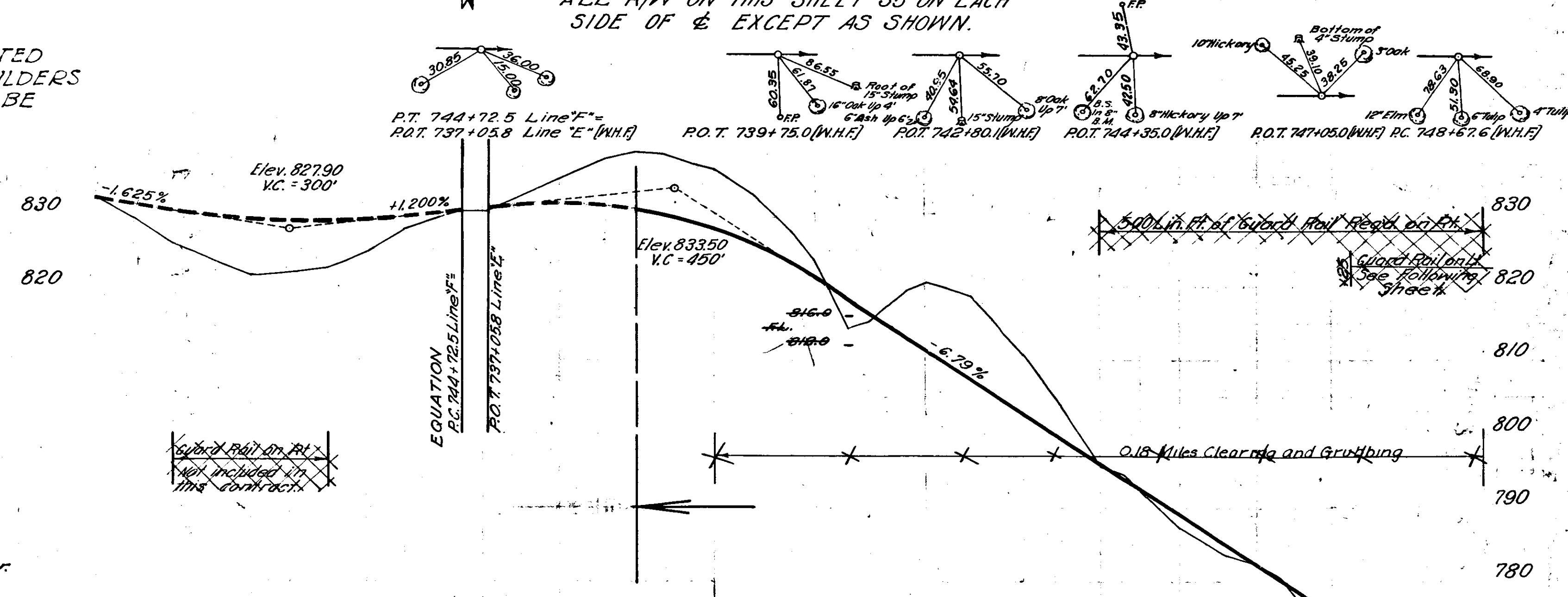
P.W.A.
BEGINNING OF P.A. PROJ. #156-SEC. "C" PAVING
STATION 739+00.0

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND	156	1933	4	22

SEC. "C" PAVING 20



ALL R/W ON THIS SHEET 35' ON EACH SIDE OF & EXCEPT AS SHOWN.



BM #104 Elev. 804.49 Boatspike in 8" W. Oak 51' R. of Sta. 744+00
 BM #105 " 768.17 " 14" Sugar Maple 70' R. " 746+65

740	1	741	2	742	3	743	4	744	5	745	6	746	7	747	8	748	9	749	10	750
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C.C.K. 11-19-32
 J.T.A. 12-20-32

STANDARD SECTION C-8-B (EXCEPT 7' SHOULDERS SHALL BE CONSTRUCTED) ADOPTED OCT 1929 REV. OCT. 21, 1933 AND STANDARD SECTION C-8-B (EXCEPT 7' SHOULDERS SHALL BE CONSTRUCTED) ADOPTED AUG 1931 REV. APRIL 29, 1932 SHALL BE USED ON THIS PROJECT

~~STANDARD SECTION A-B ADOPTED APRIL 1928 & REV. APRIL 29, 1932 AND STANDARD SECTION A-B-B ADOPTED AUGUST 1931 & REV. APRIL 29, 1932 TO BE USED ON THIS PROJECT.~~

Notes:-
 Grade line represents top of finished pavement.
 Grade to be raised 3" for Rock Asphalt and Asphaltic Macadam Pavements.
 Standards under date as listed in index on title sheet to be used on this project.
 All curves of 5° and over to be widened according to standards of 1932.
 The crown will be taken out on all curves of over 2°.

State Standard Specifications adopted 1923 and supplements of Nov. 1, 1928 and Nov. 1, 1930 shall apply in the construction of Concrete, and Bituminous Concrete, or Brick pavements on Concrete bases.
 State Standard Specifications "B" adopted April 1931, except guarantee clauses, articles 12-A-4, 12-J-4, 12-A-4, 12-L-4 and 12-M-4 and supplementary specifications No. 1 adopted March 1, 1932, shall apply in the construction of Asphaltic Macadam, and Rock Asphalt on Macadam bases.
 Features not covered by Standard Specifications "B" and supplementary specifications No. 1 adopted March 1, 1932, shall be constructed in accordance with State Standard Specifications, adopted 1923, and supplements thereto. Std. Special Provisions, Draft of Sept. 22, 1933, Rev. Oct. 6, 1933, also to apply on this Project.

Note:-
 At all road approaches where stone or gravel is required, the contractor is to salvage that portion of metal in place as directed by the project engineer.

NOTE: Where Shoulders and Ditches are constructed on one side of & only, they shall be measured and paid for as one-half the length constructed.

4-31
 7-31
 F. Estler
 G. E. Goodwin
 Z. B. Jones

1948
 1949

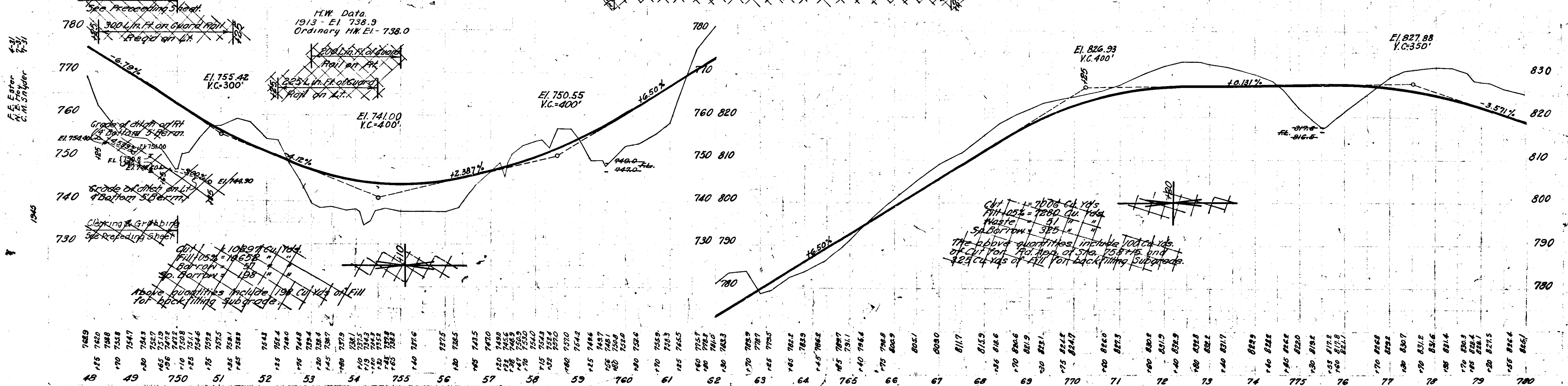
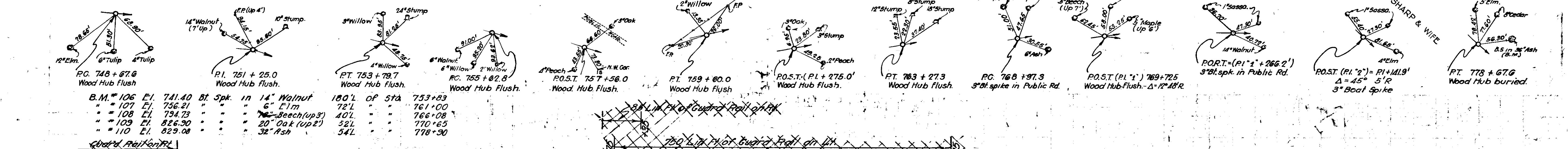
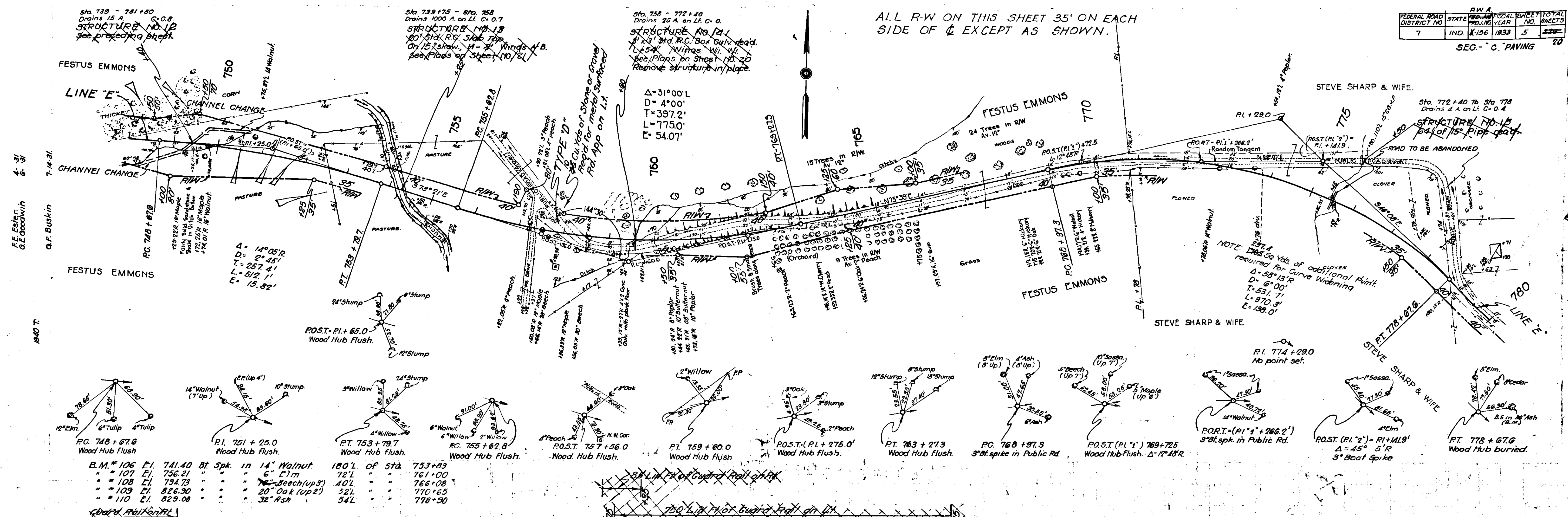
4-31
 7-31
 F. Estler
 Z. B. Jones

1948
 1949

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	LOCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	1196	1933	5	22

SEG. - C. PAVING 70

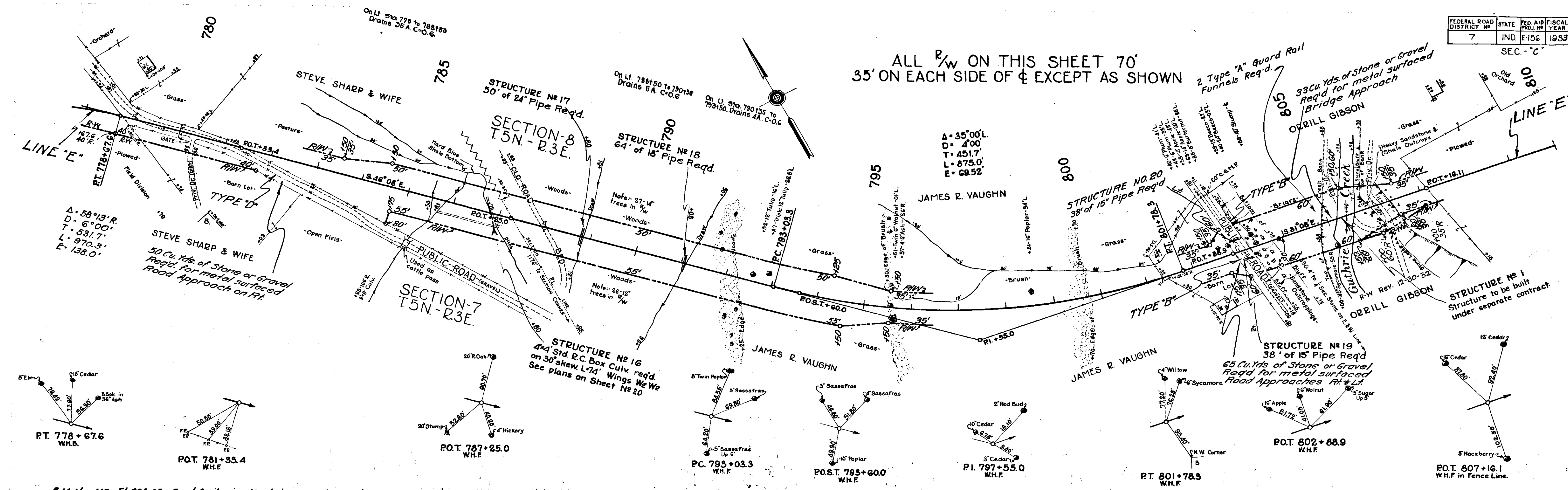
ALL R-W ON THIS SHEET 35' ON EACH SIDE OF & EXCEPT AS SHOWN.



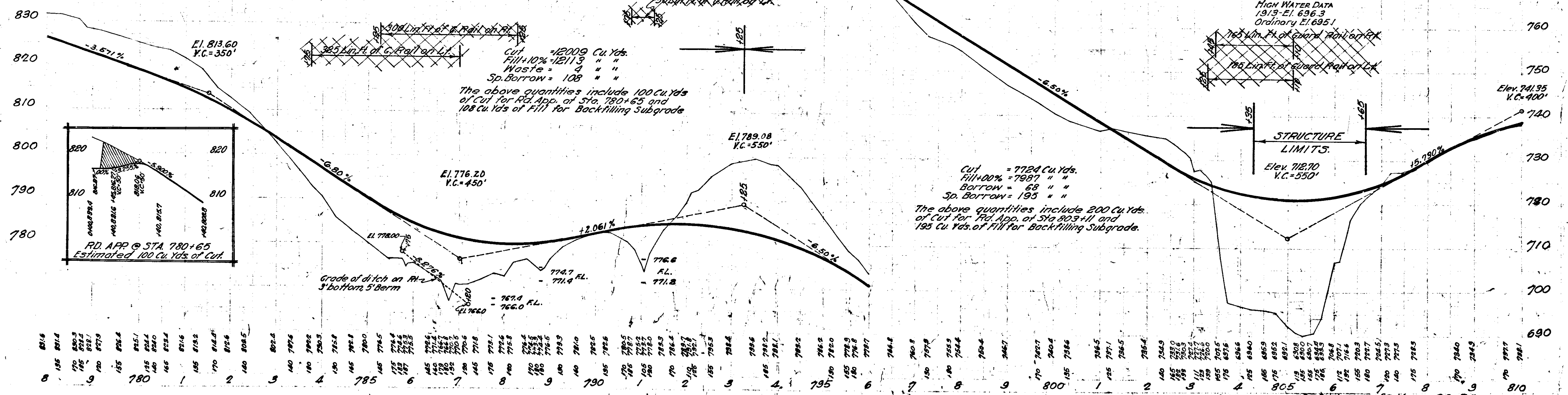
1946
 F.E. Ester
 A.E. McFarland
 C.H. Snyder
 12-19-32
 11-12-32

FEDERAL ROAD DISTRICT NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	E-156	1933	6	222

ALL R/W ON THIS SHEET 70'
35' ON EACH SIDE OF ϕ EXCEPT AS SHOWN



B.M. No. 110	El. 829.08	Boat Spike in 32" Ash	54' L. of Sta. 778+90
111	780.72	6" Sycamore	785+50
112	792.18	16" Poplar	791+82
113	765.53	8" Maple	796+97
114	741.56	6" Syc. (up 3')	799+82
115	704.61	Roof 36" Beach	803+63
116	728.38	12" Cedar	806+79

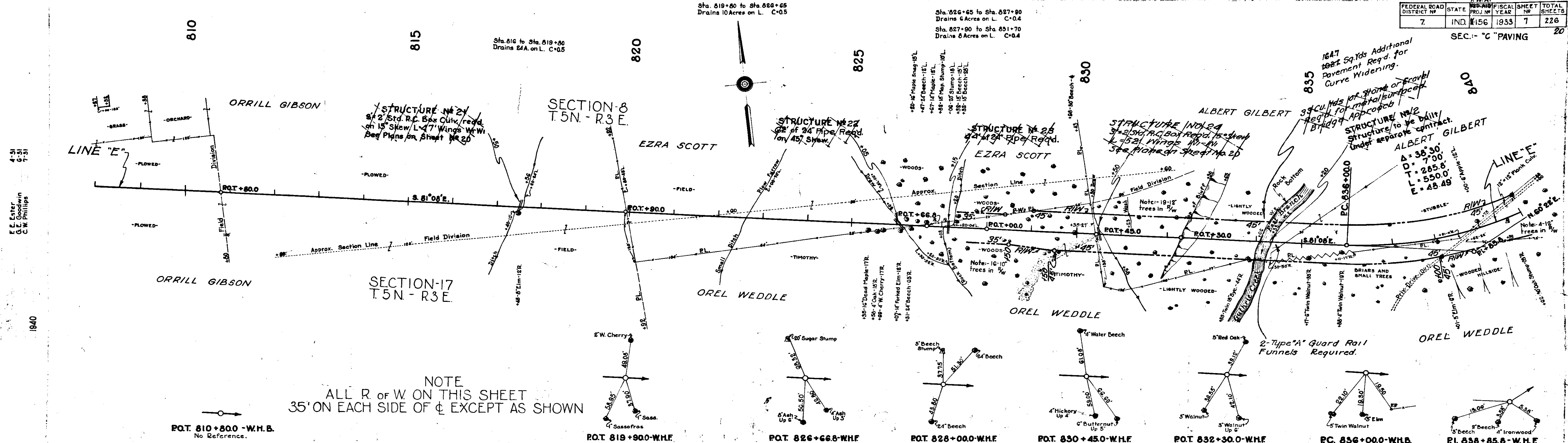


8	816	814	812	810	808	806	804	802	800	798	796	794	792	790	788	786	784	782	780	778	776	774	772	770	768	766	764	762	760	758	756	754	752	750	748	746	744	742	740	738	736	734	732	730	728	726	724	722	720	718	716	714	712	710	708	706	704	702	700	698	696	694	692	690
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C.R. 12-19-32
J.H. 12-20-32

FEDERAL ROAD DISTRICT NO.	STATE	P.A. YEAR	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	1933	1933	7	226

SEC. - "C" PAVING 20

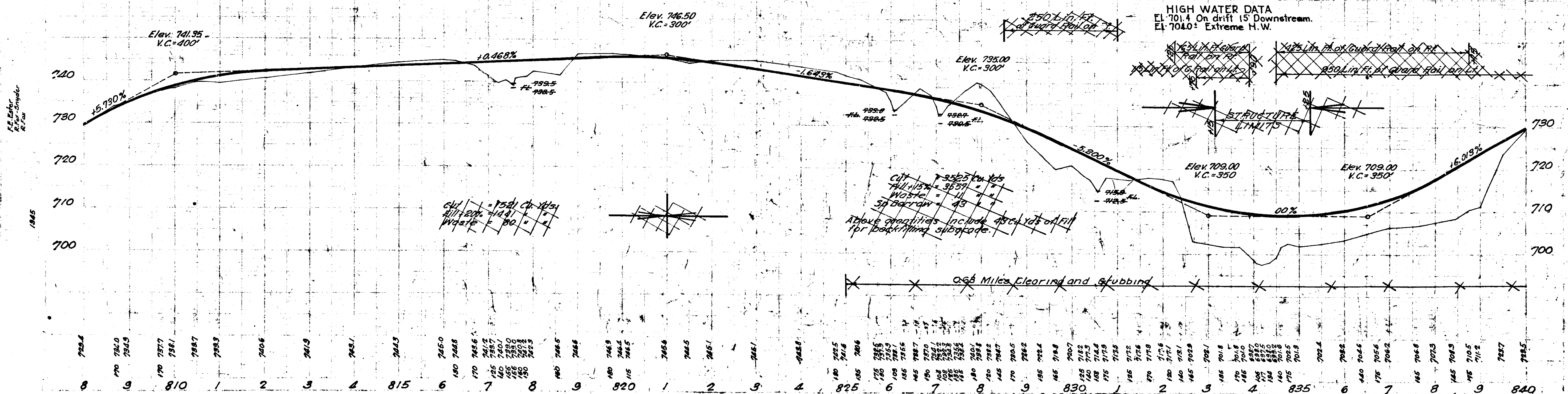


NOTE
ALL R OF W ON THIS SHEET
35' ON EACH SIDE OF C EXCEPT AS SHOWN

POT. 810+80.0 - W.H.B. No Reference.	POT. 819+90.0 - W.H.F.	POT. 826+66.8 - W.H.F.	POT. 828+00.0 - W.H.F.	POT. 830+45.0 - W.H.F.	POT. 832+30.0 - W.H.F.	P.C. 836+00.0 - W.H.B.	P.I. 838+85.8 - W.H.F.
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B.M. No. 117 - El. 738.66 - Boaf Spike in 12" Tw. Sycamore	85' E. of Sta. 817+24
118 - 741.99 - " " 20" Beech	42' " " 826+139
119 - 710.92 - " " Roof 10" Sycamore	48' " " 831+113
120 - 703.65 - " " 12" Walnut	108' " " 835+166
121 - 725.23 - " " Roof 12" Mulberry	48' " " 838+132

HIGH WATER DATA
El. 701.4 On drift 15' Downstream.
El. 704.0: Extreme H.W.

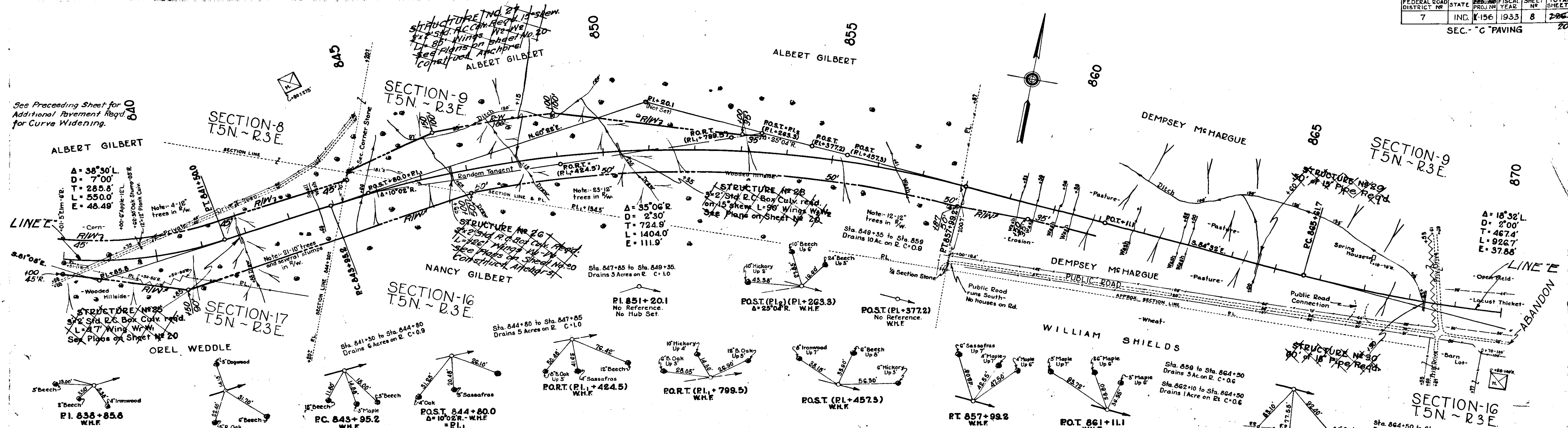


E. E. Enter
G. C. Goodwin
C. E. Phillips

1840

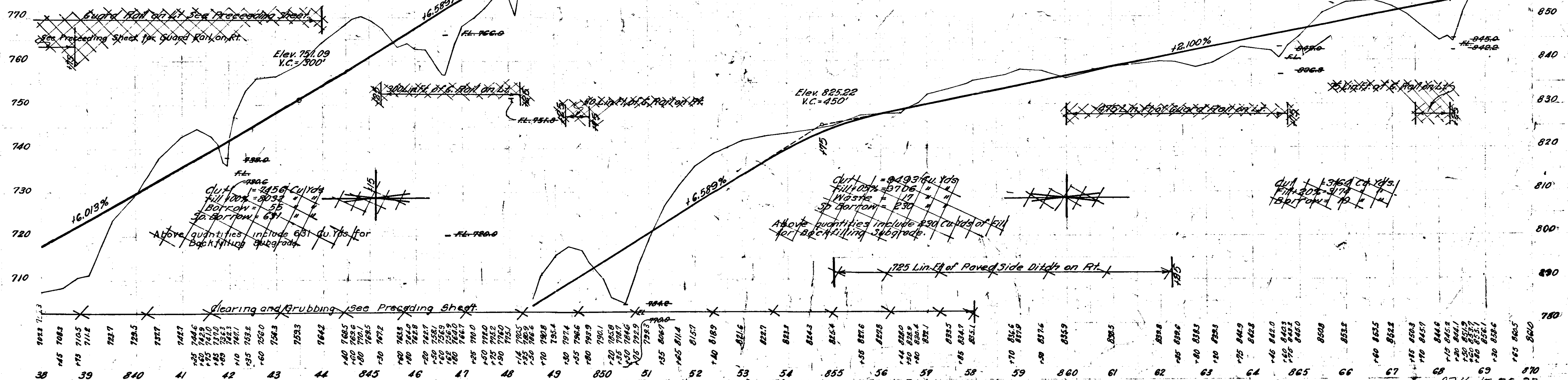
E. E. Enter
G. C. Goodwin
C. E. Phillips

1845



ALL R. OF W. ON THIS SHEET
35' ON EACH SIDE OF C EXCEPT AS SHOWN.

B.M. #21 El. 725.23	5 ft. Spt. in Root of 12" Mulberry	45' Rt. of Sta. 838+52
B.M. #22 " 757.68	" " " " " " " "	50' Rt. of Sta. 842+10
B.M. #23 " 787.21	" " " " " " " "	50' Rt. of Sta. 848+73
B.M. #24 " 810.44	" " " " " " " "	63' Rt. of Sta. 851+62
B.M. #25 " 830.57	" " " " " " " "	138' Rt. of Sta. 853+20
B.M. #26 " 848.00	" " " " " " " "	74' Rt. of Sta. 857+90
B.M. #27 " 853.41	" " " " " " " "	110' Rt. of Sta. 870+50

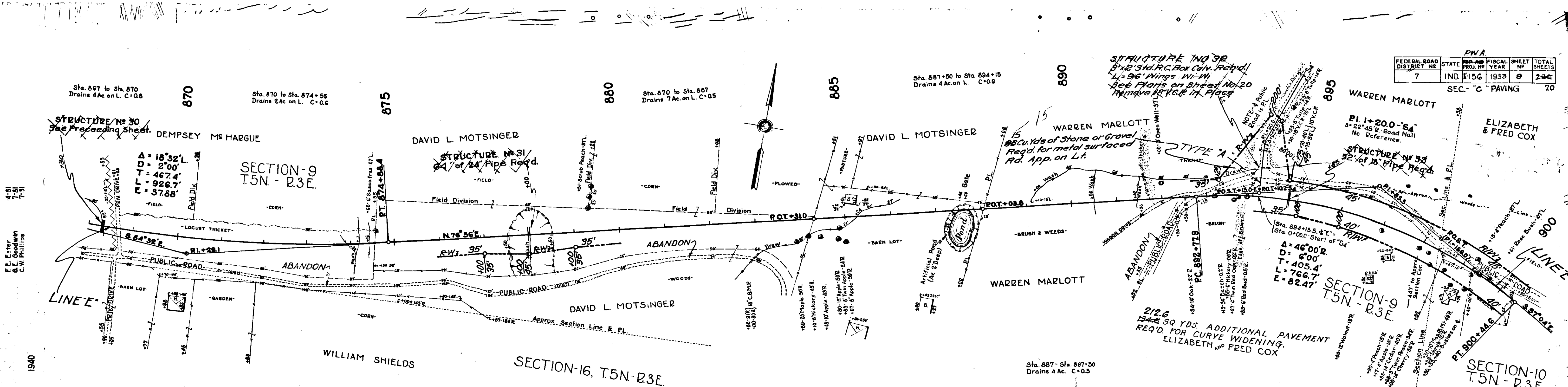


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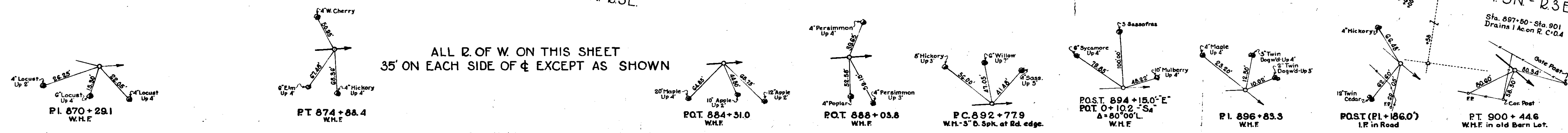
C.P. 12-17-32
8774-12-20-32

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	156	1933	9	24

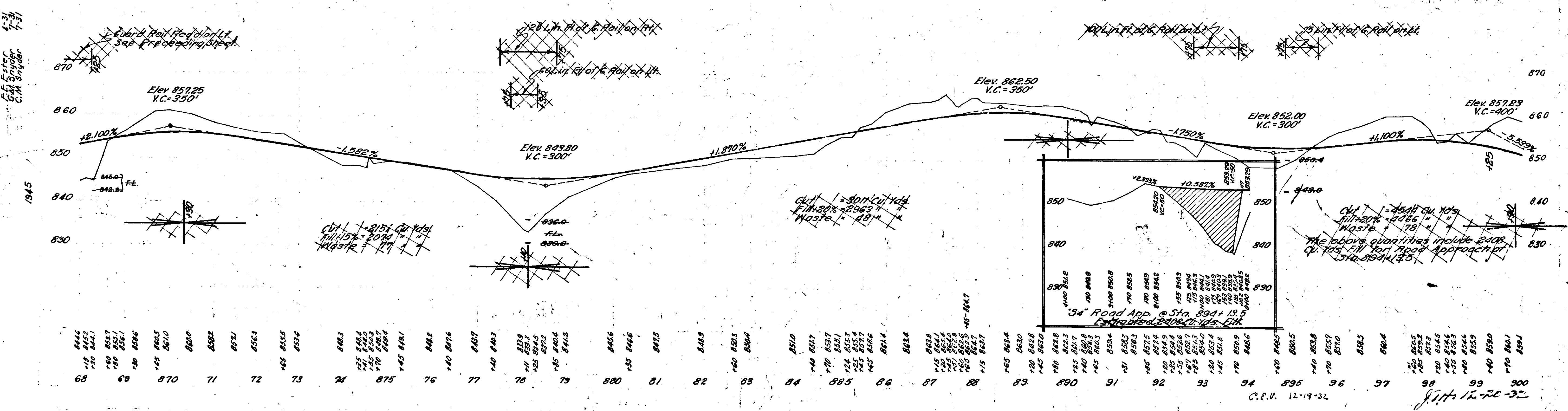
SEC. - C - PAVING 20



ALL R.O.F.W. ON THIS SHEET 35' ON EACH SIDE OF C EXCEPT AS SHOWN

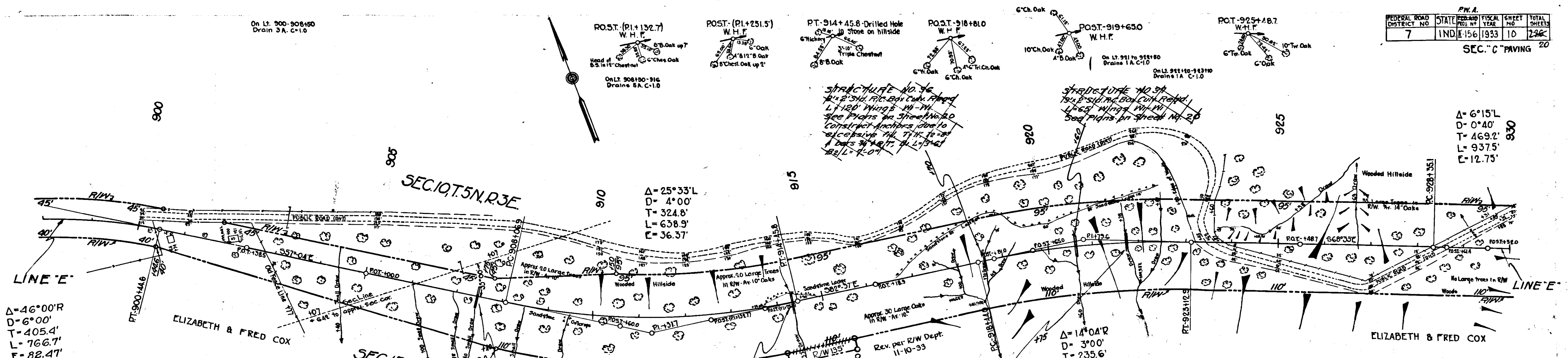


B.M. #127	Elev. 859.41	Bl. Spk. in Root of 14" Cedar 110' E of Sta. 870+50
B.M. #128	832.33	" " " " 4" Cherry 100' E of Sta. 871+45
B.M. #129	862.16	" " " " 12" Apple 12' E of Sta. 885+40
B.M. #130	862.28	" " " " 12" Apple 47' E of Sta. 899+33



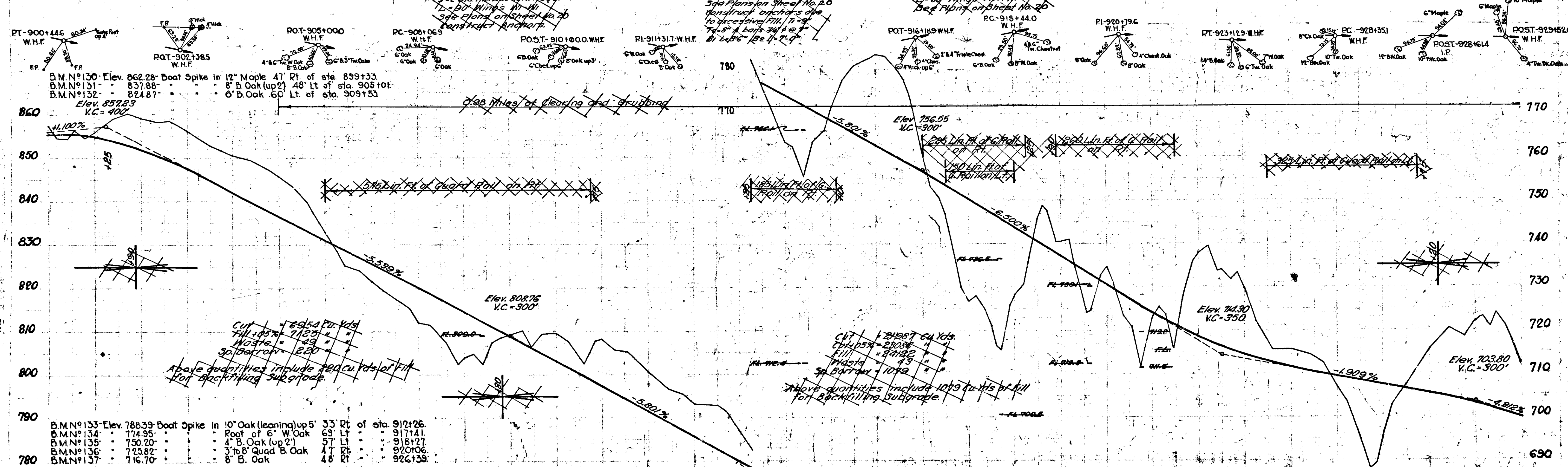
On Lt. 500+308.60
Drain 3A, C-1.0

P.N.A.				
DISTRICT NO	STATE	FISCAL YEAR	SHEET NO	TOTAL SHEETS
7	IND.	1933	10	246
SEC. C PAVING 20				



See Preceding Sheet for Additional
Pavement Req'd. for Curve Widening.

ALL RW ON THIS SHEET 35'
ON EACH SIDE OF E
EXCEPT AS SHOWN.



38	39	900	01	02	03	04	905	06	07	08	09	910	11	12	13	14	915	16	17	18	19	920	21	22	23	24	925	26	27	28	29	930
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7/17/20-32

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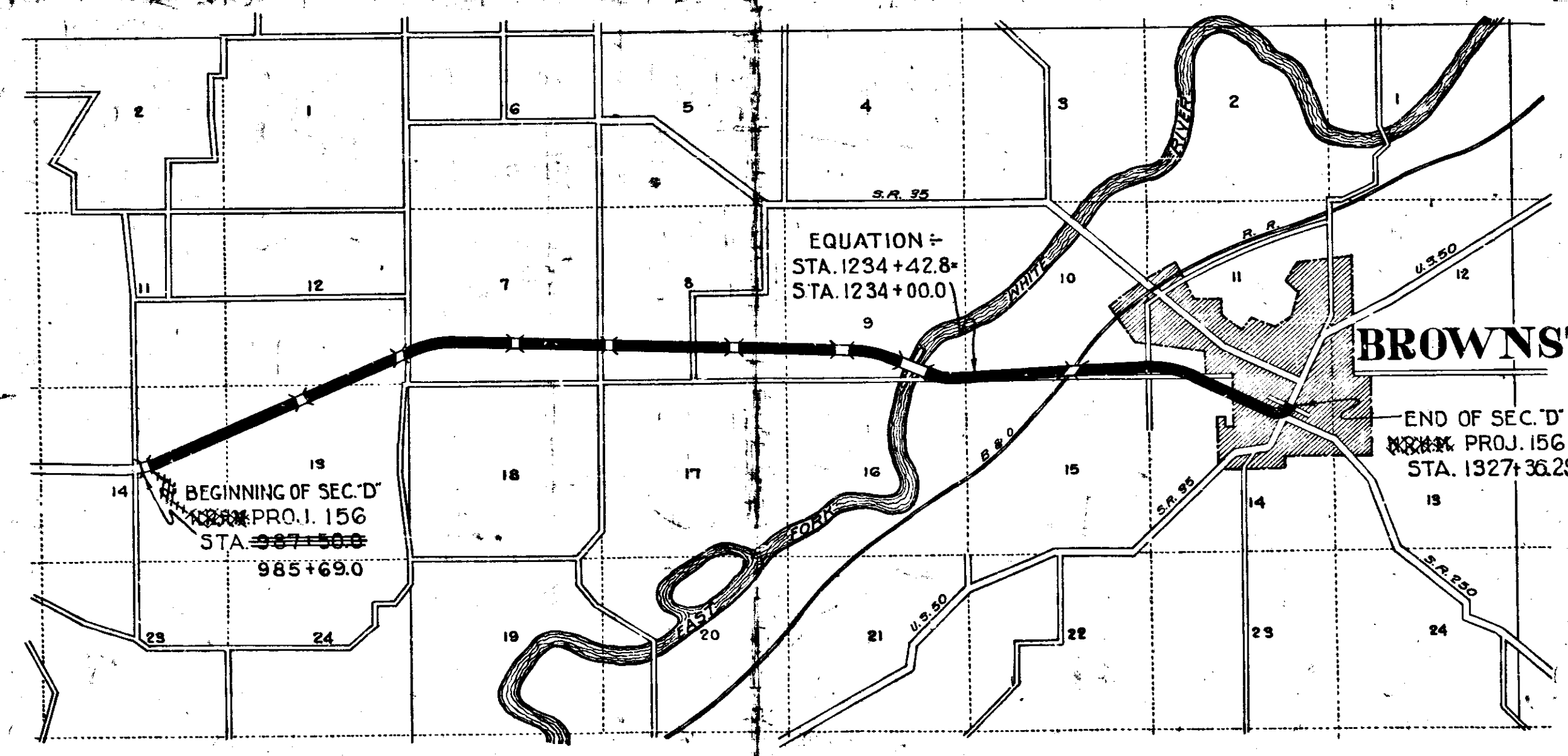
SHEET NO. 1	TITLE SHEET
SHEET NO. 2	STANDARD PAVING SECTIONS C.I.F.P. APRIL 28 1934
SHEET NO. 3	STANDARD PAVING SECTIONS C.I.F.P. MAY 1934
SHEET NO. 4-13	PLAN AND PROFILE
SHEET NO. 14	BRIDGE CULVERT DATA ESTIMATE OF QUANTITIES
SHEET NO. 15-16	MISCELLANEOUS STANDARDS SHEET A JAN 1933 REV. OCT. 6 1930 (SHEET B JAN 1933 REV. OCT. 28 1930)
SHEET NO. 17	FLEXIBLE STEEL PLATE GUARDRAIL Oct. 1934
SHEET NO. 18	STANDARD EXPANSION JOINTS AUG. 21 1934
SHEET NO. 19	STANDARD PAVEMENT JOINTS AUG. 21 1934
SHEET NO. 20	DATA FOR SUPERELEVATIONS AND WIDENING CURVES SEPT. 1932
SHEET NO. 21	STANDARD DETOUR SIGNS JAN. 1927

**STATE OF INDIANA
STATE HIGHWAY COMMISSION**

**PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY
NRHM PROJECT NO.156 SEC. D GRADING PAVING
BEDFORD-SEYMOUR ROAD**

BEGINNING APPROX. 270 FEET EAST OF THE CENTER OF SEC.14,T5N,R3E,AND
RUNNING EAST TO A POINT ON MAIN STREET APPROXIMATELY 100 NORTH OF
COMMERCE STREET IN BROWNSTOWN IN JACKSON COUNTY.

GROSS LENGTH - 3.479 MI. 6.479
NET LENGTH TO BE CONSTRUCTED - 5.875 MI. 6.154
MAXIMUM GRADE TO BE CONSTRUCTED - 7.00%
SCALE: PLAN LONG. 1"=100' PROFILE HORIZ. 1"=100' VERT. 1"=10'
MAX. GRADE= 7.00%



Scale 1"=3000'
(APPROX)

REVISIONS

DATE	SHEET NO.	REVISIONS
MARCH 10, 1934	14 & 15	Estimate
MARCH 11, 1934	Class Section	On 100-125 Fill Slopes changed from 1 1/2 to 2 to 1
MARCH 12, 1934		Specifications to 1934
April 26, 1934	1 & 4	Beginning of Project

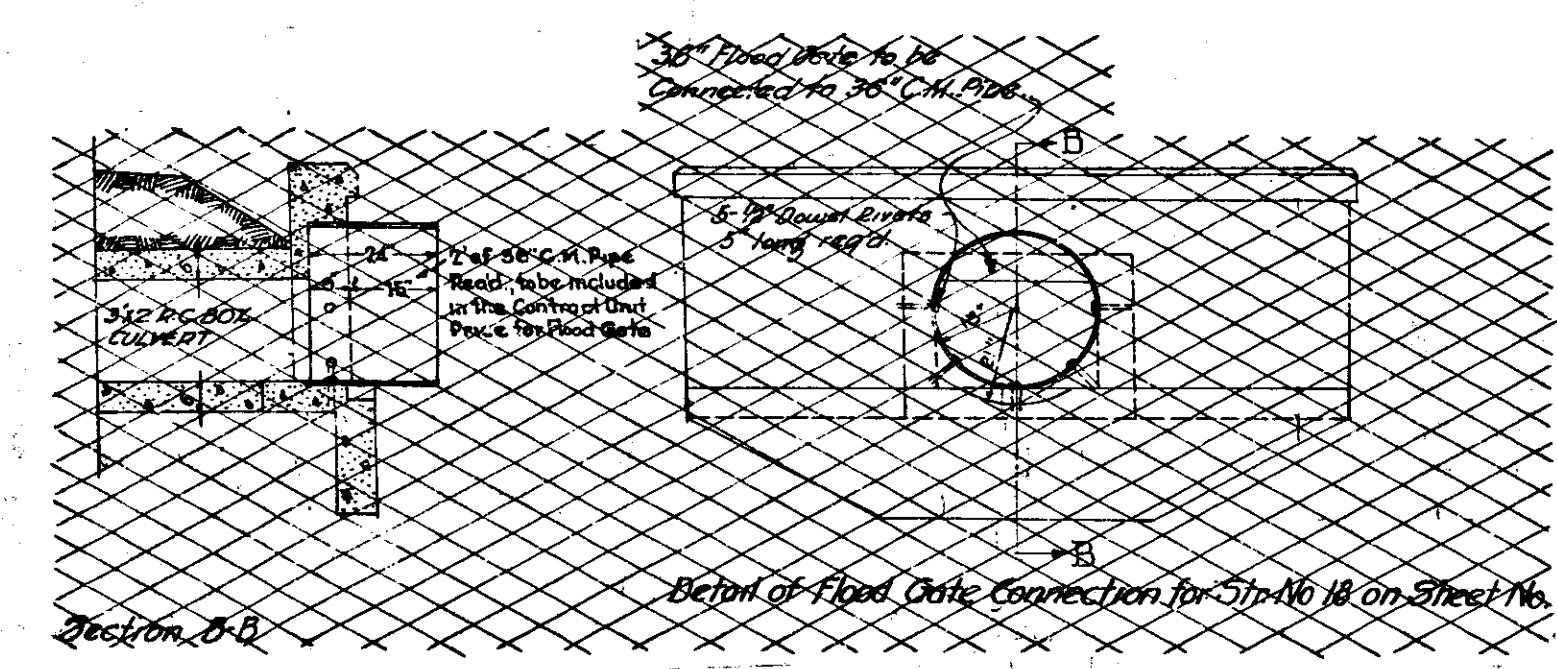
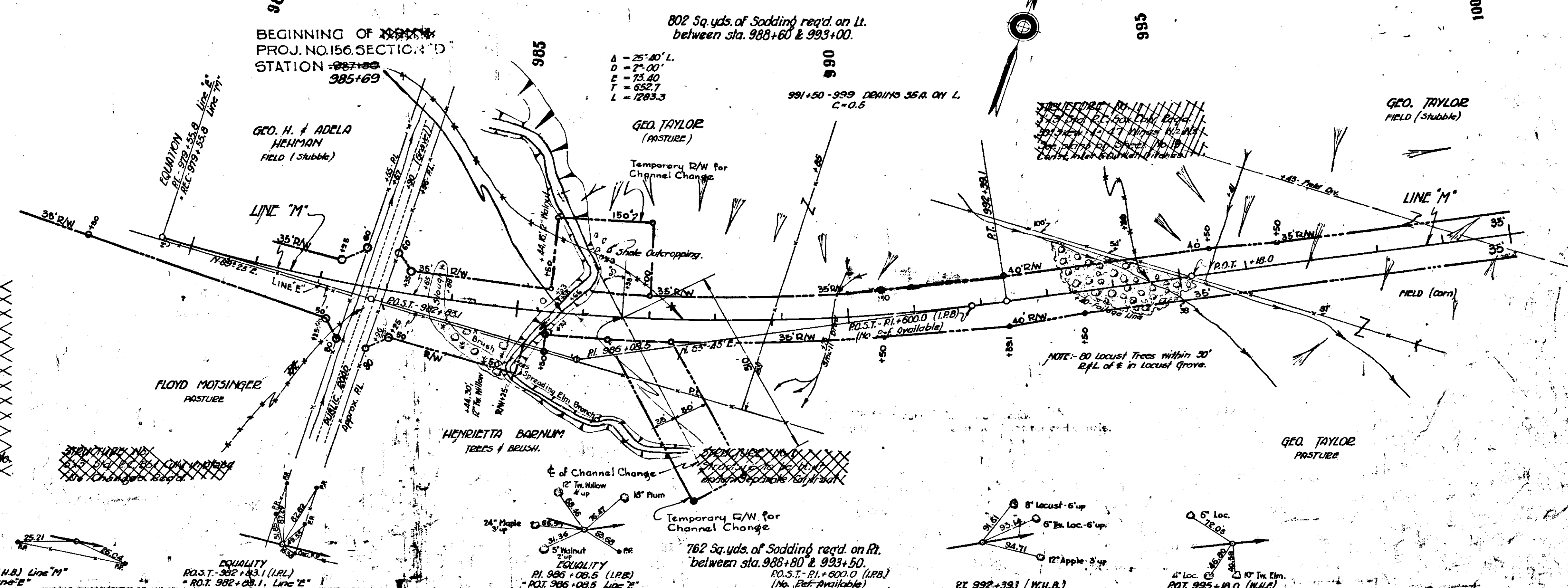
STATE HIGHWAY COMMISSION OF INDIANA
STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION, DATED 1934,
ON FILE WITH U. S. BUREAU OF PUBLIC ROADS, TO BE USED WITH
THESE PLANS.

APPROVED AND ADOPTED 11-16-34
BY STATE HIGHWAY COMMISSION OF INDIANA
James Adams
CHAIRMAN-STATE HIGHWAY COMMISSION OF INDIANA
APPROVED 11-16-34
McKeef
CHIEF ENGINEER-STATE HIGHWAY COMMISSION OF INDIANA
APPROVED AND ADOPTED 12-11-33
BY STATE HIGHWAY COMMISSION OF INDIANA
James Adams
CHAIRMAN-STATE HIGHWAY COMMISSION OF INDIANA
APPROVED 12-11-33
McKeef
CHIEF ENGINEER-STATE HIGHWAY COMMISSION OF INDIANA
RECOMMENDED FOR APPROVAL
RECOMMENDED FOR APPROVAL
APPROVED

C.C.V. 11-29-33

FEDERAL ROAD DISTRICT No.	STATE	PROJECT No.	FISCAL YEAR	SHEET No.	TOTAL SHEETS
7	IND.	156	1934	4	21
SECTION 5					REQUIRE 21
					PAVING

NOTE: ALL RIGHT-OF-WAY ON THIS SHEET AS SHOWN



G.M. #1 - Elev. 553.28 - 24' Splice in 8" Maple 180' L. 982+70
 #2 - Elev. 552.01 - 24' Splice in 8" Locust 45' L. 993+97

STANDARD SECTION C-1 ADOPTED OCT. 1929 (REV. OCT. 1933)
 STANDARD SEC. C-8 GB ADOPTED AUG. 1931 (REV. APR. 20, 1932) TYPICAL
 SECTION AS SHOWN ON SHEET NO. 13 TO BE USED ON THIS PROJECT

NOTE: Between Sta. 100+00 & 1008+00, 11% slope shall be 3.0% instead of 1.5% for
 NOTE: Grade Line indicates 300' of finished pavement.
 Grade to be set on 4" Tar Rock Subbase and Asphaltic Macadam Pavement.
 STATE HIGHWAY COMMISSION OF INDIANA - Standard specifications
 for Road construction dated 1934 to be used with these plans.

STD. PAVING SECTION C-II GB ADOPTED JAN. 1934
 STD. PAVING SECTION C-II FR ADOPTED MAY 1934 TYPICAL
 SECTION AS SHOWN ON SHEET NO. 13 TO BE USED ON THIS PROJECT.

(Except that Shoulders, Ditches, and Slopes shall be constructed as shown by Detail on Sheet No. 13)

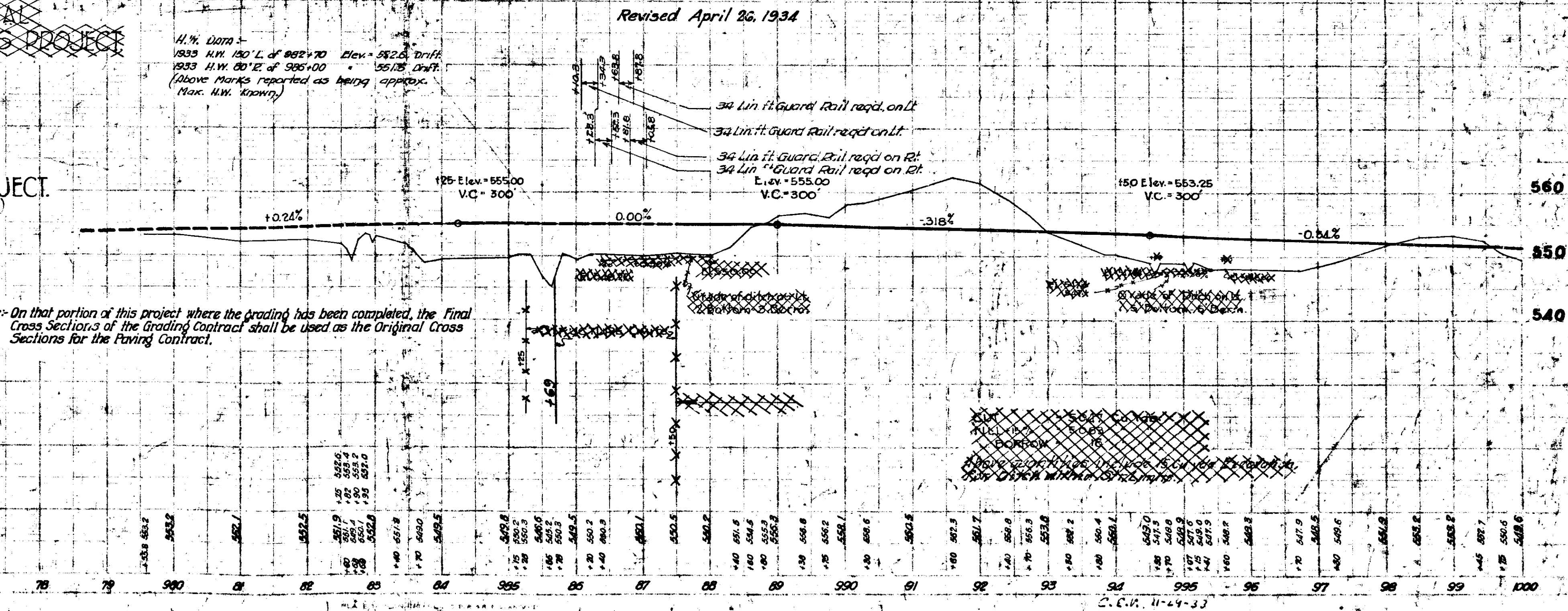
NOTE: Standards under date as listed in Index on Title Sheet, to be used on this PROJ.
 All curves of 5' and over will be widened according to Standards of 1932. The crown
 will be taken out of all curves over 25'

NOTE: All 1/2" diam. manholes, when Metal and set on the same ground level
 shall be set on a 12" x 12" x 12" concrete base as specified by the 1932 Code.
 NOTE: Where shoulders and slopes are constructed on an embankment,
 they shall be finished to the pad for as shown on the plans.

NOTE: Project Engineer shall Cross Section Paper Rebarrier between Sta. 1322+M
 & Sta. 1327+35.22 before Construction begins.

In constructing Standard or Special Combined Curb & Gutter, preformed expansion joints shall
 be placed at the beginning and end of all radii and wherever expansion joints are placed through
 Reinforced Concrete Pavement or at 80' intervals for Rock Asphalt or Bituminous Concrete
 Pavements. The cost of such labor and material for placing these expansion joints in combined
 curb and gutter shall be included in the contract unit price per lineal foot for Standard Combined
 Curb & Gutter or Special Combined Curb & Gutter.

At all radii of street intersections, 1/2" Bituminous Expansion Joint shall be placed between Gutter
 and Reinforced Concrete Pavement from beginning to end of radii. Also where Sidewalk abuts Curb
 1/2" Bituminous Expansion Joint shall be placed between Sidewalk and Curb.

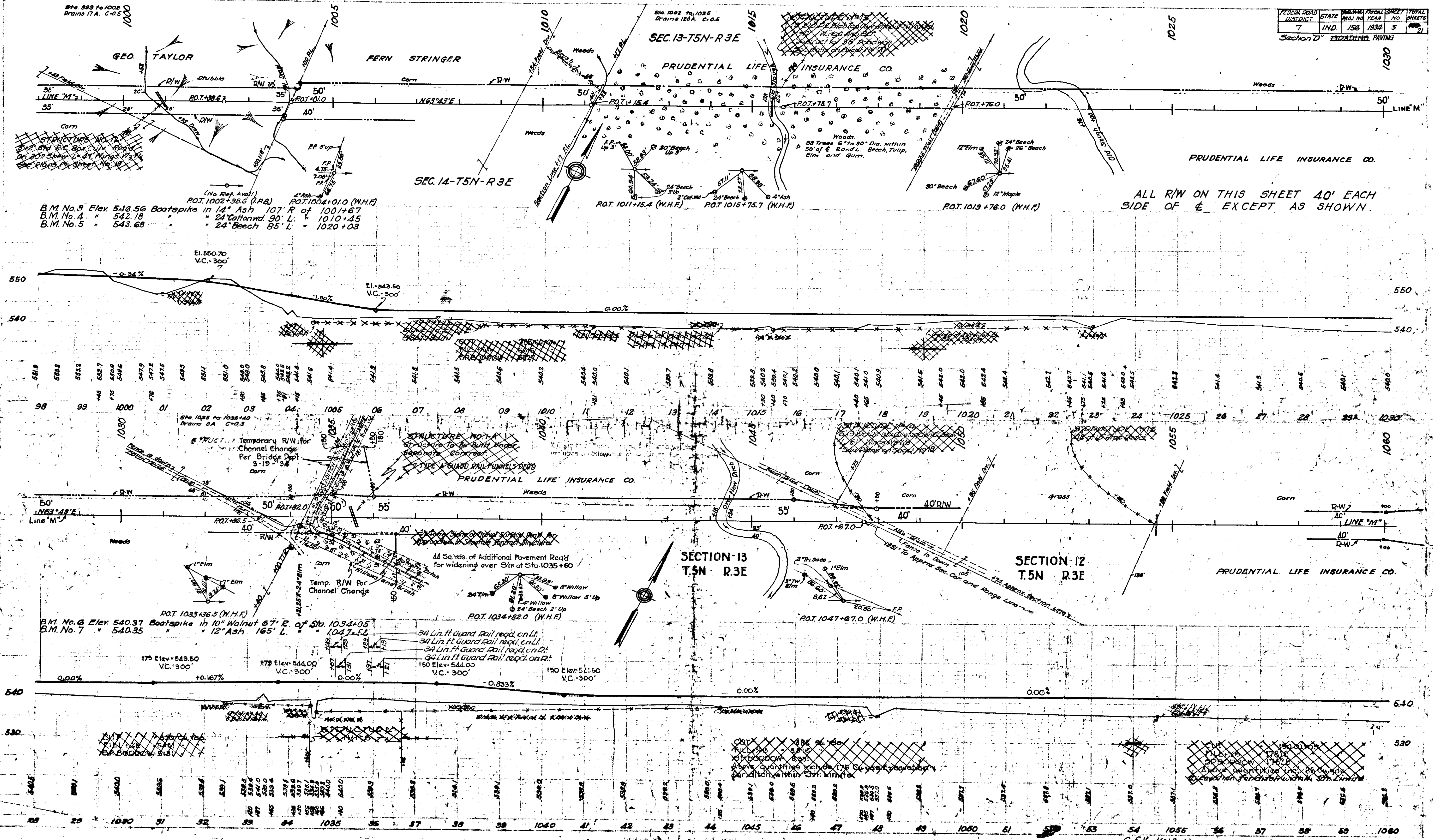


78 79 980 81 82 83 84 85 86 87 88 89 90 91 92 93 94 95 96 97 98 99 1000

C. E. W. 11-24-33

PLANNED ROAD DISTRICT	STATE	SECTION	SHEET NO.	TOTAL SHEETS
7	IND.	156	1830	5

Section D - BRADSHAW PAVING



7-35
 D.L. Dawson
 L.E. Wolfe
 Robert T. Simon

7-35
 D.L. Dawson
 L.E. Wolfe
 Robert T. Simon

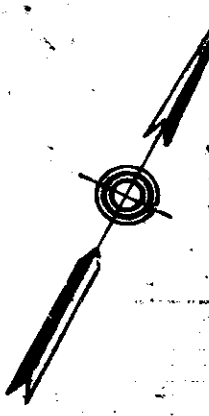
7-35
 D.L. Dawson
 L.E. Wolfe
 Robert T. Simon

(No Ref. Avail)
 ROT. 1002+98.5 (I.P.B.) ROT. 1004+01.0 (W.H.F.)
 B.M. No. 9 Elev. 546.56 Boatspike in 14" Ash 107' R of 100+45.7
 B.M. No. 4 " 542.18 " 24" Cottonwood 90' L " 1010+45
 B.M. No. 5 " 543.68 " 24" Beech 85' L " 1020+03

ROT. 1033+36.5 (W.H.F.)
 B.M. No. 6 Elev. 540.37 Boatspike in 10" Walnut 67' E of Sta. 1034+05
 B.M. No. 7 " 540.35 " 12" Ash 165' L " 1047+55

179 Elev. 543.50 VC=300'
 178 Elev. 544.00 VC=300'
 150 Elev. 541.50 VC=300'

34 Lin. ft. Guard Rail reqd. on Lt.
 34 Lin. ft. Guard Rail reqd. on Lt.
 34 Lin. ft. Guard Rail reqd. on Lt.
 34 Lin. ft. Guard Rail reqd. on Lt.



ALL R/W ON THIS SHEET 40' EACH SIDE OF & EXCEPT AS SHOWN.

PRUDENTIAL LIFE INSURANCE CO.

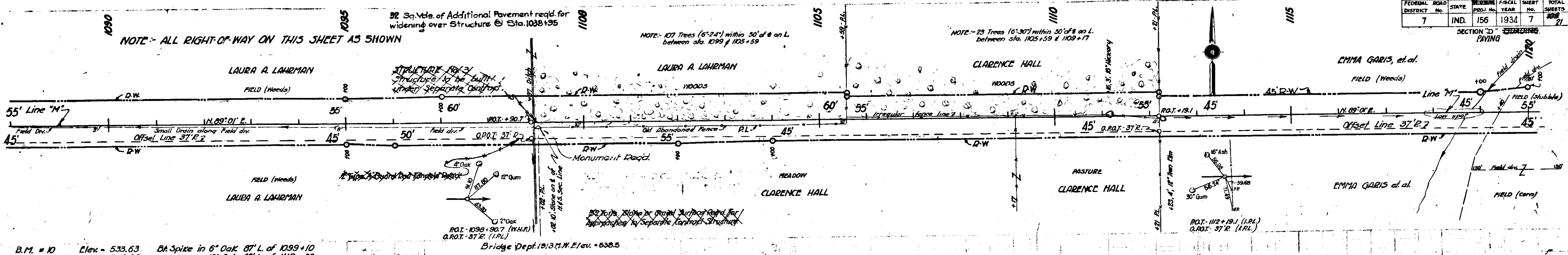
C.E.V. 11-29-33

FEDERAL ROAD DISTRICT No.	STATE	SECTION	FISCAL YEAR	SHEET No.	TOTAL SHEETS
7	IND.	156	1934	7	21

NOTE - ALL RIGHT-OF-WAY ON THIS SHEET AS SHOWN

NOTE - 17 Trees (6'-24") within 50' of L. between sta. 1099 & 1105+59

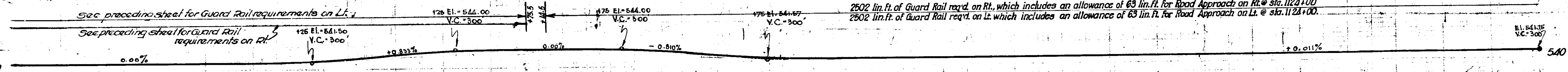
NOTE - 23 Trees (6'-30") within 50' of L. between sta. 1105+59 & 1109+77



B.M. # 10 Elev. = 533.63
 B.M. # 11 Elev. = 533.93
 8" Spike in 6" Oak 87' L. of 1099+10
 16" Ash 53' L. of 1112+00

Bridge Dept. 1913 H.W. Elev. = 538.5

2502 lin. ft. of Guard Rail req'd on Rt. which includes an allowance of 63 lin. ft. for Road Approach on Rt. @ sta. 1124+00
 2502 lin. ft. of Guard Rail req'd on Lt. which includes an allowance of 63 lin. ft. for Road Approach on Lt. @ sta. 1124+00



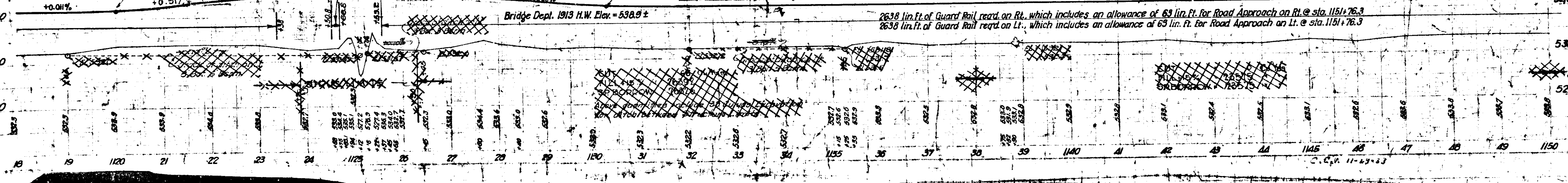
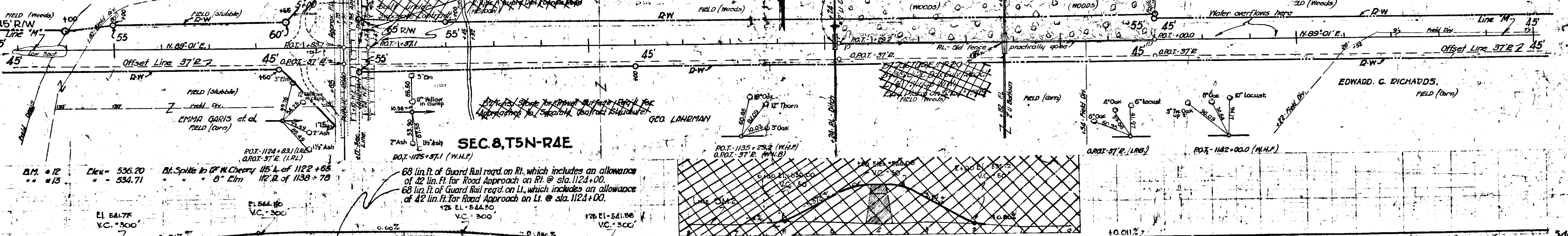
SEC. 7, T5N, R4E.

SEC. 8, T5N, R4E.

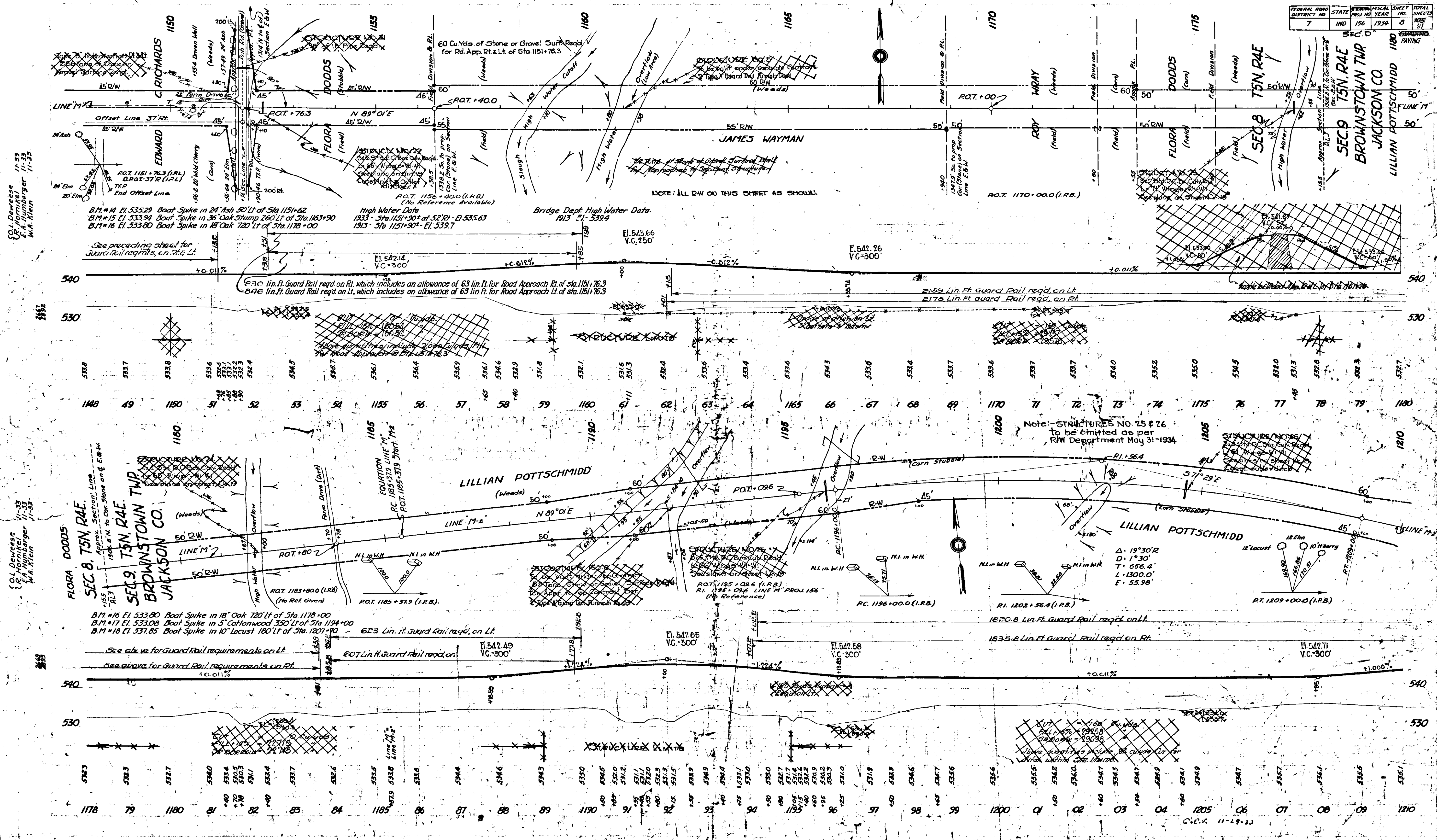
90 Cords of Stone or Gravel Surfaces Req'd. for Rd. App. Rt. & Lt. of Sta. 1124+00

68 lin. ft. of Guard Rail req'd on Rt. which includes an allowance of 42 lin. ft. for Road Approach on Rt. @ sta. 1124+00.
 68 lin. ft. of Guard Rail req'd on Lt. which includes an allowance of 42 lin. ft. for Road Approach on Lt. @ sta. 1124+00.

2638 lin. ft. of Guard Rail req'd on Rt. which includes an allowance of 63 lin. ft. for Road Approach on Rt. @ sta. 1151+76.3
 2638 lin. ft. of Guard Rail req'd on Lt. which includes an allowance of 63 lin. ft. for Road Approach on Lt. @ sta. 1151+76.3



FEDERAL ROAD DISTRICT NO.	STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND	1934	0	21



S.O. DeWesse 11-33
 E.A. Hornikel 11-33
 E.A. Humbarger 11-33
 W.H. Klein 11-33

S.O. DeWesse 11-33
 E.A. Hornikel 11-33
 E.A. Humbarger 11-33
 W.H. Klein 11-33

NOTE: ALL R/W ON THIS SHEET AS SHOWN.

Note: STRUCTURES NO. 75 & 76
 To be omitted as per
 R/W Department May 31-1934

B.M. #16 El. 533.80 Boat Spike in 18" Oak 720' Lt of Sta. 1178+00
 B.M. #17 El. 533.08 Boat Spike in 3" Cottonwood 350' Lt of Sta. 1194+00
 B.M. #18 El. 531.85 Boat Spike in 10" Locust 180' Lt of Sta. 1201+00

See above for guard rail requirements on Lt.
 See above for guard rail requirements on Rt.

C.I.C. 11-19-33

FEDERAL ROAD DISTRICT NO.	STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND	1934	9	21

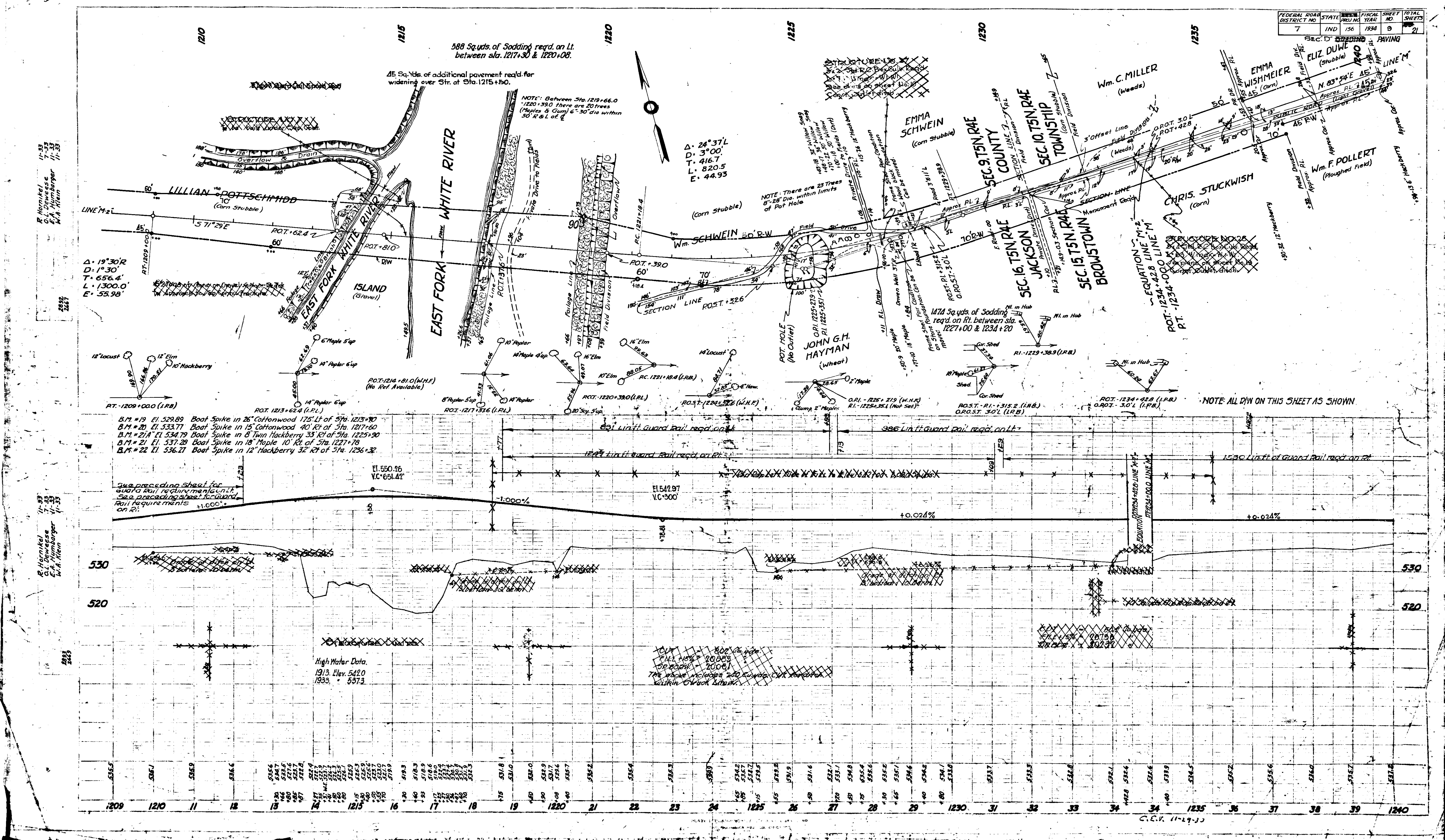
586 Sq. Yds. of Sodding req'd. on Lt. between sta. 1217+30 & 1220+00.
45 Sq. Yds. of additional pavement req'd. for widening over Str. of Sta. 1215+50.

NOTE: Between Sta. 1215+66.0 & 1220+390 there are 20 trees (Maples & Comps) 6"-30" dia within 50' R & L of C.

$\Delta = 24^{\circ}37'$
 $D = 3^{\circ}00'$
 $T = 416.7$
 $L = 820.5$
 $E = 44.95$

NOTE: There are 23 Trees 8"-28" Dia. within limits of Pot Hole.

NOTE: ALL D.W. ON THIS SHEET AS SHOWN.



1-33
E. Harnikel
F. A. Humberger
W. A. Klein

1-33
E. Harnikel
F. A. Humberger
W. A. Klein

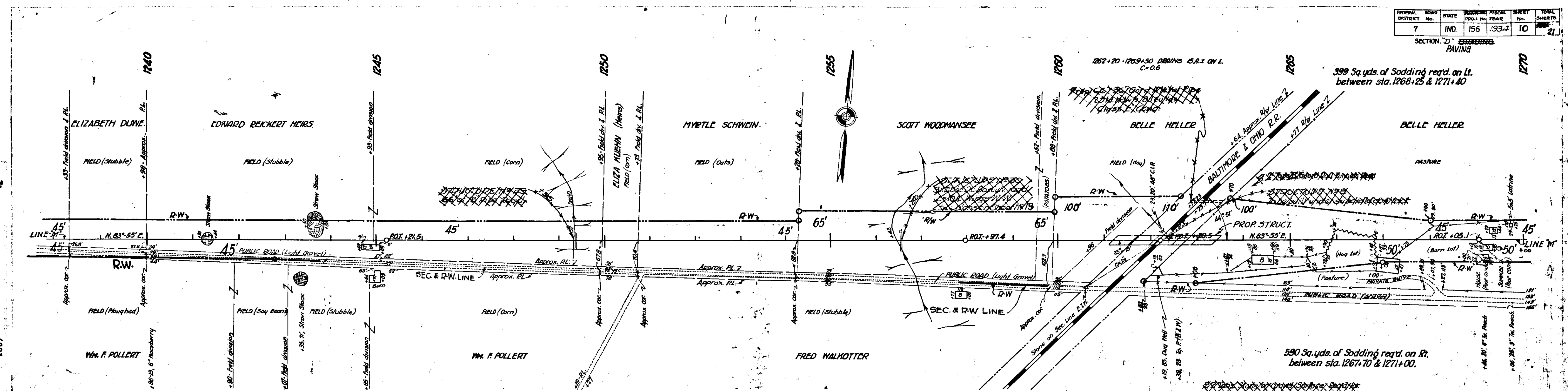
High Water Data
1913 Elev. 542.0
1933 " 537.3

NOTE: ALL D.W. ON THIS SHEET AS SHOWN.

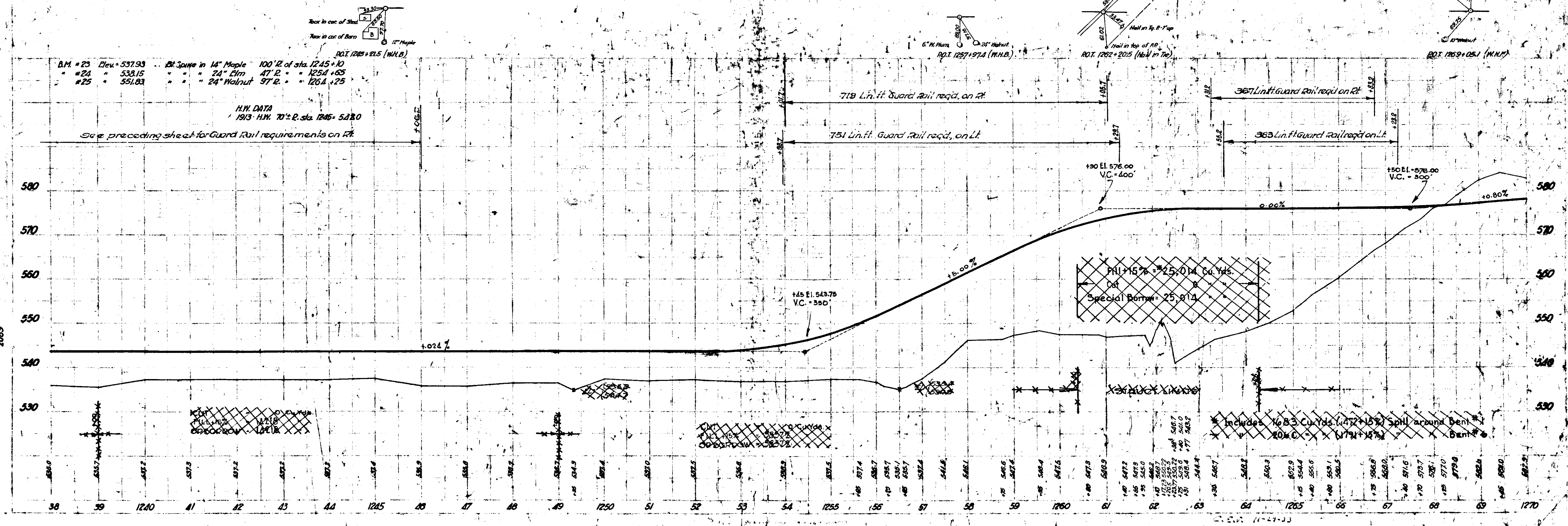
C.C.R. 11-29-33

TOWNSHIP	RANGE	STATE	PROJECT	YEAR	SHEET	TOTAL SHEETS
7	IND.	156	1934	10	21	

SECTION 20 - PAVING



NOTE - ALL RIGHT-OF-WAY ON THIS SHEET AS SHOWN



BM # 23 Elev. 537.93
 # 24 " 538.15
 # 25 " 551.83

H.W. DATA
 1913 H.W. 70" R. sta. 1245+10
 See preceding sheet for Guard Rail requirements on Rt.

718 Lin. ft. Guard Rail reqd. on Rt.
 751 Lin. ft. Guard Rail reqd. on Lt.
 367 Lin. ft. Guard Rail reqd. on Rt.
 363 Lin. ft. Guard Rail reqd. on Lt.

PHI 15% = 25.014 Cu Yds.
 Special Bottom = 25.014

Includes 1/2 Cu Yds. (172' x 15% Spill around Bent)

O.L. Drawings
 L.A. Public
 F.A. Perry
 7-31
 6-33
 6-33
 2667

O.L. Drawings
 L.A. Public
 F.A. Perry
 7-31
 6-33
 6-33
 2669

Station	Quantity	Description
1270	1320 Sq. yds.	Sodding reqd. on Lt. between sta. 1273+75 & 1280+65
1309	"	"
1405	"	"
668	"	"
217	"	"
247	"	"

FEDERAL ROAD DISTRICT	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	156	1932	11	21

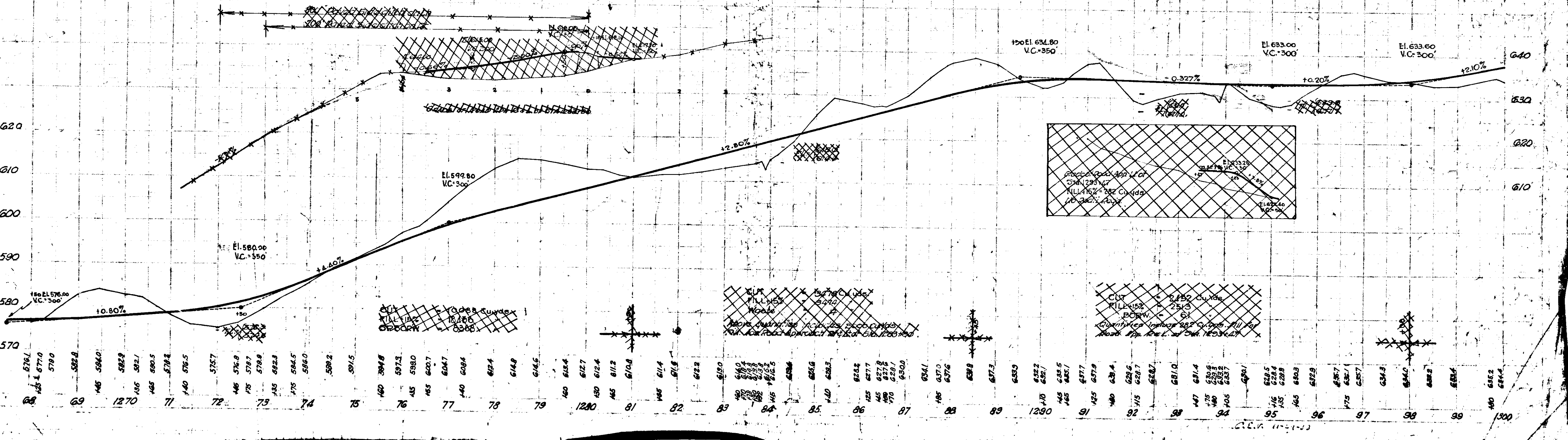
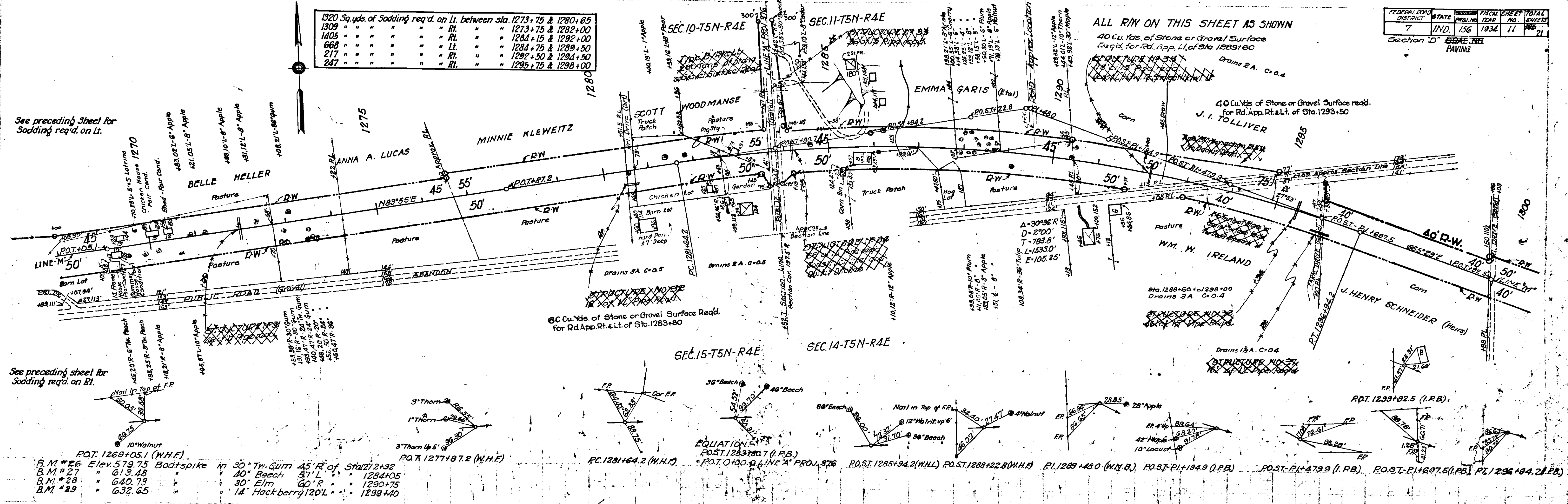
Section D
GRAVEL SURFACE PAVING

ALL R/W ON THIS SHEET AS SHOWN

40 Cu. Yds. of Stone or Gravel Surface reqd. for Rd. App. Rt. Lt. of Sta. 1289+00

40 Cu. Yds. of Stone or Gravel Surface reqd. for Rd. App. Rt. Lt. of Sta. 1293+50

60 Cu. Yds. of Stone or Gravel Surface reqd. for Rd. App. Rt. Lt. of Sta. 1283+80



See preceding sheet for Sodding reqd. on Lt.
See preceding sheet for Sodding reqd. on Rt.

POT. 1269+05.1 (W.H.F.)
B.M. #26 Elev. 579.75 Boat spike in 30" Tr. Gum 45' E of Sta. 1272+92
B.M. #27 " 613.48 " 40" Beach 31' E of " 1284+05
B.M. #28 " 640.73 " 30" Elm 60' R " 1290+75
B.M. #29 " 632.65 " 14" Hackberry 120' L " 1299+40

EQUATION:
POST. 1283+70.7 (I.P.B.)
= POT. 0+100.0 LINE A PROJ. 576

O.L. Parsons
Ralph T. Simon

68 69 1270 71 72 73 74 75 76 77 78 79 1280 81 82 83 84 85 86 87 88 89 1290 91 92 93 94 95 96 97 98 99 1300

ALL R/W ON THIS SHEET AS SHOWN

BROWNSTOWN

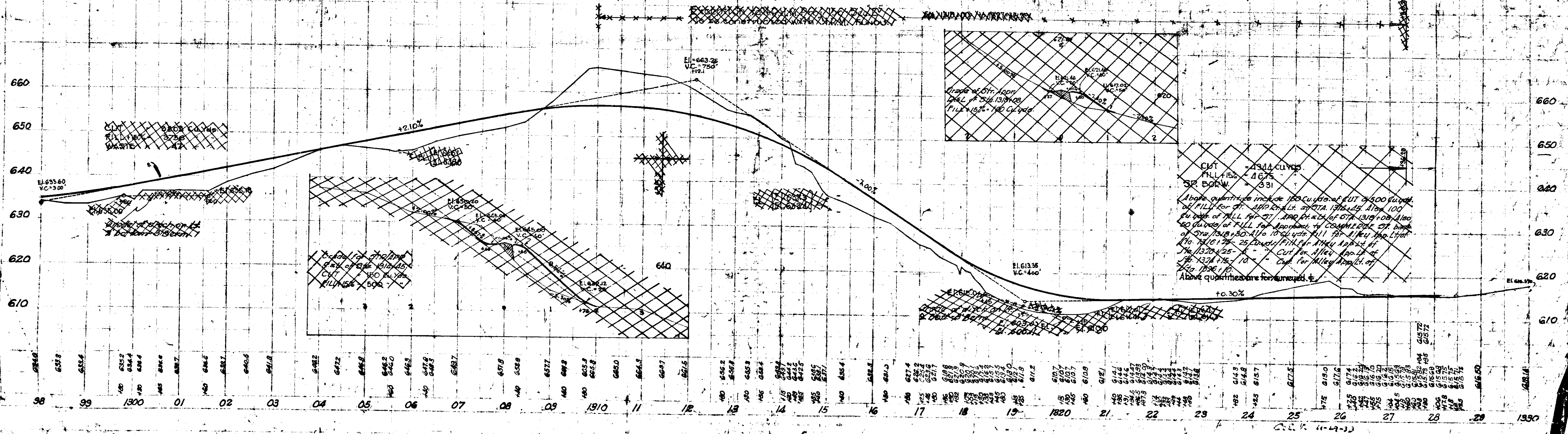
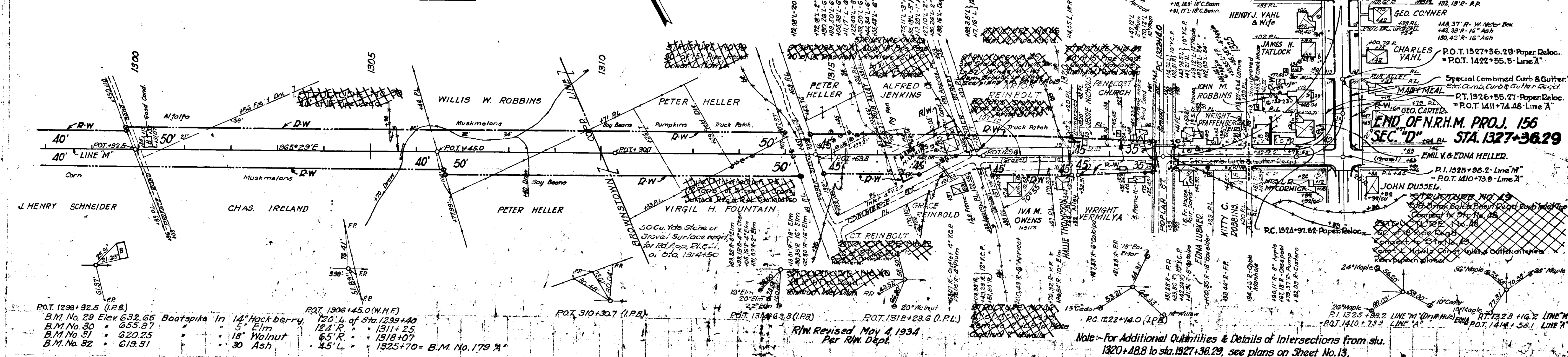
FEDERAL RD. DIST.	STATE	PROJ. NO.	YEAR	SHEET NO.	TOTAL SHEETS
7	IND	156	1934	12	12

1033 Sq. yds. of Sodding req'd. on Rt. between sta. 1308+00 & 1314+15			
1140	"	"	Li. " 1308+40 & 1314+65
241	"	"	" " " 1324+00 & 1327+00
136	"	"	" " " 1324+25 & 1326+25

FOR DETAILS OF INTERSECTION
SEE SHEET No. 13

Stone or Gravel Surface req'd.
for Street & Alley Approaches
1316+25 Left 10 Cu. Yds.
1316+30 Left Right 30 Cu. Yds.
1320+25 Left 10 Cu. Yds.
1322+14 Left & Right 5 " "
Commerce St. East

PAPER RELOCATION.
A = 90° 20' Lt.
D = 57° 18'
T = 100.58'
L = 157.65'
R = 11° 05'
D = 100'
SURVEY C.
Δ = 90° 20' Lt.
D = 15° 00'
T = 394.2'
L = 602.2'
E = 153.8'



O. L. Deweese
L. E. Riddle
Ralph T. Simon

O. L. Deweese
L. E. Riddle
Ralph T. Simon

END OF N.R.H.M. PROJ. 156
SEC. "D" STA. 1327+36.29

Note - For Additional Quantities & Details of Intersections from sta. 1320+48.8 to sta. 1327+36.29, see plans on Sheet No. 13.

