INDEX

SHEET NO.	DESIGNATION	B. P. R. APPROVAL	DATE ADOPTED A or LATEST REVISION R
	TIFLE SHEET	CREATE TO A CANADA CONTRACTOR OF THE CANADA	
	ST D. DIV. LANE (INTERSTATE) ST D. DIV. LANE (INTERSTATE)	Control of the Contro	
	ST'D. DIV. LANE (INTERSTATE)	Company Command on Marketing	
2	STO CROSS SECTION, (E-11-1R)	and the second of the second o	4-17/64
	ST'D CROSS SECTION, ST'D CROSS SECTION,	A CONTRACTOR OF THE PROPERTY O	
3 & 4	TYPICAL CROSS SECTION	Section 1 to the second of the	
	ST'D 8 INCH RAMP SECTION	Contract Contract (Contract Contract Co	The same of the same reconstruction of the same of the
5	ST'O NO INCH RAMP SECTION PLAT 2 FOR RIW DEPARTMENT		
	ILB		6-11-69
6 7 - 25	ST'D. PAVEMENT JOINTS SHEET "A" PLAN AND PROFILE	5-23-62	5-11-62
	CEAN AND INCIDEN		
26	STRUCTURE DATA STRUCTURE DATA	<u> </u>	promoter for \$155,500 as summer would as advantage and assessment of the summer of the sum of the s
27	ESTIMATE OF QUANTITIES	* Commence of the commence of	C S AND
28	ESTIMATE OF QUANTITIES	and the second s	
29	MISCELLANEOUS STANDARDS, SHEET "MA"	3.25-64	2-1-64
30	MISCELLANEOUS STANDARDS SHEET "MB"	3-25-64	2-1-64
31	MISCELLANEOUS STANDARDS, SHEET "MC"	1-26-60	4 - 1 - 5 <u>9</u> 7 - 6 - 60
32 33	MISCELLANEOUS STANDARDS, SHEET "MC!" MISCELLANEOUS STANDARDS, SHEET "MD"	8-26-60 1-26-60	5-26-59
34	MISCELLANEOUS STANDARDS, SHEET "MDI"	1-26-60	3-6-57
35	MISCELLANEOUS STANDARDS, SHEET ME	and the same of the same and th	R-9-1-64
	MISCELLANEOUS STANDARDS, SHEET "ME"	A CONTRACTOR OF THE PARTY OF TH	
36	MISCELLANEOUS STANDARDS, SHEET "MH"	3-25-64	2-1-64
37	MISCELLANEOUS STANDARDS, SHEET "MI"	3-25-64	2-1-64
38	MISCELLANEOUS STANDARDS, SHEET "M/" MISCELLANEOUS STANDARDS, SHEET "MK"	3-25-64	2-1-64
39	MISCELLANEOUS STANDARDS, SHEET MN	5-9-61	4-11-61
40 & 41	MISCELLANEOUS STANDARDS, SHEET "MP", MP1	6-4-64	11-20-63, 11-20-63
42 43	MISCELLANEOUS STANDARDS, SHEET "MQ" MISCELLANEOUS STANDARDS, SHEET MR	and a second	4-17-64 4-17-64
43	MISCELLANEOUS STANDARDS, SHEET S		
	MISCELLANEOUS STANDARDS, SHEET "S-1"		
44 45	MISCELLANEOUS STANDARDS, SHEET MT-G" MISCELLANEOUS STANDARDS, SHEET MU	5- 7-62 3-25-64	11-2-61
43	MISCELLANEOUS STANDARDS, SHEET MY		
	MISCELLANEOUS STANDARDS, SHEET MV-1		
46	MISCELLANEOUS BRIDGE STANDARDS M-3	4-17-64 3-28-63	8-27-63 R 2-18-63
47A	BRIDGE STANDARDS J		A-7-27-64
47B	MISCELLANEOUS BRIDGE STANDARDSM-5	12-4-62	R-10-4-62
ina di	and the second s		
		entre de la composition de la composit La composition de la composition della compo	
	STD. SUARI RAIL	6 - 6-61	5-25-61
48	BEAM GUARD RAIL - GR-1	<b>V</b> -	× 23 01
		A CAMPAGA A CAMPAGA A CAMPAGA A CAMPAGA A CAMPAGA A	
	CTO FOR SUPERSUEVATION	2 -27-61	A. Jon. 1861
49 /	STO FOR SUPERELEVATION STO FOR SUPERELEVATION AND WIDENING OF CURVES	4	
50/	ST'D. DETOUR SIGNS, SHEET I	The second secon	₹. <u>3 - 8-63</u>
51/ 52/- 54	STD DETOUR SIGNS, SHEET 2 STD. DETOUR SIGNS, SHEET 3, 3A, 5		R. 3 - 8-63 R. 3 - 8-63
527-34   55	STD CONSTRUCTION IDENTIFICATION SIGN	3 - 9-64	A. Feb. 1964
56-103	CROSS SECTIONS: 100		

	REVI	SIONS
SHEET NO.	DATE	REVISED
1. 8,12,13,16,21&	22 6-1-64	RIW-Per Road and Bridge Design Rept.
1,5,7,8,9,10,0,12,13	8-19 1-25-65	RIW & References Per Road Design Dept.
1,5,7,8,9,10,11,12,13	2-16-65	Length & Location of Drive Per Rd. Design
16/18	1-10-66	
7 £ 8	1-25-66	CHANGE PARCEL NO.16 TO NO 30 AND NO 16A TONG 16
8,15,20\$22	5-20-66	DRIVE RELOCATED
7615	5-31-66	PROVISIONAL RIW PER RIWLEPT
7911121317,202	1422 8-23-66	SEWER & TEMP. RIW ADDED
6613	12-7-66	P.L. & R/W
8, 16,20,22	7-26-68	Lt. Turn & Drive Added

INDEX	FOR RIGHT OF WAY PLANS
SHEET NO	DESCRIPTIONS
	TITLE SHEET AND R/W INDEX
2	PARCEL LISTING
3 & 4	TYPICAL CROSS SECTIONS
100% / <b>5</b> / 100	PLAT 2
GTHRU12	PLAN AND PROFILE
13 THRU 19	DETALLS
20 & 21	APPROACH TABLE
22	STRUCTURE DATA
23	PLAT 3

BEGINNING OF "S" PROJ. NO. 330 (G) -Sta. 118 + 33.47 P.R. Line #1 O.P.O.T. 118 + 33.47 Line "B" 42.00' LT. = END OF "S" PROJ. NO. 330 (4) STA. 160+90.16

> LEGEND Barricade Type A Barricade Type B Tupical Signistandarch Construction Identification Signs

Urban Limits

INDIANA STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS DATED 1963 TO BE USED WITH THESE PLANS

RIGHT OF WAY PLANS

STATE OF INDIANA

CODE #0530

## DESIGN DATA A.D.T. (1963) A.D.T. (1983) PROJECTED DESIGN SPEED ACCESS CONTROL 70 M.P.H. PARTIAL LIMITED

THIS-PROJECT-INCLUDES-R/W-FOR SEPARATE CONTRACT STRUCTURE

## U.S. PROJECT NO. 330 (1) P.E.

=(1)=R/W. (1) CONST.

BEGINNING AT A POINT ON RELOCATED SR. NO. 135 37.5 FT. NCRTH OF THE SOUTH LINE OF SECTION 36, T. 15 N. - R. 3 E. AND EXTENDING THE A NORTHERLY DIRECTION TO A POINT ON S.R. NO. 135 A DISTANCE OF 1626.09 ALL IN PERRY \* TOWNSHIP, MARION COUNTY.

GROSS LENGTH:- 0.307\_MI

## EQUATIONS

- 1 {P.O.C. 150 + 38.62 P.R. Line #1= P.C. 150 + 16.7 Line "B"
- @ {P.O.T. 169 + 34.92 Line '8" = P.C. 169 + 34.92 P.R. #2
- 3 (PT. 188 + 29.92 PR #2-10POT 188 + 28.63 Line B. 42.00'Lt.
- (POT 190 + 00.0 P.R Line # 2= (0.20+ 190+00.0 Line "B" 42.00 Lt.
- 5 (POT. 192 + 74.8 Line "B" Extended = POT. 40 + 00.0 Line "S-9-ZZ"

@[POT. 38 + 96.33 Line 5.9-ZZ = P.G. 38 + 96.33 P.R. Line #3

5-330(6)

PROJECT NO. LINE SHEET TOTAL FILE .

23 Sheets

INDIANA STATE HIGHWAY COMMISSION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY

S. PROJECT NO. 330 (6) P.E.

(6) R/W.

(8) CONST.

BEGINNING AT A POINT ON SRING 135 (SMERIDIAN ST.) 827.83 FT. SOUTH OF THE NORTHWEST CORNER OF SECTION 12, TI4N+R3E. THENCE IN A NORTHERLY DIRECTION A DISTANCE OF G2 22.45 FT. TO A POINT 37.5 FT. NORTH OF THE NORTH LINE OF SECTION 1, T.14N - R 3 E., ALL IN PERRY TOWNSHIP, MARION COUNTY.

> GROSS LENGTH:- 1.178 MI. NET LENGTH: 1.178 MI.

MAX. GRADE 1.80 %

STRUCTURE No. I
S.R. 135 OVER LITTLE BUCK CREEK
TO BE CONSTRUCTED UNDER THIS CONTRACT END OF "S" PROJ. NO. 330 (G)
SEC 35 T-15 N. R.3-E. STA. 180 +34 P.R. Line # 2
BEGINNING OF PAVING EXCEPTION STA. 130 + 40.92 TO STA. 131 + 89.08
BRIDGE FILE NO. 135-P-5404 SEC.2 T-14-N. R-3-E. SEC. 11 T-14-N R-3-E. U.S. PROJ. NO. 330 (1) TOWNSHIP PERRY \_\_\_\_\_ Locwood Drive END OF U.S. PROJ. NO. 330 (1) STA. 36 116 P.R. Line #3 Brill Rd. SEC. I T.-14-N. R-3-E. SEC. 12 T-14-N. R-3-E. SEC.36 T-15-N. R-3-E MARION COUNTY

RECOMMENDED FOR APPROVAL 7-25-64 C. J. Klingelheefer
ENG NEED OF PLANS AND REGIFICAY ONS

						L10-6	6-01			R LAND ACQUISIT				
						PARCE NUMBE		CENTER FRO	M TO OX APPROX	PLAN BRIDGE		EXISTING OF		RESIDUE BLDG AREA
						1	PARCEL 1 ON PROJECT	STA S-330(6)	<ul><li>STA</li><li>AND PARC</li></ul>	EL ON PROJE	CT S-330(		E ACQUIRED HE SAME LAND	• WITH
						1	ACQUISITION THEREOF EN	TIRELY UNDER	SAID PROJ	ECT S-330(6)				
						1	BARRON, ROD ET UX.	PR1 11	6 118	6+13	27,675SF	4. (*) ** <b>F</b> \$	4,982SF	A= 22,693SF
						2	MCKENZIE, THOMAS ET UX.	PR1 11	8 121	6+ 7+13	44,000SF	FS	6,253SF	A= 37,747SF
						3	CHRIST PRESBY CHURCH	PR1 11	8 122	6+ 7+13	4•252AC	0.459AC FS	0.172AC	A= 2.933AC
					Aer. 8-22-66 A.J. Nickson	3A √ 3B		PR1 12 PR1 12 • PR1 12	1 123	7+13		FS	0.122AC	
					REV. 4-28-66 T.MARON		MCCLANAHAN REVEL ET UX	• PR1 12	3 <del>≥</del> 126	7+14	2.027AC	0.506AC 0.621AC FS	0.157AC	A= 1.364AC
		<u>akkang pilangan dan perunggan dan dan perunggan dan dan dan perunggan dan dan perunggan dan dan dan dan dan da</u> Mangganggan				5	PAVEY + LOUIS E. ET UX.	PR1 12	1 123	7+13	28,000SF	FS	4,438SF	A= 23,562SF
						6	NICHELSON, RONALD ET UX	• PR1 12	2 124	7	22,000SF	F\$	3,300SF	A= 18,700SF
						7	WASSON, DAVID ET UX.	PR1 12	3 125	7+14	22,000SF	F\$	3,300SF	A= 18,700SF
						8	RILEY JAMES A. ET UX.	PR1 12	4 126	7+14	28,000SF	F\$	4,512SF	A= 23,4885F
					REV. 4-7-66, M.W.MYERS	9	ELDER, ROMENA OEHLER	PR1 12	7 129	7+14	49•593SF	F\$	5 10,731SF	A= 38,862SF
		사용하는 경우 등 경우 등 보고 있다. 경우는 경우는 경우는 경우 등 보고 있는 것이 되었다. 경우는 경우는 경우는 것이 되었다.				9A		PR1 12	7 129	7+14			13,998SF	estantina e de la compania de la co La compania de la co
						10	PRIZEVOITS, PETERIS ETUX	X PR1 12	9 130	7+14	30•135SF			A= 23,953SF
						10A		PR1 12	9 130	7+14		The state of the s		
						11	KING, LOUIS P. ET UX.	PR1 13	0 132	70 (1) (1) (1) (1) (1) (1) (1) (1) (1) (1)	32 • 702SF		The second second second second second	A= <del>24,815</del> SF
						11A		PR1 13				in the second of		
					REV. 82-66 A.J. NICKSON			PR1 13	그렇게 생활되다			F.S.		
					461.87-00 14.7.741 CWJUN	12	YEAGER.JOHN W. ET UX.	PR1 13		7+15	0•930AC			0:566 A= <del>0:61</del> 1AC
		선생님, 경우 보다 하는 것이 되는 것이 되는 것이 되었다. 생각을 들어 있는 것이 되는 것이 되는 것이 되는 것이 되었다. 생각을 받았다.			Rev. 9-1-66							FS		A- VOITAC
		지원하다 등 마시스 이 이 이 사람들이 되었다. 발표하다 하다 하는 것이 되었다. 나를 보고 있는 것이 되었다.			REV. 8-2-66 A.J. NICHJON	12A 12B 13	SPEICHER, REGINALD ET AL	PRI 133		7+15 7 7+14+15	17•696AC	1.973AC FS	0.011AC	14.010
					REV: 5-23-66 A.I.NICKON		or archemy regiment of Archemy				11.4030MC	1.973AC FS		A= <del>15.199</del> AC
		사용하는 100 mm = 1			יוטוטוטואיניני או שם - ב.ב ב ב			PR1 12		7+14+15			शंक्षांत व हैं है की	
						14	LAUCK, MARIE T.  MEVEY, RAYMOND F. ET UX.	PR1 13		7+15	405,221SF	가 되고 10 시원 (# <b>FS</b> 사용 10 시설 10 시원		A=400•252SF
					Rev. 6-6-66 Hirschy, G.L	. 15				7+ 8+15	0.517AC	0.103AC FS		A= 0.299AC
						1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1	BIXLER,GEO. M. ET AL.	PR1 13	5 140	8+15	833,990SF	7. 11 D. 1911 <b>FS</b>	42,509SF	A=455,479SF B=336,002SF
		요하다 보다는 사람들은 보는 사람이 있는 사람이라고 함께 보다. 화해 되었다. 그는 사람이 보는 사람들이 가는 것이다.			ELIMINATED 5/26/66 W.S.	Hyde <del>16</del> A-	가입하는 것이 되었다는 것을 했다. 1965년 대한 1일	<del>-PR113</del>	9 <del>140</del> -	<del>-8+1</del> 5		-sr	EASMNT RTS	
		두 15일 : 10일 : 10g				17	EGOLD DEDWARD C. ET UX.	PR1 13	5 136	8+15	0.683AC	0.107AC FS	0.028AC	A= 0.548AC
					REV. 9-9-1966 AJ. NII	WJON 18	DARKO, LOUIS L. ETAL.	PR1 14	0 154	8+ 9+16	10.031 27.330AC	0.624 0.767AC FS	4.720AC	A= 0.139AC
								$X_{ij}$						B= 4.548AC <del>C= 17.156</del> AC
					는 기술 등 시간 전 경험 등 있었다. 2007년 - 기가 기가 기가 있었다.	19	JONES,SYLVESTER ET UX.	B 15	4 156	9+17	1.000AC	0•038AC FS		A= 0.905AC
						19A		В 16		9		FS		
						20		B 15		9+17	1.000AC	0•038AC FS		
					REV. 0-31-66 M.W.MYERS	21	LUTES, MARGARET ET AL.		4 161	9+17	1.000AC	0.038AC FS		
						22	OSWALD. HULDAH			9+17		0.038AC FS		<b>X</b>
					Rev. 8-25-66 B. J. NICRSO	7/V 22A 23	ROUND HILL CEMETERY	85B B 16	7 8	9	13.238AC	0.038AC FS 0.874AC FS	0.012 Ac	
					REV. 10-6-66 M.W.MYERS		CORDES, HERMAN D. ET UX.			9+10				A= 12.203AC
					WELLIN O EN MINENALLEY	<b>-</b>	TORDESTILING DO LET UNA	10			46.260AC	2•140AC FS		A= 29.039AC B= 8.512AC
						24 A		PRTRD 1	12	18		FS	0.019AC	
						25 25	PARCEL 25 ON PROJECT	S-330(6)	AND PARCI	EL 1 ON PROJEC	CT US-330(	COVER TH	E SAME LAND	, WITH
					DEVIA IA COME HA	25	ACQUISITION THEREOF ENT				C4.960AC	<b>→0.</b> 678	C1.444 1.456AC	<b>~2.838</b>
					REV. 10-10-66 W. E. Hyde	. 25	National Control of the Control of t	PR2 178	3 182	10+11	<u> </u>	<del>`Õ∙878</del> AC FS		2,838 A= <del>2,91</del> 8AC -B
						26	JONES PERNEST E.	PRTRD	3	11+18	1.170AC	0.234AC FS	0.049AC	A= 0.887AC
					REV. 3-10-GG, M.W. MYERS		MARIANNE ET AL. CARSON, THOMAS ET AL.	PRTRD 1	1 12		1.000AC	0.064AC FS		A= 0.933AC
						28	PARCEL 28 ON PROJECT	S-330(6)	AND PARCE	EL ON PROJEC			E SAME LAND	
						28	ACQUISITION THEREOF ENT	TRELY UNDER	SAID PROJ	ECT S-330(6)				
					REV. 10-26GG, M.W.MYERS	28	CRAIG, MADGE R. ET AL.	PR1 11	5 119	<b>6</b>	8.105AC	0.309AC FS	0.155AC	A= 7.641AC
						29	HUMBLE OIL + REFINING C	.08 152	2 154	9. 11. 1	1.300AC	0.175AC FS		A= 0.767AC
					REV. 1-26-66 WE.H	4da_ 30	BUCHANAN, WM. A. ET UX	PR1 135	5 140	7. In 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 1987, 19	42,041 833,990SF	FS	7,091 <del>42,509</del> SF	34,950 A=455,479SF B=336,002SF
		respectation of the first of the second of t			eliminated 5/26/66 weifq	do 201	MERIANDUNY DI DVERNI	<b>, pp1</b> _1.27	<u>. 135</u>				-EASMNT-RTS	<del>- 330 ) 00 E 31 -</del>
				REV.7-18GG, M.W.MYERS	REV. 4-28-66 TMARO	NEY 31	MCCLAWAHAN, R.+ DYER, V. DYER, VERNON ET UX.  LIST OF EXCESS			7-44	0.998AC	0.115AC FS		A= 0.826AC
REV 7-27-72 CHRISTY D. 6 Y	<u>' 1</u>	Mª CLANAHAN				A	ND A SEGREGATION BY PROJE	CTS OF RIGHT	OF WAY AF					
CHRISTY D.E. Y	e, and a second		ZN -			DADCE	LAND AREAS LYING				- <b>-</b>			
REV 9-21-73 Y CHRISTY D.E. Y		PARCEL Y 2 INVOLVES THE SAME O	PWNERSHIP AS THE ACQUISITION OF	PARCEL 3 ON THIS	PROJECT	PARCE NUMER		PROJECT S-330(6)	PROJE S-330(4					
		CROOKS, CALVIN E. ET AL.	<b>IN</b>			1	FS 4•982SF	253SF	4,729	9SF				
						-25	ES1.456AG -	1+044AC		0.4124	<del>\</del> €			
	st a fil			(2) からないないのでは、				And the second second	- · · · · · · · · · · · · · · · · · · ·		gradient gewant	Start Control of the Control of the Control		

L.A. CODE 0530 DATE 01/25/66

IND. 5-330(6) 1964 2

\* (ASTERISK) IN THE BRIDGE COLUMN INDICATES THE PARCEL IS PARTIALLY OR COMPLETELY WITHIN THE LIMITS OF A BRIDGE PROJECT.

0.126AC

28 FS

ROAD SR135 COUNTY-MARION

0.155AC 0.029AC

<sup>\* (</sup>ASTERISK) IN THE BLDG. COLUMN INDICATES A BUILDING IS PARTIALLY OR COMPLETELY WITHIN THE LIMITS OF THE R/W REQUIRED.

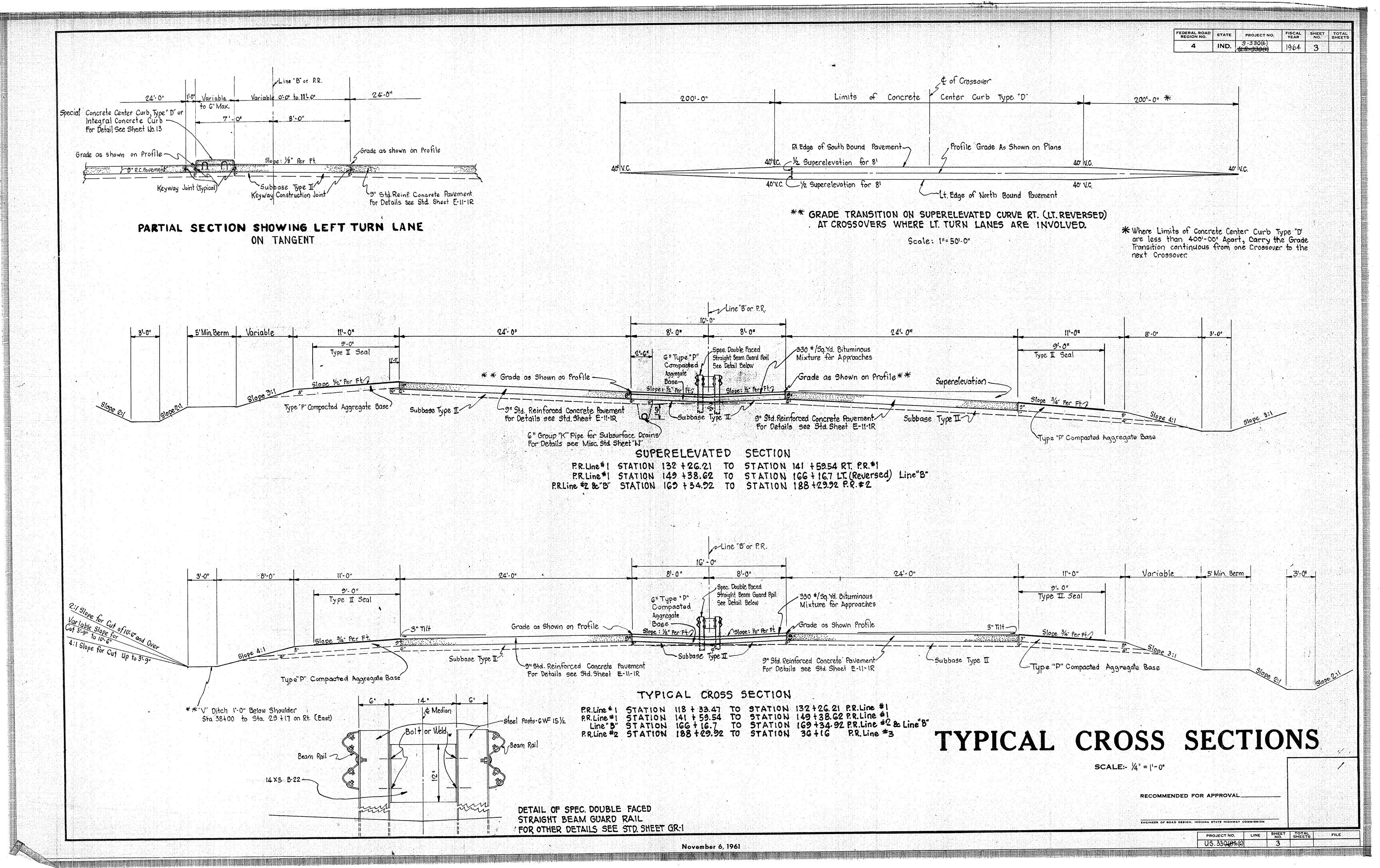
ACCESS RTS = ONLY ABUTTERS ACCESS RIGHTS TO BE ACQUIRED, LEAVING RESIDUE LANDLOCKED

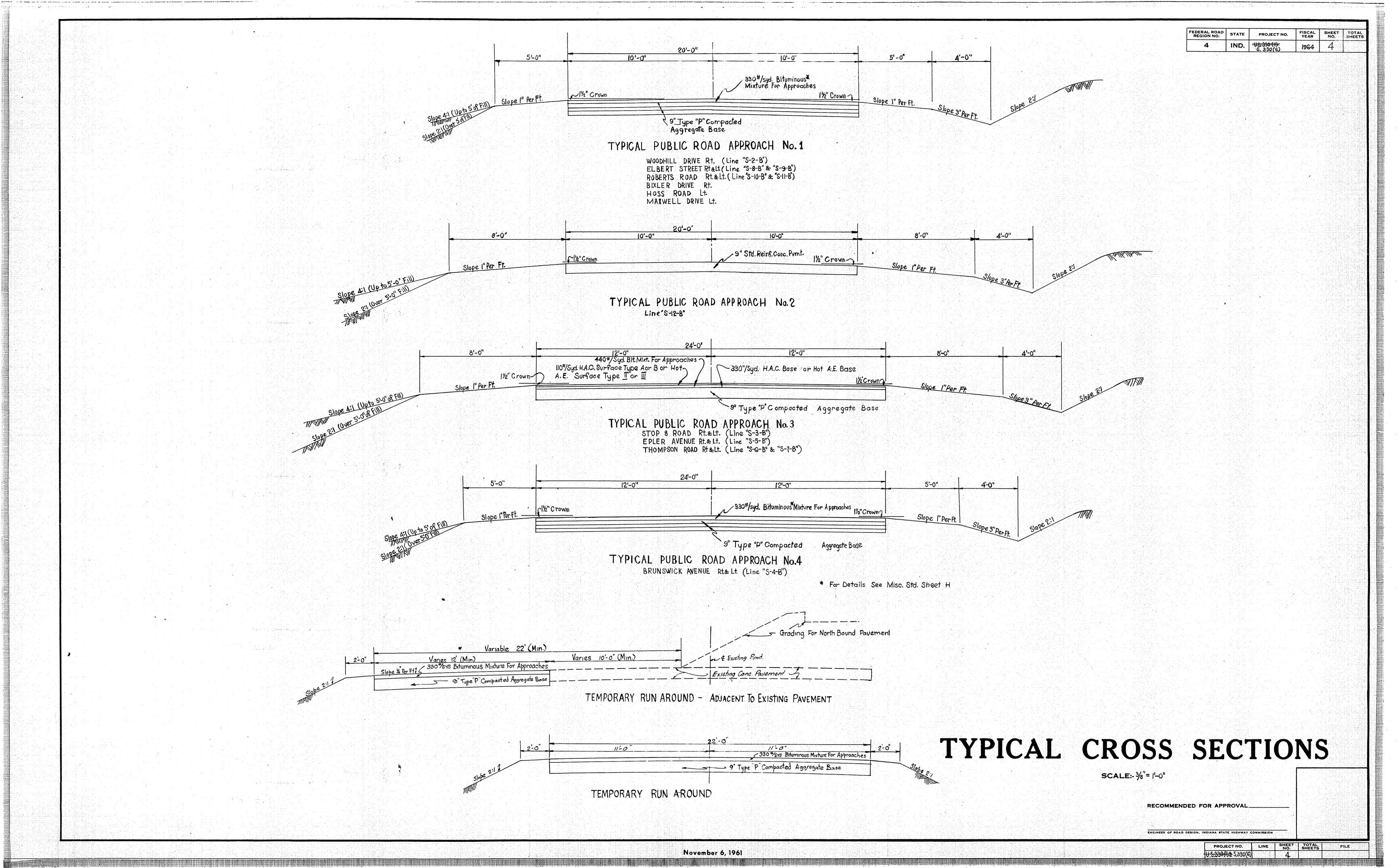
EASMNT RTS = CLEARANCE OF PRIVATE EASEMENT HELD BY THE OWNER ACROSS THE LAND OF A NEIGHBORING OWNER CLEAR RESV = CLEARANCE OF RESERVATION IN OLD GRANT OF EASEMENT MINERALS = ACQUISITION OF MINERAL RIGHTS FROM OTHER THAN THE FEE OWNER OF THE LAND

FS = FEE SIMPLE TITLE TE = TEMPORARY R/W

TB = TEMPORARY R/W FOR BUILDING REMOVAL ONLY

PV = PROVISIONAL R/W
SP = QC DEED, R/W GRANT, OR OTHER SPECIAL INSTRUMENT FOR CLEARING ENCUMBRANCES
IN = INVERSE CONDEMNATION





FEDERAL ROAD STATE PROJECT NO. FISCAL SHEET TOTAL SHEETS

4 IND. US. 830(1) 1965 5

REV. & ADDED PROPOWNERS 1-25-DE FER POAD PESISN DEPT.

335 McClanghan, Trevel Christ's Cumberland DE T.14N-R3E.

Presbyterian Church Die W.B. & M.R. Craig SECTION 2 TIAN-R3E. John E. Craig of C.M. Rish R. J. Speicher N. & S. Section Line N&S. Section Line-7 Ine B

214

205'

Rod & Mary Log

Rod & Mary Log

Barron

Thomas Mekmey

Pavey Micrelson Wasson

Fraig

All

Riley chairs

Riley 45 Line"5-12-8" Frank L. Bixler Solver Frank L. Bixler 217.88 423.64 George Burkhart n.P.R. Line to be Constructed Line BN SECTION I TIAN.-R.3E. SECTION 12 T.14 N. - R.3E. George Burkhart 74.76 Ac. Herman Cordes CRAIGWOOD 200 SEC. CRAIGWOOD ADDITION FERGUSON ADDITION SOUTHERN HILLS 49.51 Ac. 11.07 Ac. 1 Ac. James Jones GG2' 1Ac. Lutes 3

GG2' 1Ac. Lutes 3

GG2' 1Ac. Lutes 3

GG2' 1Ac. Hoswald 3

GG2' 1Ac. Hoswald 3

GG2' 1Ac. Hoswald 3 Herman Cordes 678.85' G53.12" MERIDIAN HIGHLANDS & HOSS' SOUTH MERIDIAN HIGHLANDS 151.6 To Sec Corner P.R. Line to be Constructed SECTION 35 T. 15 N. R.3E. SECTION 2 T.14N- R3E. Ernest Jones N.& S. Section Line 1 Round Hill Cemetery Herman Cordes P.R. Line to be Constructed Herman Cordes 48.47 Ac. SECTION 36 T.15 N.- R3E. SECTION 1'
T.MN: R3E. MERIDIAN Thomas Carson etal.

PLAT 2 FOR R/W DEPARTMENT

DE AILS

Scale: 1"= 200"

UNTB 9-23-64

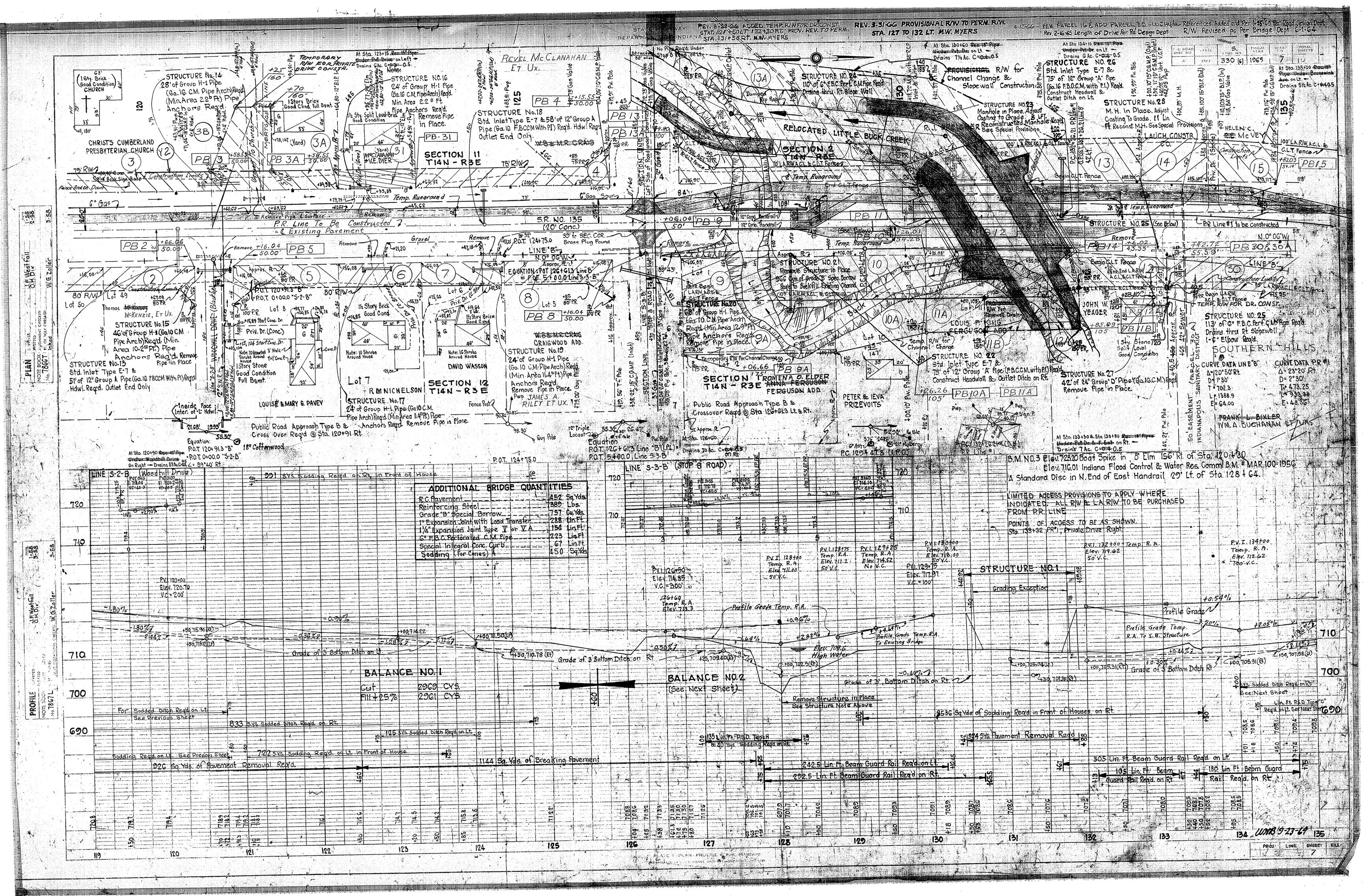
November 6, 1961

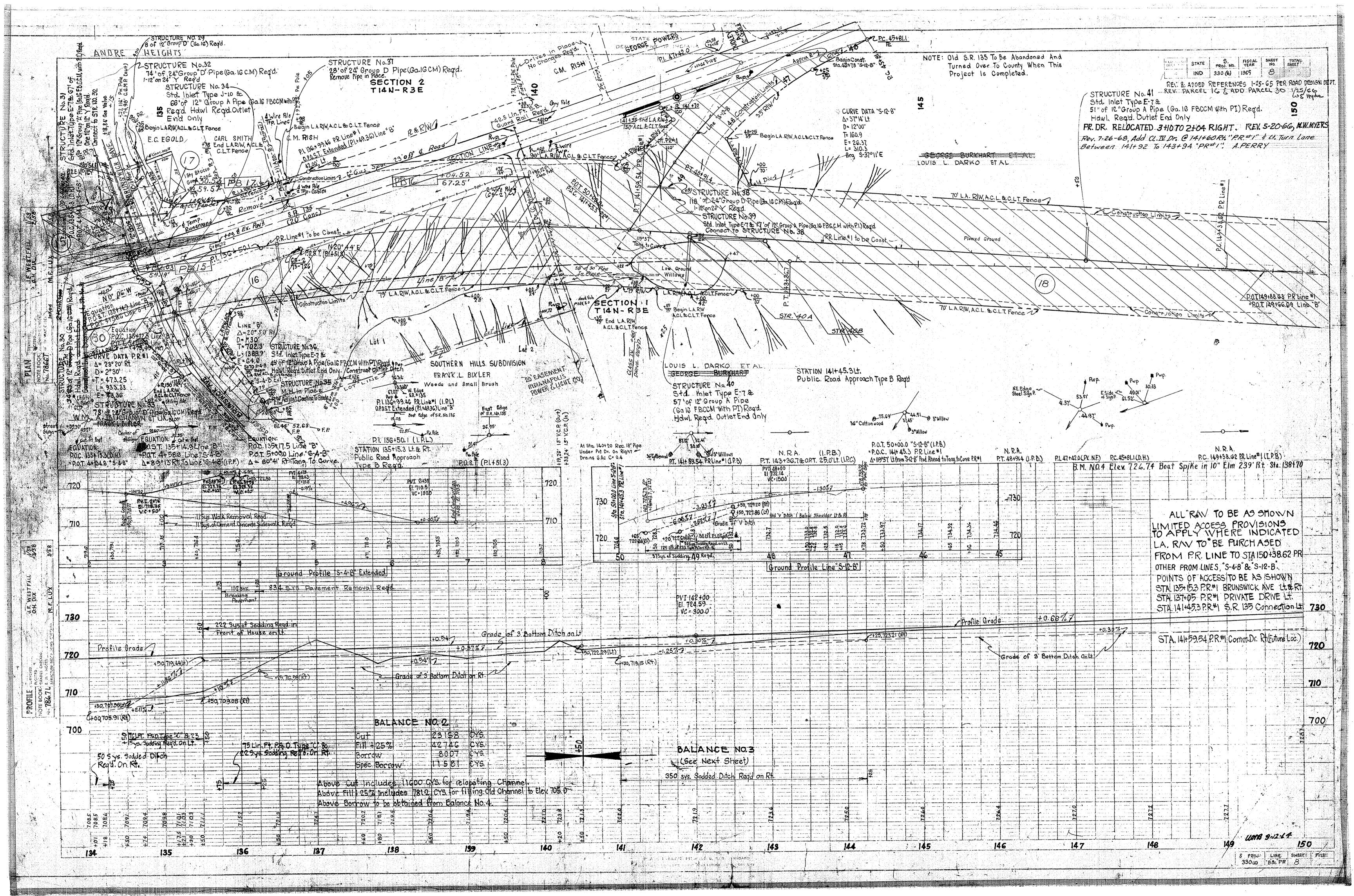
305.06'

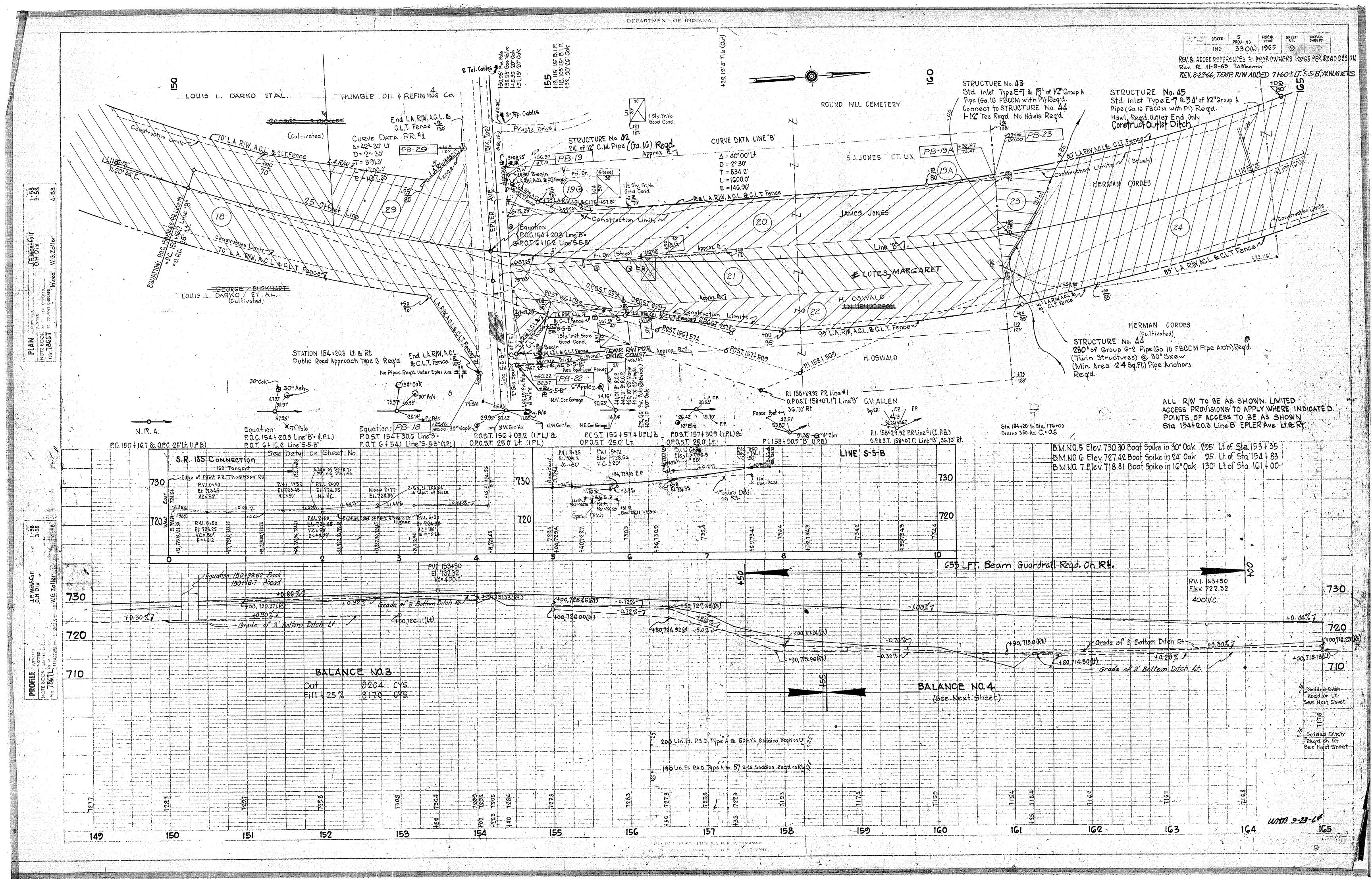
PROJECT NO LINE SHEET TOTAL NO. SHEETS

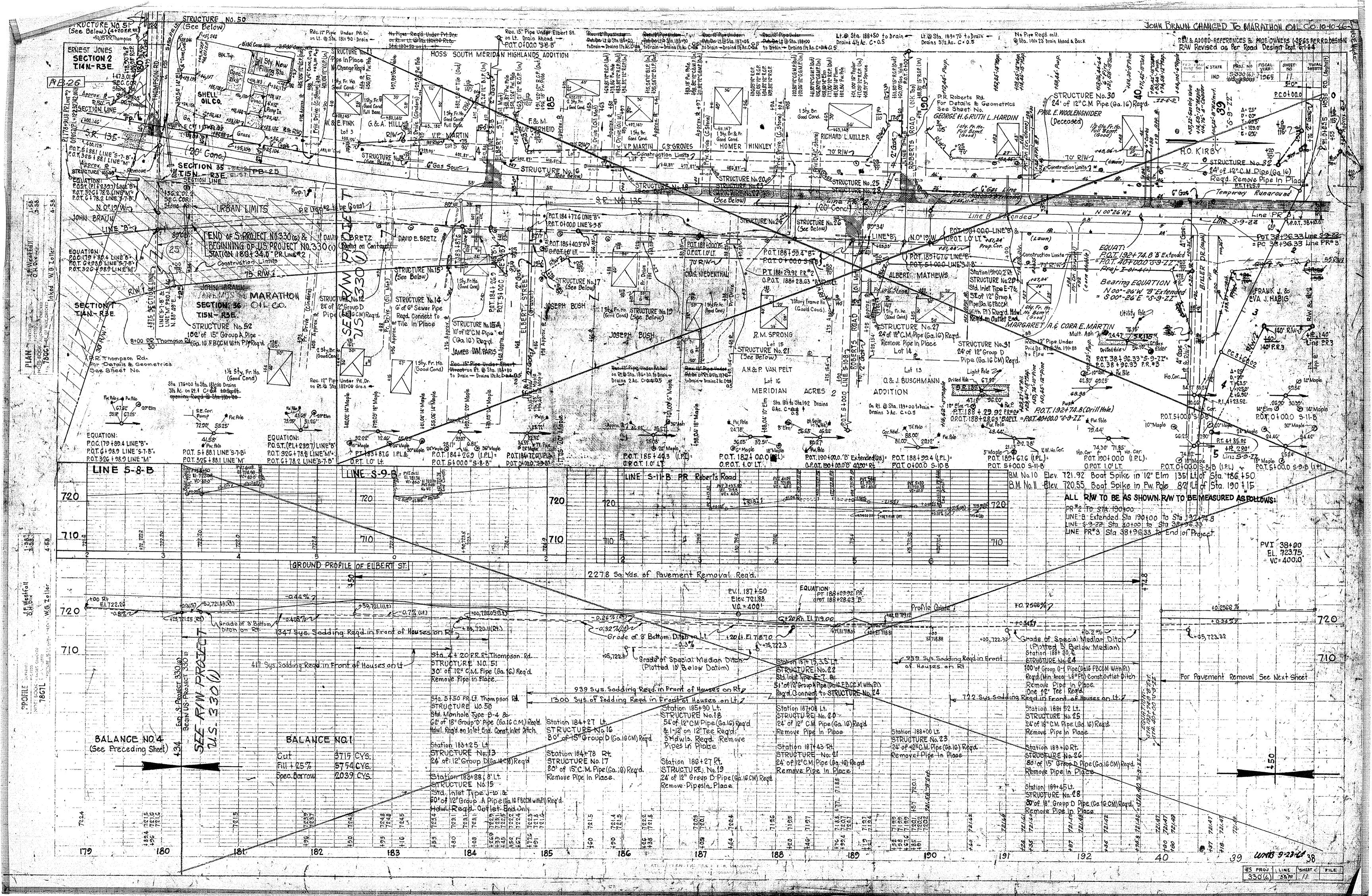
U.S. 330(G)

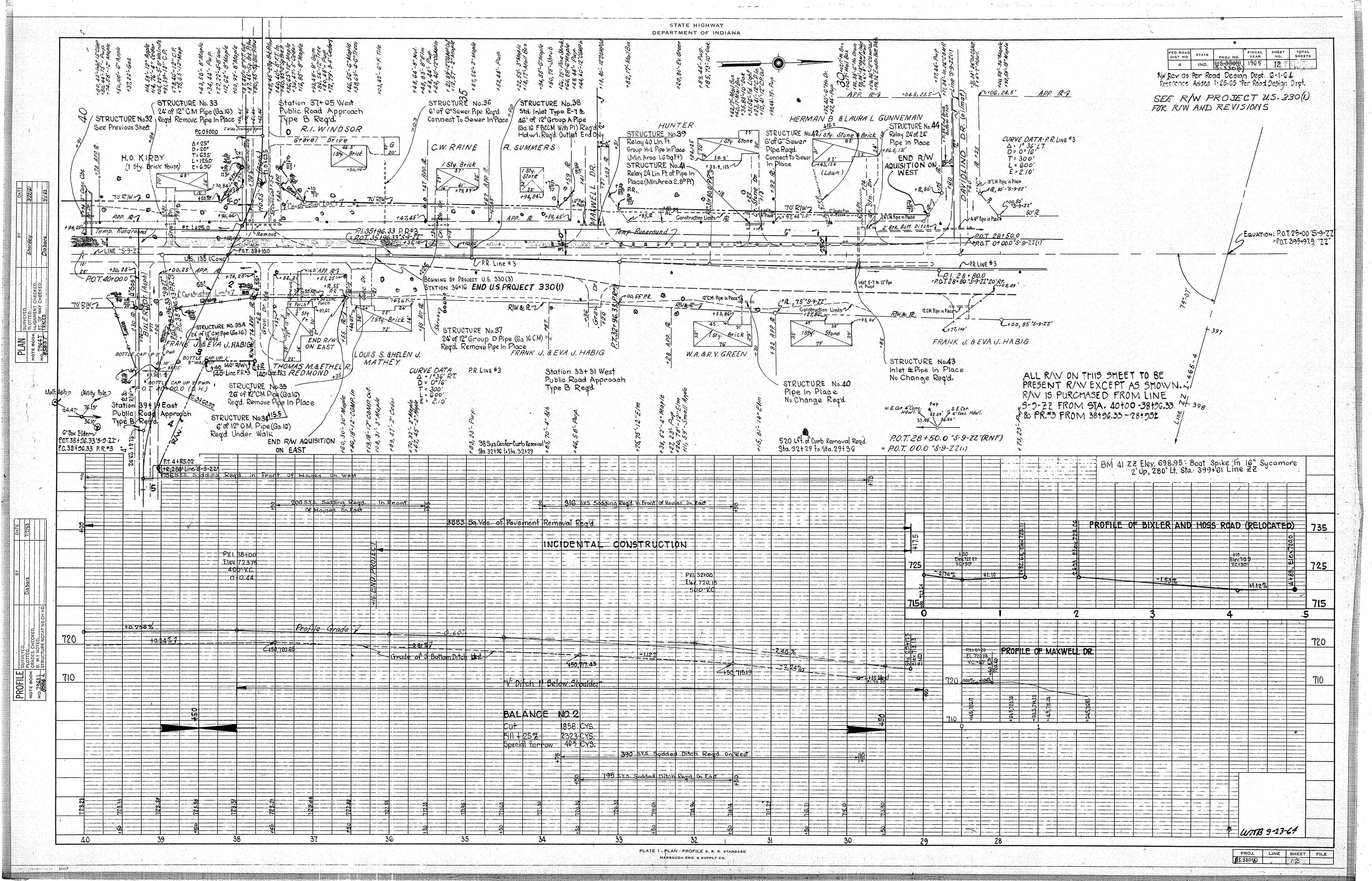
5-330(G)

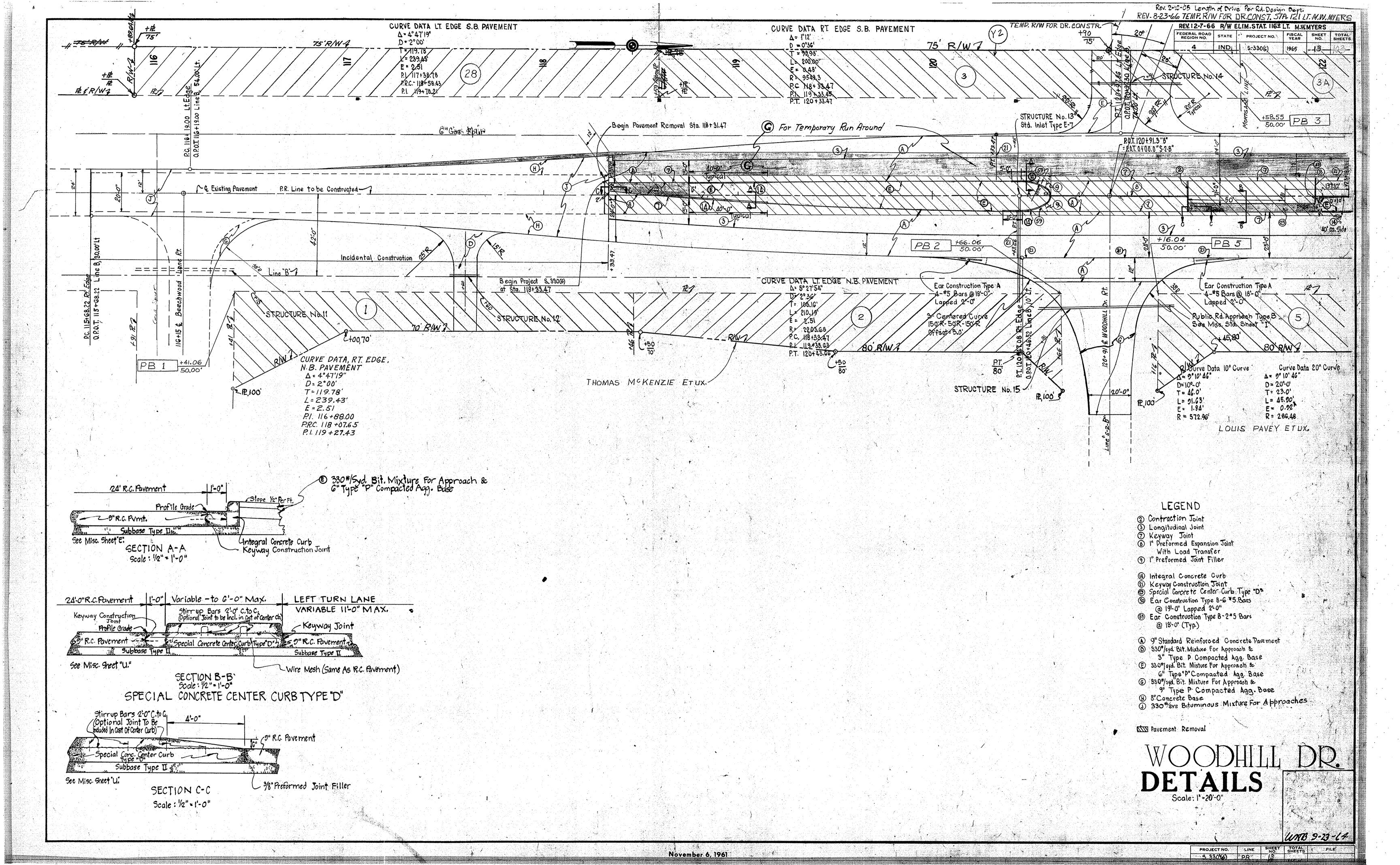


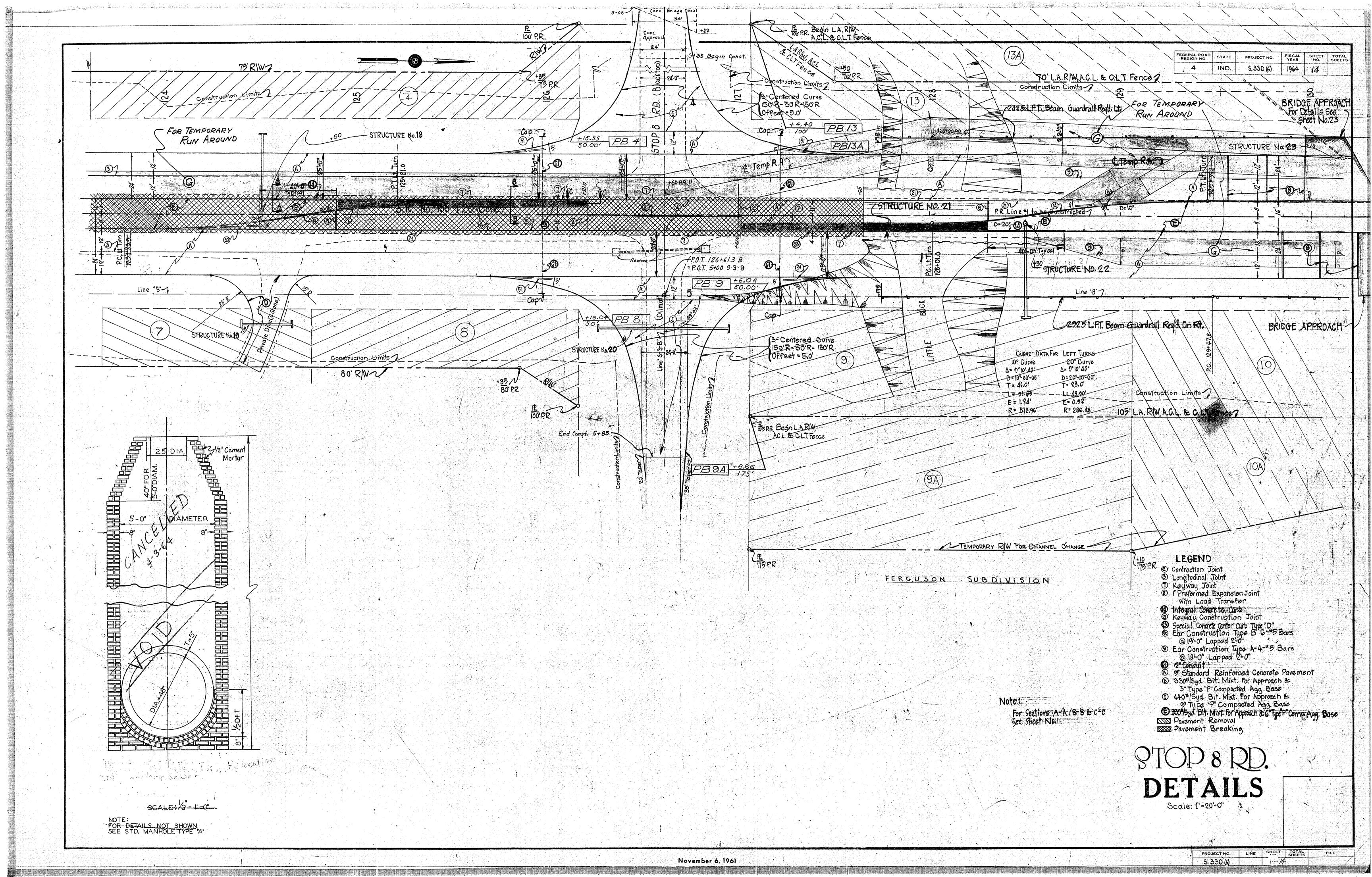


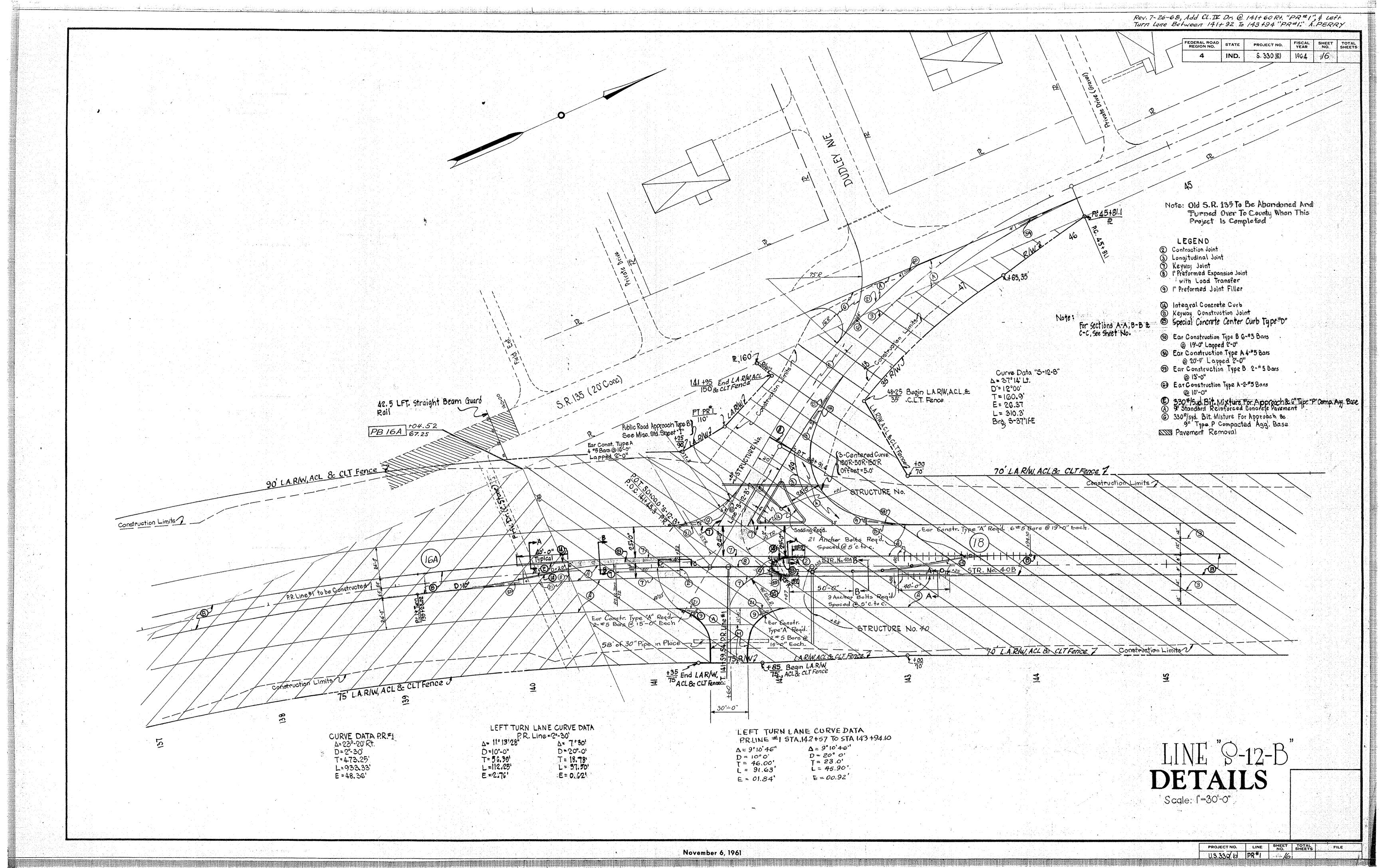


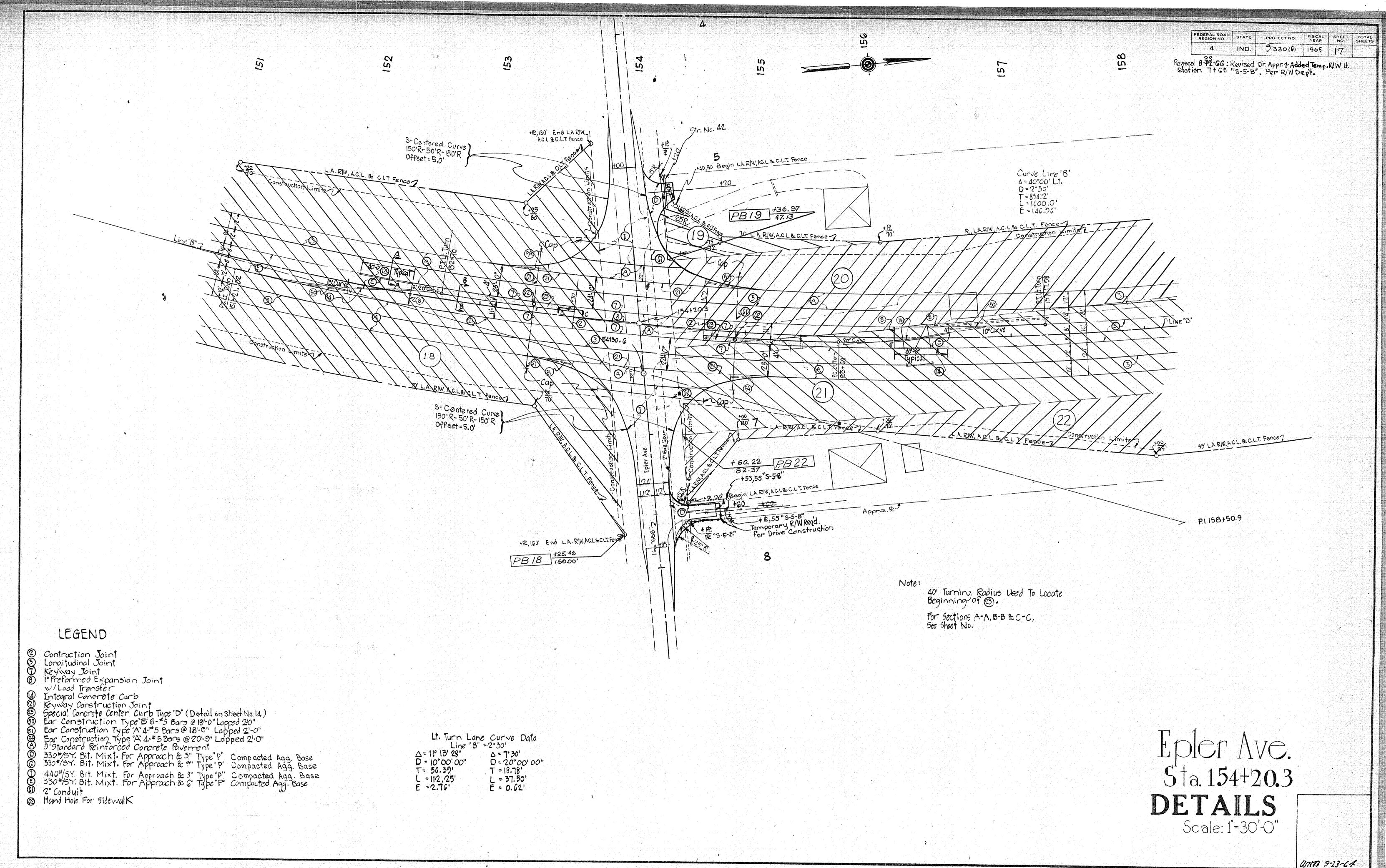






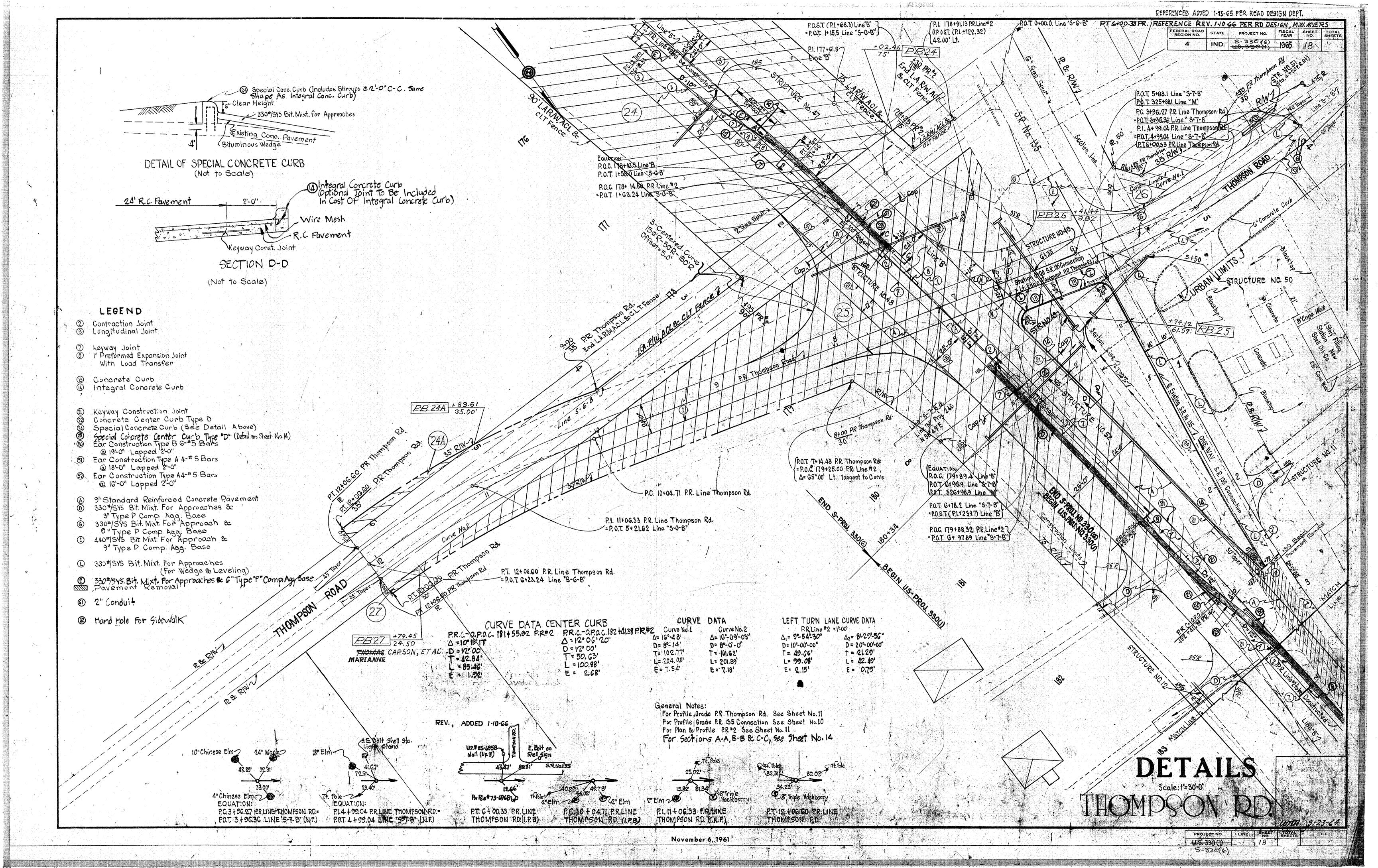


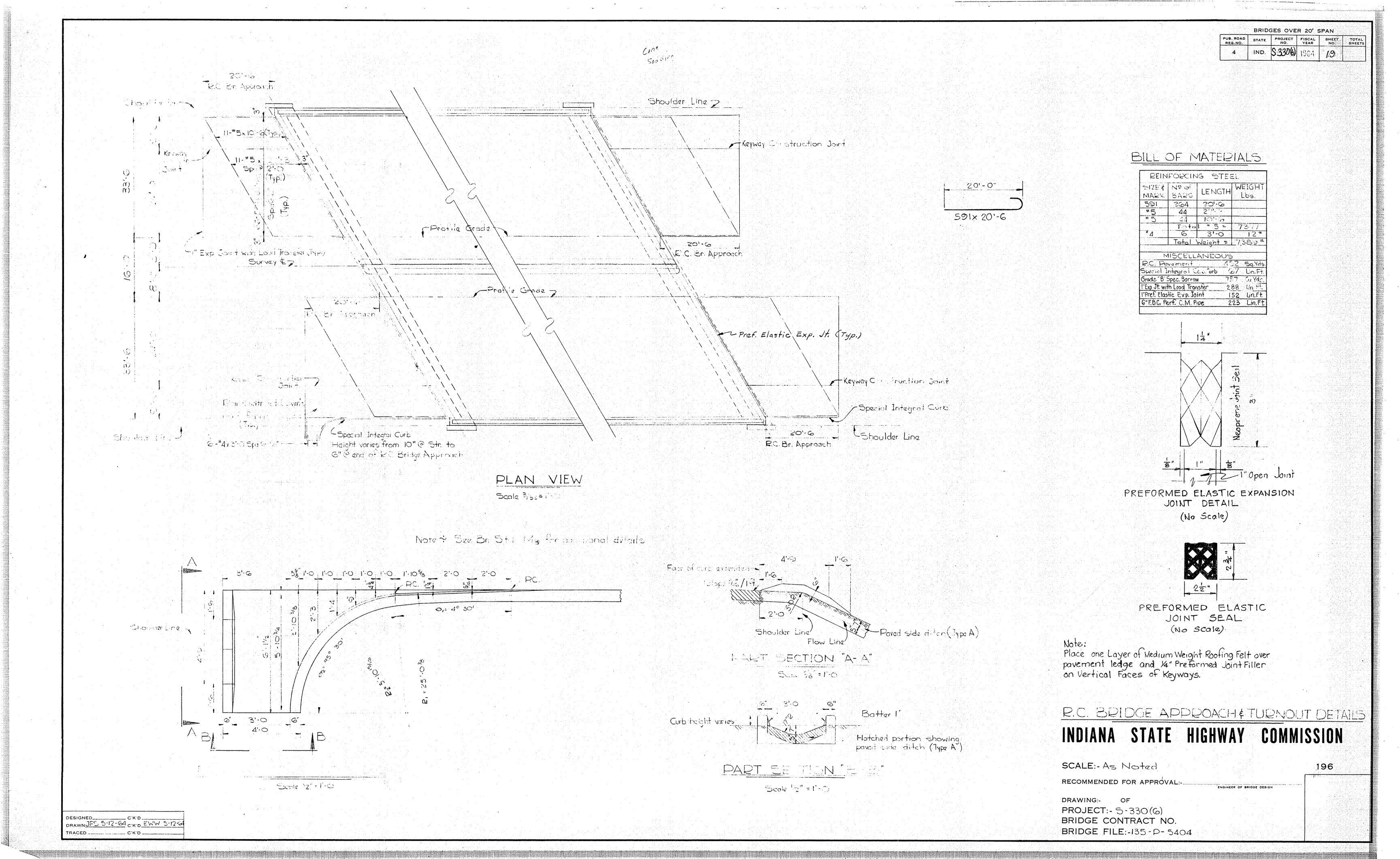




SPROJECT NO. LINE SHEET TOTAL NO. SHEETS

November 6, 1961





DRIVE RELOCATED PER R/W DEPT. 5-20-66 & STRUCTURE NO. 30

Rev. 2-16-65 Length of Drive Per Rd. Design Dept.

					* 5l	BSURFACE D	RAIN	15				orive fer kaluesign dept.
SUBSURFACE DRAINS OUTLET												
LINE	FROM	Το	LANE	GROUP K" PIPE	STATION				G" B.C.C.M PIPE	AGGREGATE FOR SUBSURFACE DRAINS	GUIDE POSTS TYPE	REMARKS
	132 +00	143 470	South bound	LFT.	132+05		1		LFT.	75.	EACH	
	147 +50 152 +95 168 +60	152 + 95 167 + 75	Northbound Northbound Southbound	572. 1485	147 + 50 164 + 28 168 + 60		1			37 95 76		Connect to Structure No.26.  Connect to Structure No.41  Connect to Structure No.45.  Connect to Structure No.46.
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DETAILS

APPROACH TABLE

	APPROACH TABLE  DESIGN DATA & QUANTITIES BASED ON MAX. OF 10% GRADE EXCEPT AS NOTED + 440 this is a second of the																															
	LOCATION	DESCRIPTION		EXCAVATIONS	WIDTH	RADII	GRADE LESS THAN 10% NOT SHOWN 1		* DISTANCE BEYOND Z * R/W LINE	330#BITUMINOUS T	APPROACHES R		MPAC	S BAY			(TE	oncresc	3" Rinforced Concrete & Pavement		Integral Concrete X Curb	Special Concrete Curb	Special Concrete Center Gurb. Type "D"	l'Reformed Filler	Permanda/Load Tr	ss. face	ACHES SAS #	Signatural Material	201 235601 201 201 201 201 201 201 201 201 201 2	Concrete Curb		
			CYS.	CYS		LFT		LFT	LFT	<b>5</b> 45.	Tons	5Y5.	TONS	5YS.	tons:	5YS.	TONS.	5YS.	SYS.	LBS.	LFT	LFT.	5YS.	LFT.	LFT.	<u>575.</u>	SYS	5Y3.	5Y5.	LFT		
	W. L.						1.000	<i>\$</i> 1																	-	.,						
) 2.		Incidental Const.							;		150.5							327			80		28					912				
	117160 Rt.	Class II	8	35	12'	25'&15'		52		90	14.9	90	15																90			-
	(20†91.3 (20†91.3 Rt. Woodhill Dr	Cross-Over			16'	w (6.20)		70'		0.40	30.0								125	54	55				70			-	0.40		- 1	-
	20+31. Lt. (20°5kew)	Class IV	11	49	20'	*&38' 20&20	123'-	80'	85'		39.9	107	2050	#5		242	121		140	150									242			<b>.</b>
ľ	COUNTY (LO JREW)	C(405) 1.V		+3	120	wew	الدمية ا	- HO.	05	1 <del>115</del> 295		40.1	2754	113	38.3														1913			
l												3344																		. •		
[	121131-123+24.12	Lt. Turn Lane			111			193.12											156	119	80		40		10				1.0		. '	
	122+85 Lt.	Class II	5	20	12'	25& 15'		43'		78	12.9	78	13							,				·					78			
1	123108 Rt.	Class II	4	50	12'	25&15		48'		85	14.0	78 85	14.1																85			
·	24+56 Rt.	Class II	5	40	12	25'&15'		48'		85	14.0	85	14.1						***	91.04			2.5		10				85		<u> </u>	
-	124+27.88 -126+21	Lt. Turn Lane		<u> </u>	11'			243.121		*			-			****	<u>villa i</u>		7248	119.	80		63		10							
ŀ				1	"			,									7 10 10 10															
†	26+613	Crossover	<u> </u>		16.			80'					. ,			· ·			143						80							
	26+ C1.3 Rt. Stop 8 Road	**************************************	58	122	24'	* & 38'	2 - 2 - 2 - 2	25'								344	172		165	150						344	344		344			1
	26+61.3 Lt. Stop8 Road		59		24'	38'8 *	yer M	91'								327	163.5		165	150						[	327		327		1	
V	127+01 - 128+94	Lt. Turn. Lane	1.6		11		1000												248	119	80		63		10		,					
- 1	BRIDGE EXCEPTION			A three parts				1974 E						:												٠						
	STA.130 +52 - STA.131 +78.					\$ 6 AV	61 (1) (1) 25 (2) (2)	300						•																-		
	23420 (0)	<u> </u>			10	me'n .E1		001		•	,																					<u> </u>
-	1 2 1 3 1 5 1 5 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Closs II	4	156	12	25'&15'		99'	36'	152	25.1	452	25.3		` '				1		0.0			,	16				152			-
	32+74:00-134+80.5 135+15.3	Lt. Turn Lone			16'			205.6											156	119	80	6	40		10			-				-
- 1-		Cross over BApproach S-4-B	1122	10/2		* 837.2	i maning V	1218		614	101.3				·	614	2 4 7	5	123	142					හ				614			-
		BApproach 5-4-B						1734		454	74.9		•			454			143	150									454			
1	2+04 Rt S-4-B	Class II		<b> </b>	12	25,15		124	111-1	185.2		185.2				754			140	1,70								_	185.2			
	3+10-R+"S-4-B"	Class II:	<del>- 3-</del>	170-	±6;	25'a13'		-005	-65	178	29.4		- <del>29.6</del>															<del></del>	178			
•	3+40 Lt. "S-4-B"	Class/II	3	29	12'	25'&15'		29'	9'	72	11.9	72	12.				<b>.</b>												72			
	135 t 40.5 - 137 t 55.1	Lt. Turn Lane			11"	**		205.6									. v . i.i.		156	119	80		40		10							
100		Class II	వ	20	12'	25'&15'		52		90	14.9	90	15																90		•	ļ.
	139+16.4-141+22	Lt. Turn Lane			11'			205.6											156	119	80		43		10							<u> </u>
	41+45.3	Crossover			16'			71'										· · · · · · · · · · · · · · · · · · ·	127	54	55		ļ	8_	71							+
-	141+60 Rt	Class IV Lt. Turn Lane		<b>_</b>	30	30-30		43	<u></u>	107					·	107			80 258	63	80		40	10	10				107			30
		B Approxh	1156	88	20'	7758*		285'		55	9.1			-		- B.G.	27.5		<del> </del>	435			40	20	15 20	<u> </u>			55			130
- 1		Lt. Turn Lane	1150	00	111	11300		255.6	1	37	3.1			*		77	21.5		248		80		63	20	10							1
-		Crossover		<u> </u>	16'			991		<u> </u>									176		00			ļ	99		11.1					1
	154+20.3 Rt. Epler Ave.	<del></del>	74	194	· • · · · · · · · · · · · · · · · · · ·	*\&37\		137'							:	470	235		170	162						470	470		470		: .	
1	154+20-3 Lt. Epler Ave.			0	<del></del>	47.5'&米'		150'									1765		170	162				Å.			353		35 <b>3</b>			
1																																
-				*			<b></b>	¥		<u> </u>	1.17														<u> </u>				. * .	<u> </u>		
1				ļ <u>.</u>		n-10 171			<b></b>		2.0							- 1					<b> </b>				8.116.416.41		60	<b> </b>		-
-	<del></del>	Class II	14	5	12'	25'&15'		36'	5' 23'	60 70	9.9	60 70	11.6					***************************************			<u> </u>	<b> </b>	1	-				1	70			-
	- <del></del>	Class II Lt. Turn Lane	4	3	12'	25'&15'	<b> </b>	255.6°		10	11.0		11.10						248	110	80	1	68		10			+	, ,		·	1
ŀ		Lt. Turn Lane			111		<u> </u>	247.16	<del> </del>		<u> </u>								235	119	80		65	<u> </u>	10	<b> </b>				<u>.</u>		
t	1 16 1 60.04 110 160							471.19					·			,				11.7									<del></del> !		<u> </u>	
f		State 1			2.11																		1									
	79 + 25.0	Crossover	ye et		16'			118'											210						118							
	79 + 25.0 Rt. P.R. Thompson		293	357	24'	18:35.5		457'								1402			192	138				<u> </u>	<u> </u>		1402		1402			1
	179+25.0 Lt. P.R. Thompson		200	725	<del></del>	* 8:355	1	2831	<u> </u>	1211	2228			323		470	394		109	<u> </u>				<u> </u>		470	470	888	793			<u> </u>
-		Lt. Turn Lane	, , , , , , , , , , , , , , , , , , ,		11'		1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	50'			) 11 A 3		,, _				1		CI				22	<u> </u>	ļ		+	1	70			-
-		Class II	17	2	15,	15'8-25	1	37' 20'		70 54	11.6	70.	11.6		1.	*				<u> </u>		<u> </u>			-		-	-	70 54	<u> </u>		+
-	4+20 Rt. P.R. Thompson	Class II	4	3	12'	2585		4 - ZU:		<b>34</b>	8.9	54	9						<u> </u>	<u> </u>							+	1.	JT		<u> </u>	1
ł	13 Mail Box Approaches				1 // //					598	98.7	598	98				<u></u>		1.1		1	<u> </u>			<u>,                                    </u>				598			<del> </del>
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ŀ	otals			1/2/2/1	7.5 384( ). 10.000 4***		\$ 4 (5) 2 (1) (8)	3		5465		1689		<del>436</del> 618		413+	2.30	241	5111	2818	1024		535	28	617	2300	3360	11800	685	<u> </u>	ļ	+

\* 3-Centered Curve on Traffic Flow Radius · 150,50,150, Offset · 5 \*\* Right of Entry Required;

FEDERAL ROAD STATE 45 PROJECT NO. FISCAL SHEET TOTAL SHEETS

4 IND. 330(6) 1964 321

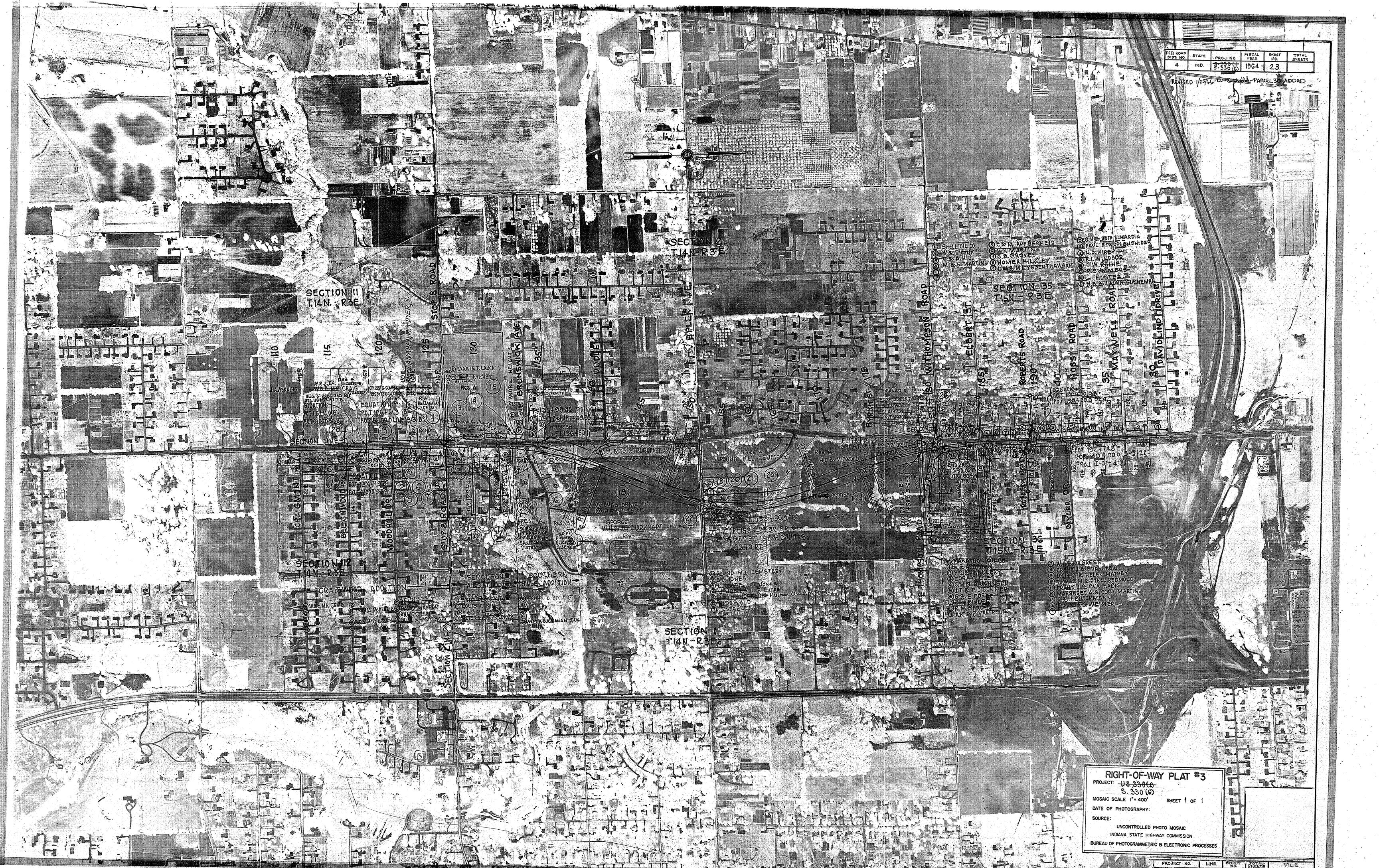
## APPROACH TABLE

		.0				D	E316		ATA	& Q	UAN	TITIE	53 BA	SED	ON P	MAX	OF 10			EXCE	PTAS	NOT	ED	tc.	Ŋ.	<i>,</i> 0%		4			
LOCATION	DESCRIPTION	EXCAVATIONS	<b>)</b>	NVIDTH	RADII	GRADE LESS THAN 10% NOT SHOWN	LENGTH	* DISTANCE BEYOND * RAV LINE	330* BITUMINOUS MIXTURE FOR	APPROACHES	, PL, BAS	ANT MO SE FO	MIX DISTUR	C-CON TE AG PPRO	ITROI GREG ACHI	lled ATE ES		Rinforced Concrete Pavement	Reinforcing Steel	the Type B	SX. H.	bete Gr	المالية	Sod IT	Private Drive Pavement	420 4/545. Bit: Mixture	For Approaches	Bifurninous Material	For Mime		
		CUTF	FILL			0-			1		3	y.	6	#	2	<b>J</b>	90,	<b>3</b>							Ω			0.10 Gal ( Ar sys. 1	3.5 Gall		
		CY5.C	YS.						SYS.	Tons	SYS.	Tons	SYS.	TONS	<u>375.</u>	TONS	SYS.	575.	LBS.	TONS	TONS	<u>575.</u>	LFT.	LFT.	5YS.	<i>5</i> Y5.	TONS	5Y3.	<u> 375  </u>		
S.R.*135 Connection			- 3	100	May 1		0.001										•							<u> </u>							
180+25-182+31.16	Lt. Turn Lane			121			206.16													-1920											
	*										<u> </u>																je sec				
181+20 Lt.	Class II			12'	25'&15'		26'											. v příme je													
181+98 Rt.	ClassIL				25'&15'	······································	43'			ļ			9		\$ 1 × 1																
181+93.84 -183+91	Lt. Turn Lane			15,	0010-101	<del></del>	197.16							₩.																	77.
182+05 Rt. 183+ 25 Lt.	Class II				25'&15' 25'&15'	1	75'	32'																							
183+91-185+1G	Crossover			12'	25 W 10		125																							<u> </u>	
184+28 Lt. Elbert St.	B Approach S-8-8				38′& <b>*</b>	•	281																								
184+78 Rt. Elbert St.	B Approach S9-B				* & 38'		GO'		200														***************************************								
And the state of t					5			`.									, v														
105110	-					-			<u> </u>	<u> </u>				# 25 M		<u></u>														Have Tolland	
185+16-187+13.16 185+58 Lt.	Class II			121	25'&15'		197.16' 38'	1. 1.			1		-																	<u>- 1</u>	
185+30 Lt.	Class II				25 & 5'	<del>                                     </del>	<i>⊃</i> 8¹ ∂8¹																								
186+27 Rt.	Closs E			12'	25'&5		381																	41.71.							
187+08 LT.	Closs I			121	25'&15'		381				6								1												
****											i																				
											ļ	17								<i>/</i>											
187+43 Rt.	Class II				25'&15'		38'										· · · · · · · · · · · · · · · · · · ·														
188+00 Lt.	ClassII			····	25'&15'	1	51'	131		<u> </u>	<u> </u>					1															
188+52 Lt. 186+84.13-188+80	Class II Lt. Turn Lane			12'	25 & 15		38' 197.16																						<u> </u>		
188+80-180+80	Crossover			16'			102,																								
																				N. C. C.											
		·						* 7																							
189+34.10 - PR Line	Robert's Rd. Lt.				67.5&*		108.18			<u> </u>																				<u></u>	
189+34.19 - P.R. Line	Robert's Rd. Rt.	3.4			* 8:38		113.35			<u> </u>									, W												
5t70Lt. P.R. Line Robert's				121	25'&15'		221																				<u> </u>				
182+82-121+82.12 121+23 Lt.	Lt. Turn Lone Class II				25'&15'		193.12' 38	ļ			<u> </u>																				
10 1 20 LI.	<u> </u>			12	236.0	-	30	<b> </b>								***************************************									A. W. A. S.						
191+88 Rt.	Class II	\		121	25°6 15'		381																								
39 to West	ClassII			12'	75'&15'		პ8'									1 1 1 1															
30+49-38+79	Crossover			16'			70'																		<u> </u>						
38+10 East Bixler Dr.	BApproach			0.000	*\&38	· • · · · · · · · · · · · · · · · · · ·	207			<u> </u>																	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1				*
38+84 West 3+10 Bixler Relocated U	Class II				25'&15'		38'	- 	1																						
38+79-37+36	Crossover & Lt. Turn			161	258-15		1431		· · · · ·																						
38+10 West Hoss Rd.	BApproach				38'&米'	1	132								<u></u>																
37+63 East	ClassII			12'	25'&15'		30'	7'											V 7												
36+35 East	Class II				25'& 15'		33'	10,																							<u> </u>
35+65.08-33+71	Lt. Turn Lane			121			124.08	1			1			<b> </b>		· ·						<u> </u>									
7-1-00 21/1-4	Clast			1.01	0510 15	,	381										<u> </u>								<del> </del>		<u> </u>			<u> </u>	
35+22 West 33+71 - 33+01	Class II Cross over			16'	25'&15'		70'		<del>                                     </del>		1		<u> </u>			<u> </u>				1			1	<u> </u>						<del></del>	
33t31 West Maxwell D					38'&*		40'													<u> </u>		1	:						y" e j		
33+28 East	Class II				25'&20'		38'	15'																							
36+16-29+17.5	Incidental Corst.					1	C98.5	· · · · · · · · · · · · · · · · · · ·																						<u> </u>	
31+30 East	ClassI	<u> </u>			25 <u>&amp;15</u>		46'	18,		ļ						<u> </u>								ļ	<u> </u>	ļ	-				
31 + 18 West	Class II			····	25'&15'	<del></del>	19'	11,			<u> </u>		ļ													<u> </u>				<del></del>	
29+ 68 West	Class II			12'	25 615		60'	55.			<del> </del>		<u> </u>		<u> </u>							<u> </u>	<u> </u>								
23 Mail Box Approaches								1	1058		1058		<u> </u>			1											<del> </del>		1058		
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					51	UBSURFACE COUTLET	PRAIN	15				
	LOCA	TION				OUTLET				, w		
LINE		ТО	LANE	PIPE		LOCATION			G" BC.C.M PIPE	AGGREGATE FOR TORSURFACE ORAINS	GUIDE POSTS TYPE	REMARKS
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DETAILS

\* 3-Centered Curve on Traffic Flow Radius · 150, 50, 150; Offset = 5'
\*\* Right of Entry Regd.



	"S" PROJECT NO. 330 (6)	STRUCT	URE DATA	*US* PROJECT NO. 330 (1)	rive Relocated Par R/W Dept. 5-20-66 FEDERAL ROAD STATE PROJECT NO. FISCAL SHEET TOTAL SHEETS STRUCTURE No. 30 Revised.  4 IND. 15:330 (6) 1965 226 103
DESCRIPTION HENCE THE SIZE SIZE	SKEW COVER UP STREAM GO STREAM GO STREAM GO SPECIAL BORROW GRADE"B" REINFORCING STEEL	REMARKS ON SHEET NO	SIZE DESCRIPTION SIZE	LENGTH  "L"  SKEW  COVER  CLASS "Q"  CLASS "Q"  CLASS "Q"  COVER   Rev. 2-17-66 ADDED CLASS I DRIVE 5+72RT.  P.R. ROBERTS RD.  REMARKS  Drive Relocated Per R/W Dopt. 4-15-66  & STRUCTURE NO. 44 Revised.	
1 131 +15 Bridge over Buck Creek 148.1	ELEV, ELEV. CUYDS, CUYDS. LBS.				Rev. 8-12-GG SEWER STRUCTURGS ADDEO. Herkd. Design Rev. 7-26-68, Str. No. 40, Add Str. No. 40 A & 40B. A. PERRY
11 116 + 15 Rt. 24" Pipe in Place 12 117 + 60 Rt. 6.301 Group"H-1" Pipe (Ga. 12 C.M. Pipe Arch) Regid. 30 13 120 + 45 Rt. Std. Injet Type E-7 &	G.09 8 717.19 716.89	No Changes Reg'd.  Drains 25 Acres. C. = 0.5. Remove Pipe in Place.	11 181+90 Lt. Pipe in Place 12 182+95 Rt. 12" Group D" Pipe (Ga. 16 C.M.) Regid. 13 183+25 Lt. 12" Group "R" Pipe (Ga. 16 C.M.) Regid.	24' 1' 0.56 1 24' !! 0.56 1	No Changes Regid.  Connect to Tile in Place.
12" Group A Pipe (Ga. 16 F.B.C.M. with P.I.) Req'd. 51  14 120 + 91 Lt. 2.2" Group H-1 Pipe (Ga. 16 C.M. Pipe Arch) Req'd. 28  15 120 + 91 Rt. 10.2" Group H-1 Pipe (Ga. 16 C.M. Pipe Arch) Req'd. 46  16 122 + 85 Lt. 2.2" Group H-1 Pipe (Ga. 16 C.M. Pipe Arch) Req'd. 24	1 1 2.44 25	Headwall Regid on Outlet End Only.  Pipe Anchors Regid.  Pipe Anchors Regid. Remove Pipe in Place.  Pipe Anchors Regid. Remove Pipe in Place.	14 183 + 84 Rt. 6" Sewer Pipe Req'd.  15 183 + 88 Lt. Std. Inlet Type J-10 &c  12° Group A Pipe (Ga. 16 F.B. O. C. M. with P.I.) Req'd  15 A 184 + 10 12" CM. Pipe (Ga. 16 J. Req'd.  16 184 + 27 Lt. 15" Group "D" Pipe (Ga. 16 C. M.) Req'd.  17 184 + 78 Rt. 15" CM. Pipe (Ga. 16) Rea'd.	723.47 720.47  1. 60' 2' 720.47 720.36 0.28 4  10' 0.56  80' 3' 720.05 719.81 0.69 4  80' 3' 719.97 719.82 0.69 5  54' 1' 0.84 3	Headwall Regid. on Outlet End Only.
17 123 + 08 Rt. 11.4 <sup>ml</sup> Group H-1 Pipe (Ga. 10 C.M. Pipe Arch) Req'd. 24 18 124 + 50 Lt. Std. Inlet Type E-7 &  12" Group A Pipe (Ga. 16 F.B.C.C.M. with P.L.) Req'd. 58 19 124 + 56 Rt. 11.4 <sup>ml</sup> Group H-1 Pipe (Ga. 10 C.M. Pipe Arch.) Req'd. 24 20 126 + 61 Rt. 12.9 <sup>ml</sup> Group H-1 Pipe (Ga. 10 C.M. Pipe Arch.) Req'd. 68	716.32 713.24 719.5 0.28 4 2 11 2.62 12 719.5 2.69 36	Pipe Anchors Regid. Remove Pipe in Place.  Headwall Regid on Outlet End Only.  Pipe Anchors Regid. Remove Pipe in Place.  Pipe Anchors Regid. Remove Pipe in Place.	15A 184 + 10 12" CM. Pipe (Ga. 16 7 Req'd.  16 184 + 27Lt. 15" Group "D" Pipe (Ga. 16 C.M.) Req'd.  17 184 + 78Rt. 15" C.M. Pipe (Ga. 16) Req'd.  18 185 + 90 Lt. 12" C.M. Pipe (Ga. 16) Req'd.  19 186 + 27Rt. 12" Group "D" Pipe (Ga. 16 C.M.) Req'd.  20 187 + 08Lt. 12" C.M. Pipe (Ga. 16) Req'd.	24'   1'   0.56   1	Remove Pipe in Place.  One 12" on 12" Tee Regid. Three Headwall's Regid. Remove Pipes in Place.  Remove Pipe in Place.  Remove Pipe in Place.  Remove Pipe in Place.
21   128 † 00 ±   Structure in Place. 22   128 † 50   Std. Inlet Type E-7 &   12"   Group "A" Pipe (Ga. 16 F.B.C.C.M. with P.I.) Regid 7.5 23   130 † 20 Lt.   Manhole in Place. 24   130 † 35   G"   F.B.C. Perforated G.M. Pipe Regid.   11.5 25   131 † 25   G"   F.B.C. Perforated C.M. Pipe Regid.   11.5	71G.69 713.55 5' 4' 711.0 0.28 4 0' 6.4'	Remove Structure in Mace. Special Borrow Regid to Backfill Existing Channel.  Construct Headwall & Outlet Ditch on Rt.  Eight Lin. Ft. of Reconstructed Manhole Regid. Adjust Casting to Grade.  Drains thru Rt. Slape Wall.  Drains thru Rt. Slape Wall. One G" Elbow Regid.	21 187 † 43Rt. 12" C.M. Pipe (Ga. 16) Regid. 22 187 † 75Lt. 5td. Inlet Type E-7 &  12' Group A Pipe (Ga. 16 F.B.C.C. M. with P.I.) Regid.  23 188 † 00Lt. 12" C.M. Pipe (Ga. 16) Regid.  24 188 † 20 1.8" [t. Group G 1 Pipe (Ga. 16) Regid.  25 188 † 52 Lt. 18" C.M. Pipe (Ga. 16) Regid.	727.42 720.92	Connect to STR NO.24. Construct Headwall on Outlet End.  Remove Pipe in Place.  Construct Outlet Pitch. Remove Pipe in Place. One 12" Teo Regid.  Remove Pipe in Place.  Remove Pipe in Place. Anchors Regid.
		Construct Headwall & Outlet Ditch on Lt.  Remove Pipe in Place.  Eleven Lin. Ft. Reconstructed Manhole Regid. Adjust Casting to Grade.	23 188 † 00 Lt. 12" C.M. Pipe (Ga. 16) Regid.  24 188 † 20 1.8"   Etcup o 1 Pipe (Go. 16 F. 8. C. C. M. with P. 1.) Regid.  25 188 † 52 Lt 18" C.M. Pipe (Ga. 16) Regid.  26 189 † 10 Rt. 15" Group "D" Pipe (Ga. 16 C.M.) Regid.  26 5 † 72 PR Rt. 8.7 Sq. Ft  Roberts Rd. 12" C.M. Pipe (Ga. 16) Regid.  27 5 † 70 Lt. P.R.  Roberts Rd. 12" C.M. Pipe (Ga. 16) Regid.  28 189 † 45 Lt. 18" Group D" Pipe (Ga. 16 C.M.) Regid.  29 191 † 00 Rt. 5td. Inlet Type E-7 &c.	21.3 - 0.56 1 24' 0.56 1 80' 0.80 8	Remove Pipe in Place.  Remove Pipe in Place.  Remove Pipe in Place.  Remove Pipe in Place.
26 132 † 05 Std. Inlet Type E-7 &  12" Group "A" Pipe (Ga. 1G F.B.C. C. M. with P.I.) Red 75  27 133 † 32Rt. 24" Group "D" Pipe (Ga. 1G C. M.) Req'd. 42  28 133 † 62Lt. Monhole in Place.  29 3 † 005 † 81t. 12" Group "D" Pipe (Ga. 1G C. M.) Req'd. 63  30 2 † 04 \$ 4 \$ 8 \$ 12" CS Pipe (Ga. 1G C. M.) Req'd. 63  31 3 † 205 † 8 \$ 15" Std. Inlet Type E-7 &  12" Group "A" Pipe (Ga. 1G C. M.) Req'd. 74  \$ 135 † 15 Rt. 24" Group "D" Pipe (Ga. 1G C. M.) Req'd. 74  \$ 135 † 15 Rt. 24" Group "D" Pipe (Ga. 1G C. M.) Req'd. 78  34 135 † 50 Std. Inlet Type J-10 &  12" Group "A" Pipe (Ga. 1G F.B.C.C. M. with P.I.) Reg'd. 66  12" Group "A" Pipe (Ga. 1G F.B.C.C. M. with P.I.) Reg'd. 66  12" Group "A" Pipe (Ga. 1G F.B.C. C. M. with P.I.) Reg'd. 66	0.56 <del>0.88</del> 4. 0.56 1 710.5 713.4 71 711.5	Connect to Structure No. 31. Construct Headwall & Inlet Ditch on Inlet End.  Construct Special V-Ditch to obtain necessary cover.  Connect to Structure No. 32. One 12" on 12" Y Regid.  One 12" on 24" Y Regid.	730 191 + 23Lt. 12" C.M. Pipe (Ga. 16) Regid.	24' 1' 0.56 1	Aleadwall Regid. on Outlet End.
85 G179 3-4-8 Manhole in Place		Headwall Regid on Oaflet End Only. Adjust Casting to Grade.	-8.7 Sq. Ff.  31 191 + 88 Rt. + Group DPipe (Ga. 16 C.M.) Regid.  32 39 + 91 West 12" C.M. Pipe (Ga. 16) Regid.  33 38 + 84 West 12" C.M. Pipe (Ga. 16) Regid.  34 37 + 15.5 East 12" C.M. Pipe (Ga. 16) Regid.  35 36 + 35 East 12" C.M. Pipe (Ga. 16) Regid.  35 36 + 35 East 12" C.M. Pipe (Ga. 16) Regid.  35 36 + 35 East 12" C.M. Pipe (Ga. 16) Regid.	24'	Anchor's Rea'd.  Remove Pipe in Place.  Remove Pipe in Place.  Under Walk  Remove Pipe in Place.
36 136+75 Std. Inlet Type E-7 & 12" Group A Pipe (Ga.16 F.B.C.C.M. with P.1.) Regid. 45 37 137+05Lt. 24" Group D" Pipe (Ga. 16 C.M.) Regid. 28 38 141+80Lt. 24" Group D" Pipe (Ga. 16 C.M.) Regid. 119 39 142+01Lt. Std. Inlet Type E-7 & 12" Group A Pipe (Ga.16 F.B.C.C.M. with P.1.) Regid. 17	[16.2.76] 162.7	Headwall Regid. on Outlet End Only. Construct Outlet Ditch.  Remove Pipe in Place.  One 12° on 24° Y' Regid.  Connect to Structure No.38.	36 36+03West 6" Sewer Pipe Read.  37 35+223West 12" Group D' Pipe (Ga.16 C.M.) Read.  38 34+90 West Std. Inlet Type E-7 &c  12" Group A" Pipe (Ga.16 F.B.C.C.M. with P.1.) Re  39 33+31 West 1.60 Ft. Group H-1 Pipe in Place.		Connect to Sewer in Place.  Remove Pipe in Place.  Headwall Regul. on Outlet End Only.  Relay Pipe in Place.
40 142+25  12" Group "A" Pipe (Ga. 16 F.B.C.C.M. with P.1.) Regul 57  40A 142+25  Std Inlet Type J-10 Regid.  41 147+50  Std. Inlet Type E-7 &c  12" Group "A" Pipe (Ga. 16 F.B.C.C.M. with P.1.) Regid. 51	724.46 721.38 720.96 0.28 4 727.89 724.81 724.64 0.28 4 1 0.56 1	Headwall Regid. on Outlet End on Rt.  Connect To Pipe in Place, Remove Inlet Type "E" in Place  Headwall Regid. on Outlet End on Lt.	40 31+30 East Pipe in Place  41 31+18 West 2.8° Ft. Pipe in Place.  42 30+15 ± West 6" Sewer Pipe Regid.  43 29+85 East Inlet & Pipe in Place  44 29+68 West 12' E4" Pipe in Place C.S. Pipe (Ga.16)		No Changes Regid. Relay Pipe in Place. Connect to Sewer Pipe in Place.
42 5+205-561.t. 12" C.M. Pipe (Ga. 16) Rega. 22  43 161+00 Std. Inlet Type E-7 &c  12" Group A Pipe (Ga. 16 F.B.C.C.M.with P.I.) Regy. 15  44 161+00 Rt. 24" Group G-2 Pipe (Ga. 10 F.B.C.C.M. Pipe Arch) Regy. 280  45 164+28 Std. Inlet Type E-7 &c  12" Group A Pipe (Ga. 16 F.B.C.C.M. with P.I.) Regy. 50	30° 4' 715.00 714.50 10.12 190	Connect to Structure No. 44, One 12" Tee Regid.  Twin Structures. Pipe Anchors Regid.  Headwall Regid. on Outlet End Only. Construct Outlet Ditch.	# 331 37190 PR#2 # Special Manhole Tuge F-A 80		No Changes Regid.  Retay Pipe in Place. Headwalls Regid.  Anchor Regid. on Inlet End Only. Connect to Structure No. 33 B
46 168 + 60 Std. Inlet Type E-7 8c  12" Group "A" Pipe (Ga. 16 F.B.C.O.M. with P.I.) Regid. 51  47 176 + 85 Std. Inlet Type E-7 8c  12" Group "A" Pipe (Ga. 16 F.B.C.C.M. with P.I.) Red. 48  48 178 + 60 Std. Inlet Type J-10 8c	3 <sup>1</sup>   2 <sup>1</sup>   723.0   0.28   4   1   1   1   1   1   1   1   1   1	One G"on 12" Tee Regid. Headwall Regid. on Outlet End Only.  Headwall Regid. on Outlet End.	8.856Ft Group Ri Pipe Read.  338 37+25PR'S *Special Manhole, Type E:4 & 8.854Ft Group Ri Pipe Read  330 37:25,60'4PR#3 Standard Injet, Type E:7 & 12" Group "P" Pipe Read  38A 33:70PR#3 Standard Injet, Type E-7 & 12" Group"P" Pipe Read	/ 20 117-50 17-50	Connect to Str. No. 33 B  Connect to Str. No. 38 B
15" Group A Pipe (Ga.16 F.B. C. C.M. with P.1) Reg'd. 12'	6 15° 4 720.80 720.60 0.69 7	Construct Outlet Ditch.  Construct Inlet & Outlet Ditch.  Connect to Pipe in Place.  Headwall Regid. on Inlet End. Construct Inlet Ditch.	38A 33+70PR#3 Standard Inlet, Type E-7 &  12" Group P" Pipe Read  38B 33+60PR#3 #Special Manhole, Type E-4 &  8.85qFl Group R1 Pipe Read  38C 33+60+32+80 8.85qFl Group R1 Pipe Read  West  41 31+18 PR#3 10.25qFl Group H1 Pipe Read	360' 715.56 714.46 714.21 713.97 1.13 38 713.52 713.45 2.37 5	Anchor Read on Outlet end Only. Connect to Str. No. 38 B  Remove pipe in place - Anchors Read.
51 4+20 P.R.Rt.  THOMPSON RD 12" C.M. Pipe (Ga.16) Regid.  52 180+25 15" Group A Pipe (Ga.16 F.B.C.C.M. with P.L.) Regid 100  * 40B 143+25 Std. Inlet Type "E" Regid. \$  12" Group A Pipe (Ga.16 FRCCS W/PI) Regid 100	0' 1' 719.65 719.48 0.56 1 2' 3' 721.25 720.90 0.69 9	Remove Pipe in Place.  Connect to Str. No. 40A-Use 7 Casting from Str. No. 40.	45 28+50 PR#3 II.4 Suft. Group HI Pipe Read.	G4' 2.44 28	Anchors Regid
UNDISTRIBUTED QUANTITIES  G" Sewer Pipe 80  8" Sewer Pipe G0	O'				
10" Sewer Pipe 40					
* Out of Sequence		Nover	* See STRUCTURES BELOW mber 6, 1961		S PROJECT NO. LINE SHEET TOTAL PILE  330(6) US 350(1) 228 103