

INDEX

Table with columns: SHEET NO., DESIGNATION, B.P.R. APPROVAL, DATE ADOPTED 'A' or LATEST REVISION 'R'. Lists various sheets including Title Sheet, Cross Sections, Standards, and Structure Data.

Table with columns: SHEET NO., DATE, REVISIONS. Lists changes to various sheets, such as 'R/W Per Road and Bridge Design Dept.' and 'DRIVE RELOCATED'.

Table with columns: SHEET NO., DESCRIPTIONS. Lists sheet numbers and their corresponding descriptions, such as 'TITLE SHEET AND R/W INDEX' and 'APPROACH TABLE'.

INDIANA STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS DATED 1963 TO BE USED WITH THESE PLANS

LEGEND table with symbols and descriptions: Barricade Type A, Barricade Type B, Typical Sign Standards, Construction Identification Sign, Urban Limits.

RIGHT OF WAY PLANS

STATE OF INDIANA

INDIANA STATE HIGHWAY COMMISSION

CODE # 0530

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY

S. PROJECT NO. 330 (6) P.E. (6) R/W. (8) CONST.

BEGINNING AT A POINT ON SR. NO. 135 (S. MERIDIAN ST.) 827.83 FT. SOUTH OF THE NORTHWEST CORNER OF SECTION 12, T.14-N. R.3-E. THENCE IN A NORTHERLY DIRECTION A DISTANCE OF 62.2245 FT. TO A POINT 37.5 FT. NORTH OF THE NORTH LINE OF SECTION 1, T.14-N. R.3-E., ALL IN PERRY TOWNSHIP, MARION COUNTY.

GROSS LENGTH: 1.178 MI. NET LENGTH: 1.178 MI.

SCALES:

PLAN LONG: 1"=50' TRANS: 1"=50' PROFILE HORIZ: 1"=50' VERT: 1"=10'

MAX. GRADE 1.80 %

THIS PROJECT INCLUDES R/W FOR SEPARATE CONTRACT STRUCTURE

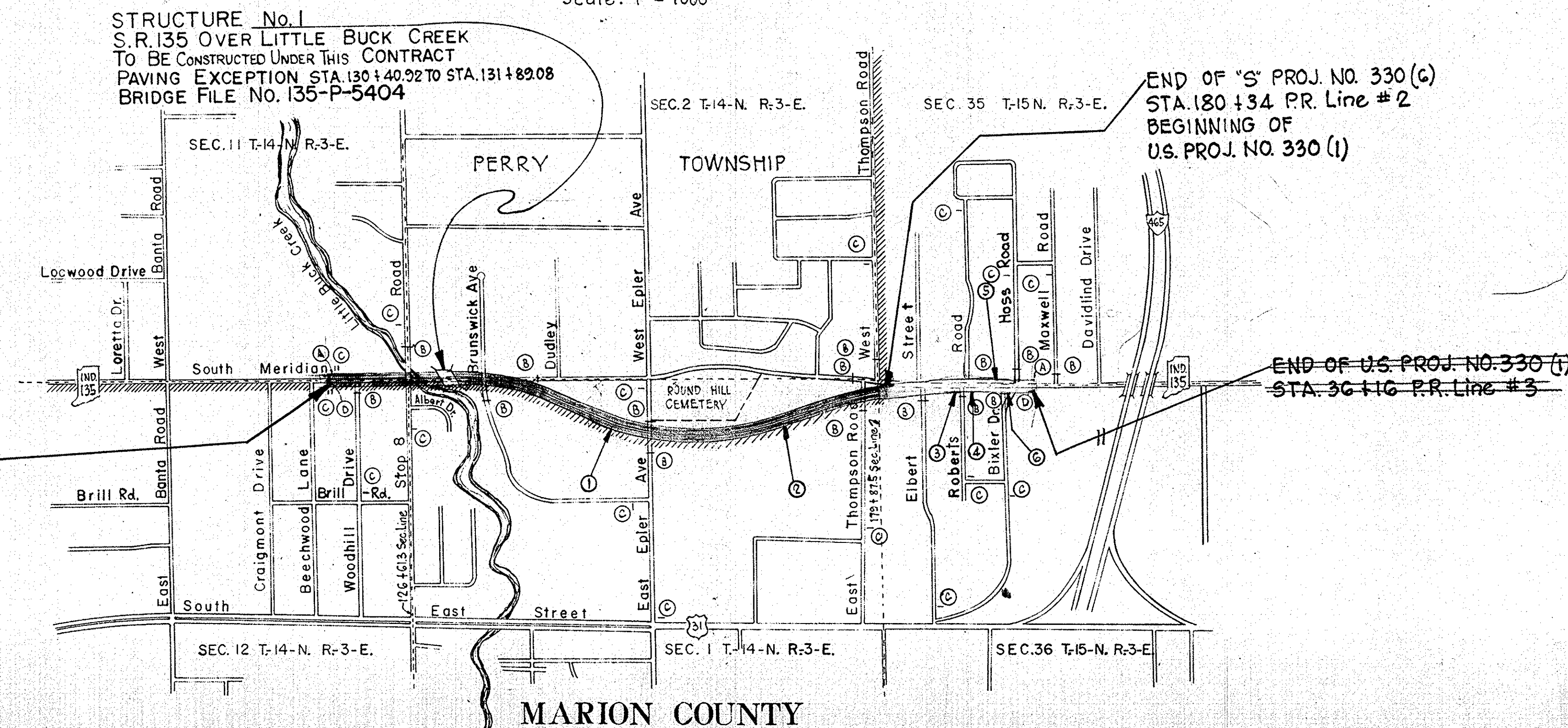
U.S. PROJECT NO. 330 (1) P.E. (1) R/W. (1) CONST.

BEGINNING AT A POINT ON RELOCATED SR. NO. 135 37.5 FT. NORTH OF THE SOUTH LINE OF SECTION 36, T.15-N. R.3-E. AND EXTENDING IN A NORTHERLY DIRECTION TO A POINT ON SR. NO. 135 A DISTANCE OF 1626.09, ALL IN PERRY TOWNSHIP, MARION COUNTY.

GROSS LENGTH: 0.307 MI. NET LENGTH: 0.307 MI. MAX. GRADE: 2.48 %

EQUATIONS

- List of equations for points of interest, such as 'P.O.C. 150 + 38.62 P.R. Line #1' and 'P.O.T. 169 + 34.92 Line #2'.



MARION COUNTY

APPROVED 9-29-64 [Signature] CHIEF ENGINEER - INDIANA STATE HIGHWAY COMMISSION

Code 0530 S-330(6) SR-135 Marion Co. 23 Sheets

RECOMMENDED FOR APPROVAL 9-25-64 [Signature] ENGINEER OF PLANS AND SPECIFICATIONS INDIANA STATE HIGHWAY COMMISSION

Table with columns: PROJECT NO., LINE, SHEET NO., TOTAL SHEETS, FILE. Shows project details for S-330(6).



ROAD SR135, COUNTY-MARION PROJECT S-330(5) L.A. CODE 0530 DATE 01/25/66

PARCEL LISTING FOR LAND ACQUISITION INDIANA STATE HIGHWAY COMMISSION

PARCEL NUMBER	GRANTOR	CENTER LINE	FROM APPROX STA.	TO APPROX STA.	PLAN SHEET	BRIDGE	TOTAL AREA	R/W EXISTING	NATURE OF TITLE	LAND TO BE ACQUIRED	RESIDUE AREA	BLDG.
1	PARCEL 1 ON PROJECT S-330(6) AND PARCEL 1 ON PROJECT S-330(4)											COVER THE SAME LAND, WITH
1	BARRON, ROD ET UX.	PR1	116	118	6+13		27,675SF		FS	4,982SF	A= 22,693SF	
2	MCKENZIE, THOMAS ET UX.	PR1	118	121	6+ 7+13		44,000SF		FS	6,253SF	A= 37,747SF	
3	CHRIST PRESBY CHURCH	PR1	118	122	6+ 7+13		4,252AC	0.459AC	FS	0.172AC	A= 2,933AC	
3A		PR1	121	123	7+13				FS	0.057AC	B= 0.631AC	
3B		PR1	123	124	7				TE	0.157AC	A= 1.364AC	
4	MCCLANAHAN, REVEL ET UX.	PR1	123	126	7+14		2,027AC	0.506AC	FS	0.157AC	A= 1.364AC	
5	PAVEY, LOUIS E. ET UX.	PR1	121	123	7+13		28,000SF		FS	4,438SF	A= 23,562SF	
6	NICHELSON, RONALD ET UX.	PR1	122	124	7		22,000SF		FS	3,300SF	A= 18,700SF	
7	WASSON, DAVID ET UX.	PR1	123	125	7+14		22,000SF		FS	3,300SF	A= 18,700SF	
8	RILEY, JAMES A. ET UX.	PR1	124	126	7+14		28,000SF		FS	4,512SF	A= 23,488SF	
9	ELDER, ROMENA OEFLER	PR1	127	129	7+14		49,593SF		FS	10,731SF	A= 38,862SF	
9A		PR1	127	129	7+14				TE	13,998SF		
10	PRIZEVOITS, PETERIS ET UX.	PR1	129	130	7+14		30,135SF		FS	6,182SF	A= 23,953SF	
10A		PR1	129	130	7+14				TE	6,860SF		
11	KING, LOUIS P. ET UX.	PR1	130	132	7		32,702SF		FS	7,887SF	A= 24,815SF	
11A		PR1	130	132	7				TE	3,313SF		
11B		PR1	130	132	7				FS	3,834SF		
12	YEAGER, JOHN W. ET UX.	PR1	132	134	7+15		0,930AC	0.178AC	FS	0.141AC	A= 0.566	
12A		PR1	131	133	7+15				FS	0.045AC		
12B		PR1	133	134	7				PV	1,266SF		
13	SPEICHER, REGINALD ET AL	PR1	127	133	7+14+15		17,696AC	1.973AC	FS	0.011AC	A= 14.010	
13A		PR1	127	132	7+14+15				FS	1.189AC		
14	LAUCK, MARIE T.	PR1	133	134	7+15		405,221SF		FS	4,969SF	A=400,252SF	
15	MCVEY, RAYMOND E. ET UX.	PR1	134	135	7+ 8+15		0,517AC	0.103AC	FS	0.115AC	A= 0.299AC	
16	BIXLER, GEO. M. ET AL.	PR1	135	140	8+15		833,990SF		FS	42,509SF	A=455,479SF B=336,002SF	
17	EGOLD, EDWARD C. ET UX.	PR1	135	136	8+15		0,683AC	0.107AC	FS	0.028AC	A= 0.548AC	
18	DARKO, LOUIS L. ET AL.	PR1	140	154	8+ 9+16		10,031 27,930AC	0,624 0,767AC	FS	4,720AC	A= 0.139AC B= 4.548AC C= 17,156AC	
19	JONES, SYLVESTER ET UX.	B	154	156	9+17		1,000AC	0.038AC	FS	0.052AC	A= 0.905AC	
19A		B	160	161	9				FS	0.005AC		
20	JONES, JAMES T.	B	154	161	9+17		1,000AC	0.038AC	FS	0.962AC		*
21	LUTES, MARGARET ET AL.	B	154	161	9+17		1,000AC	0.038AC	FS	0.962AC		*
22	OSWALD, HULDAH	B	154	161	9+17		1,000AC	0.038AC	FS	0.473AC	A= 0.489AC	
22A		B	7	8	9				TE	0.012 AC		
23	ROUND HILL CEMETERY	B	161	162	9		13,238AC	0.874AC	FS	0.161AC	A= 12.203AC	
24	CORDES, HERMAN D. ET UX.	PR2	161	178	9+10		46,260AC	2,140AC	FS	6,550AC	A= 29,039AC B= 8,512AC	
24A		PRTRD	11	12	18				FS	0.019AC		
25	PARCEL 25 ON PROJECT S-330(6) AND PARCEL 1 ON PROJECT US-330(1) COVER THE SAME LAND, WITH											
25	MARATHON OIL CO.	PR2	178	182	10+11		4,960 0,660AC	0,678 0,878AC	FS	1,444 1,456AC	A= 2,838 3,918AC B= 2,000AC	
26	JONES, ERNEST E.	PRTRD	3	6	11+18		1,170AC	0.234AC	FS	0.049AC	A= 0.887AC	
27	CARSON, MARIANNE ET AL.	PRTRD	11	12	18		1,000AC	0.064AC	FS	0.003AC	A= 0.933AC	
28	PARCEL 28 ON PROJECT S-330(6) AND PARCEL ON PROJECT S-330(4) COVER THE SAME LAND, WITH											
28	CRAIG, MADGE R. ET AL.	PR1	115	119	6		8,105AC	0.309AC	FS	0.155AC	A= 7.641AC	
29	HUMBLE OIL + REFINING COB		152	154	9		1,300AC	0.175AC	FS	0.358AC	A= 0.767AC	
30	BUCHANAN, WM. A. ET UX.	PR1	135	140	7		42,041 0,930SF		FS	7,091 42,509SF	A= 34,950SF B= 336,002SF	
31	MCCLANAHAN, R. + DYER, V. ET UX.	PR1	134	135	7		0,998AC	0.115AC	FS	0.057AC	A= 0.826AC	

REV. 7-18-66, M.W. MYERS ELIMINATED 5/26/66 W.E. Hyde 30A  
REV. 4-28-66 T. MARONEY

LIST OF EXCESS LANDS TO BE ACQUIRED AND A SEGREGATION BY PROJECTS OF RIGHT OF WAY AREAS AND EXCESS LAND AREAS LYING IN TWO OR MORE PROJECTS

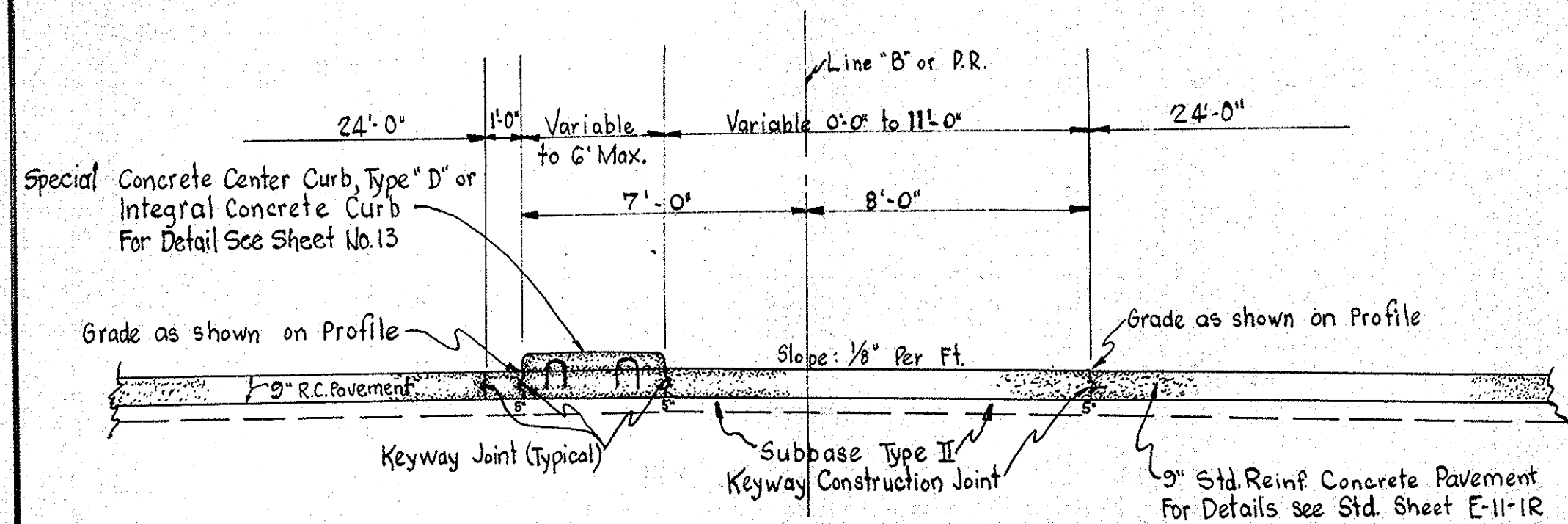
PARCEL NUMERAL	TYPE OF TAKING	LAND TO BE ACQUIRED	PROJECT S-330(6)	PROJECT S-330(4)	PROJECT US-330(1)
1	FS	4,982SF	253SF	4,729SF	
25	FS	1,456AC	1,044AC	0,412AC	
28	FS	0,155AC	0,029AC	0,126AC	

\*(ASTERISK) IN THE BRIDGE COLUMN INDICATES THE PARCEL IS PARTIALLY OR COMPLETELY WITHIN THE LIMITS OF A BRIDGE PROJECT.  
\*(ASTERISK) IN THE BLDG. COLUMN INDICATES A BUILDING IS PARTIALLY OR COMPLETELY WITHIN THE LIMITS OF THE R/W REQUIRED.  
ACCESS RTS = ONLY ABUTTERS ACCESS RIGHTS TO BE ACQUIRED, LEAVING RESIDUE LANDLOCKED  
EASMNT RTS = CLEARANCE OF PRIVATE EASEMENT HELD BY THE OWNER ACROSS THE LAND OF A NEIGHBORING OWNER  
CLEAR RESV = CLEARANCE OF RESERVATION IN OLD GRANT OF EASEMENT  
MINERALS = ACQUISITION OF MINERAL RIGHTS FROM OTHER THAN THE FEE OWNER OF THE LAND  
FS = FEE SIMPLE TITLE  
TE = TEMPORARY R/W  
TB = TEMPORARY R/W FOR BUILDING REMOVAL ONLY  
PV = PROVISIONAL R/W  
SP = QC DEED, R/W GRANT, OR OTHER SPECIAL INSTRUMENT FOR CLEARING ENCUMBRANCES  
IN = INVERSE CONDEMNATION

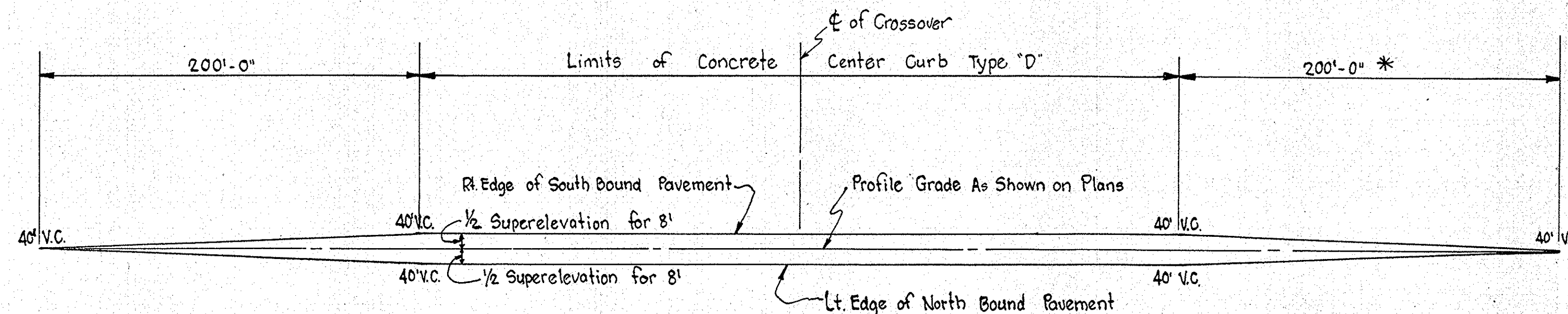
REV 7-27-71 CHRISTY D.E. Y 1 MCCLANAHAN IN  
REV 9-21-71 CHRISTY D.E. Y 2 PARCEL Y 2 INVOLVES THE SAME OWNERSHIP AS THE ACQUISITION OF PARCEL 3 ON THIS PROJECT.  
Y 2 CROOKS, CALVIN E. ET AL. IN



FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	3-330(6) #3-330(4)	1964	3	



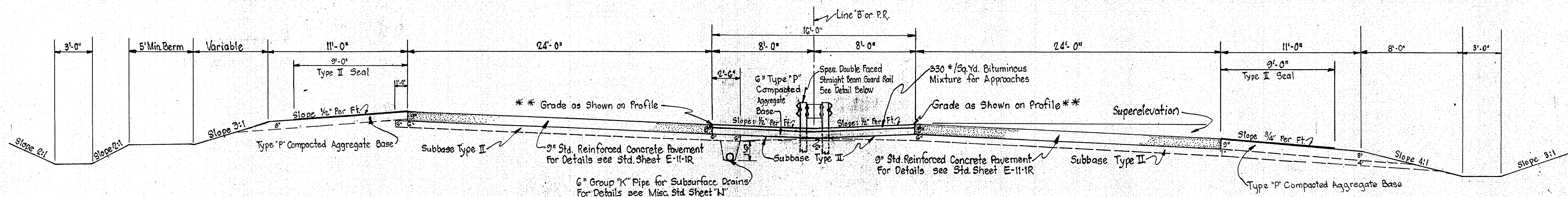
**PARTIAL SECTION SHOWING LEFT TURN LANE ON TANGENT**



**\*\* GRADE TRANSITION ON SUPERELEVATED CURVE RT. (LT. REVERSED) AT CROSSOVERS WHERE LT. TURN LANES ARE INVOLVED.**

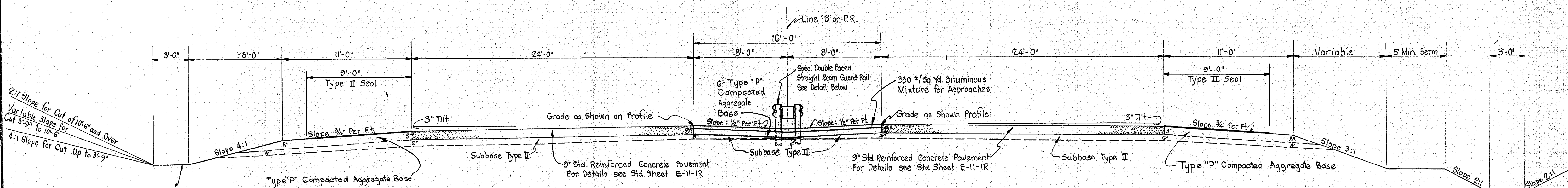
Scale: 1" = 50' 0"

\* Where Limits of Concrete Center Curb Type "D" are less than 400'-00" Apart, Carry the Grade Transition continuous from one Crossover to the next Crossover.



**SUPERELEVATED SECTION**

P.R. Line #1 STATION 132 + 26.21 TO STATION 141 + 59.54 RT. P.R. #1  
 P.R. Line #1 STATION 149 + 38.62 TO STATION 166 + 16.7 LI (Reversed) Line "B"  
 P.R. Line #2 & "B" STATION 169 + 34.92 TO STATION 188 + 29.92 P.R. #2

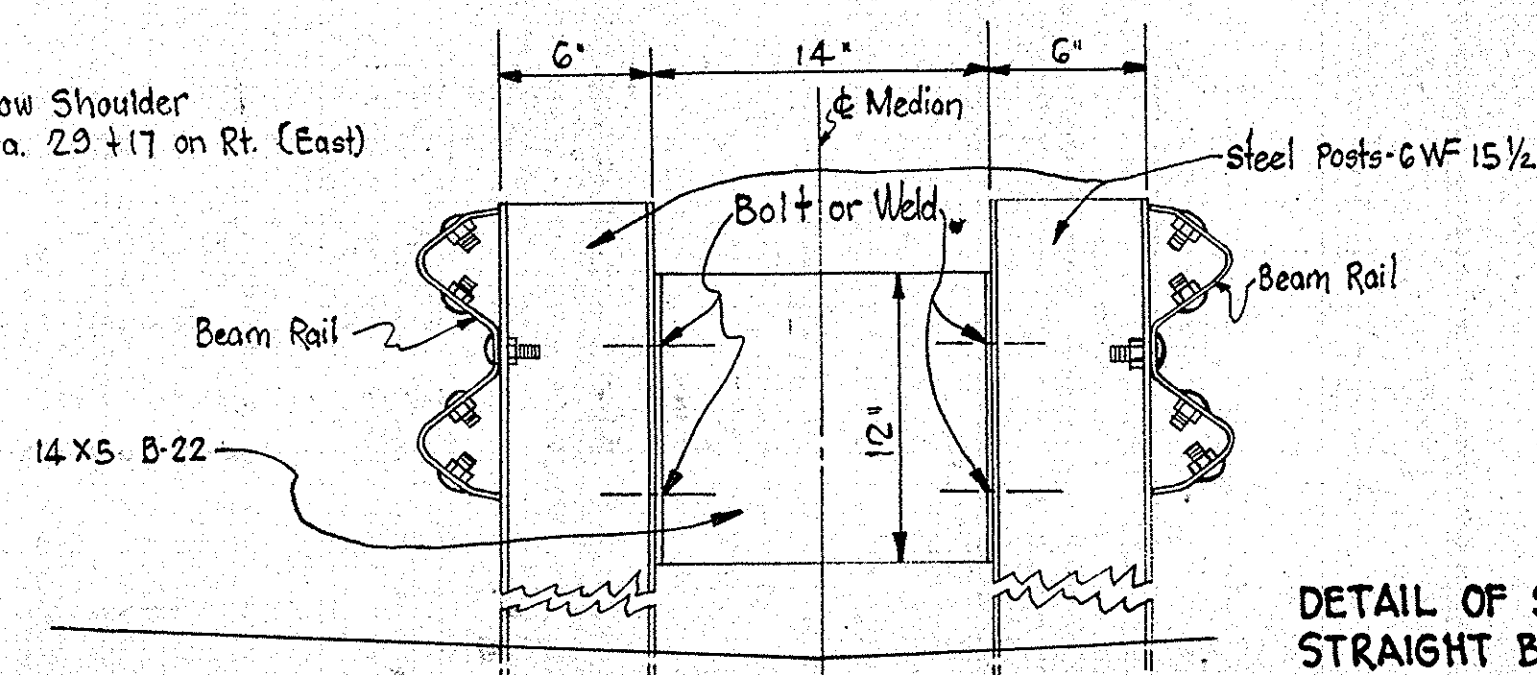


**TYPICAL CROSS SECTION**

P.R. Line #1 STATION 118 + 33.47 TO STATION 132 + 26.21 P.R. Line #1  
 P.R. Line #1 STATION 141 + 59.54 TO STATION 149 + 38.62 P.R. Line #1  
 Line "B" STATION 166 + 16.7 TO STATION 169 + 34.92 P.R. Line #2 & Line "B"  
 P.R. Line #2 STATION 188 + 29.92 TO STATION 36 + 16 P.R. Line #3

**TYPICAL CROSS SECTIONS**

SCALE: 1/4" = 1'-0"



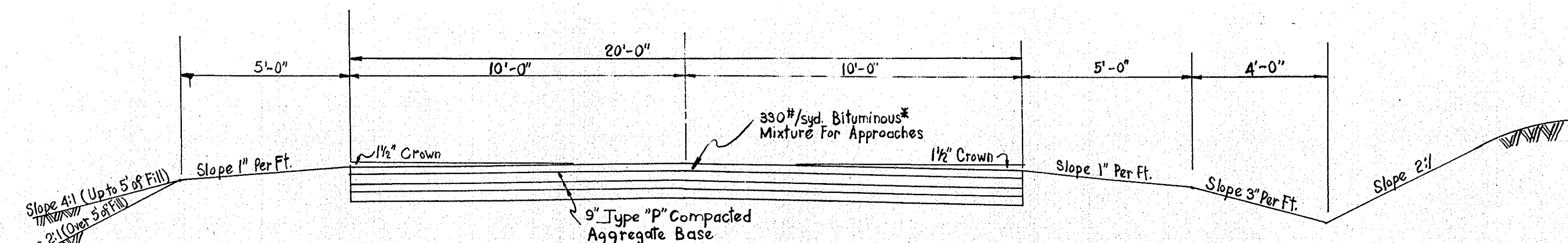
**DETAIL OF SPEC. DOUBLE FACED STRAIGHT BEAM GUARD RAIL FOR OTHER DETAILS SEE STD. SHEET GR-1**

RECOMMENDED FOR APPROVAL

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
US 330(6)		3		

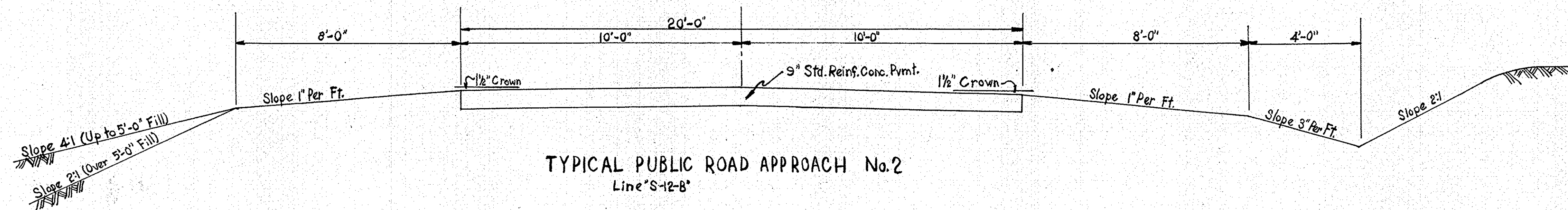


FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	4-330(4) S. 330(6)	1964	4	



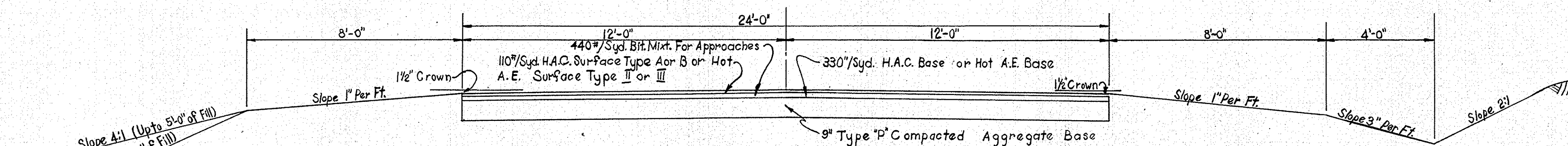
**TYPICAL PUBLIC ROAD APPROACH No.1**

WOODHILL DRIVE Rt. (Line "S-2-B")  
 ELBERT STREET Rt.&Lt. (Line "S-8-B" & "S-9-B")  
 ROBERTS ROAD Rt.&Lt. (Line "S-10-B" & "S-11-B")  
 BIXLER DRIVE Rt.  
 HOSS ROAD Lt.  
 MAXWELL DRIVE Lt.



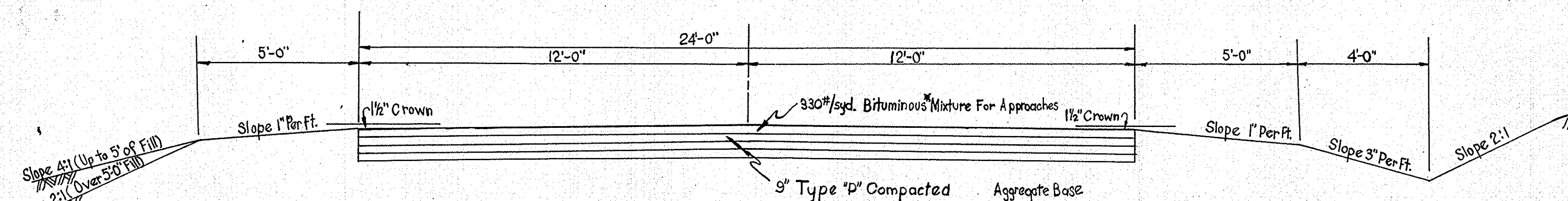
**TYPICAL PUBLIC ROAD APPROACH No.2**

Line "S-12-B"



**TYPICAL PUBLIC ROAD APPROACH No.3**

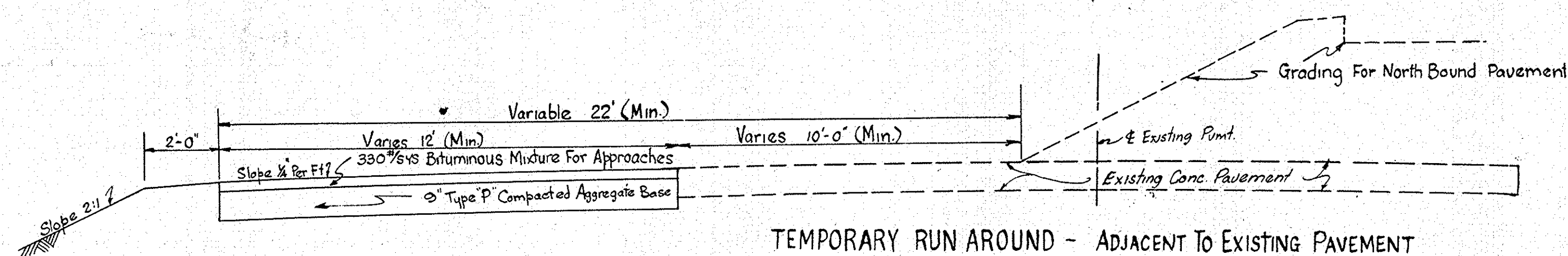
STOP 8 ROAD Rt.&Lt. (Line "S-3-B")  
 EPLER AVENUE Rt.&Lt. (Line "S-5-B")  
 THOMPSON ROAD Rt.&Lt. (Line "S-6-B" & "S-7-B")



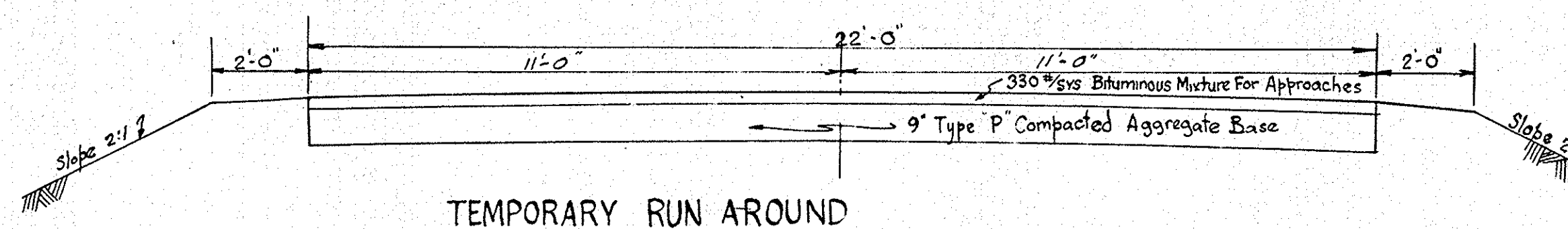
**TYPICAL PUBLIC ROAD APPROACH No.4**

BRUNSWICK AVENUE Rt.&Lt. (Line "S-4-B")

\* For Details See Misc. Std. Sheet H



**TEMPORARY RUN AROUND - ADJACENT TO EXISTING PAVEMENT**



**TEMPORARY RUN AROUND**

# TYPICAL CROSS SECTIONS

SCALE: 3/8" = 1'-0"

RECOMMENDED FOR APPROVAL \_\_\_\_\_

ENGINEER OF ROAD DESIGN, INDIANA STATE HIGHWAY COMMISSION

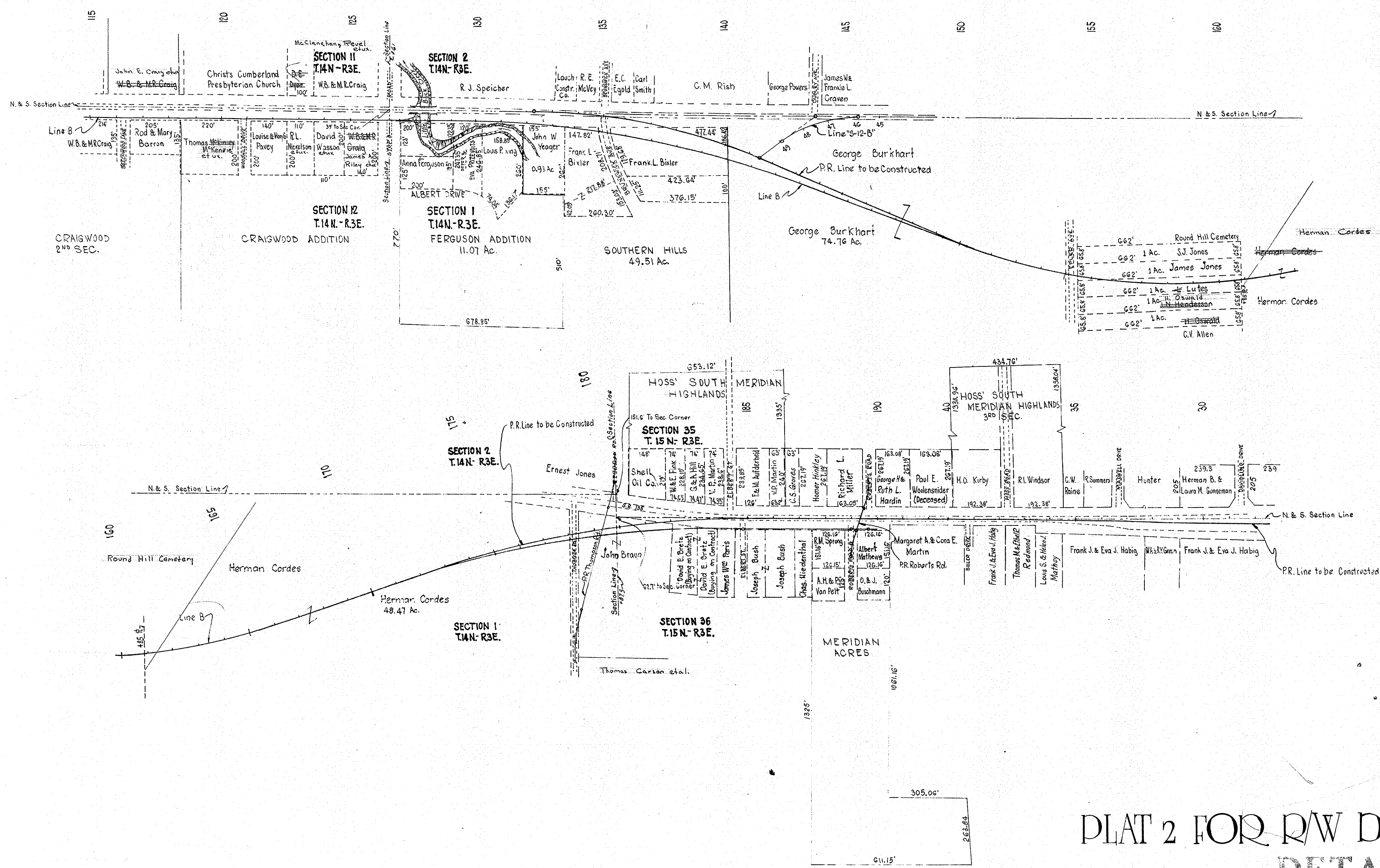
November 6, 1961

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
4-330(4) S. 330(6)		4		



FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	5-330(6)	1965	5	

REV. & ADDED PROVISIONS 1-25-52 PER ROAD DESIGN DEPT.



# PLAT 2 FOR R/W DEPARTMENT DETAILS

Scale: 1"=200'

November 6, 1961

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
5-330(6)		5		

UNB 9-23-64



DATE: 1-25-65  
BY: J. W. Myers  
CHECKED: J. W. Myers  
DATE: 1-25-65  
BY: J. W. Myers  
CHECKED: J. W. Myers

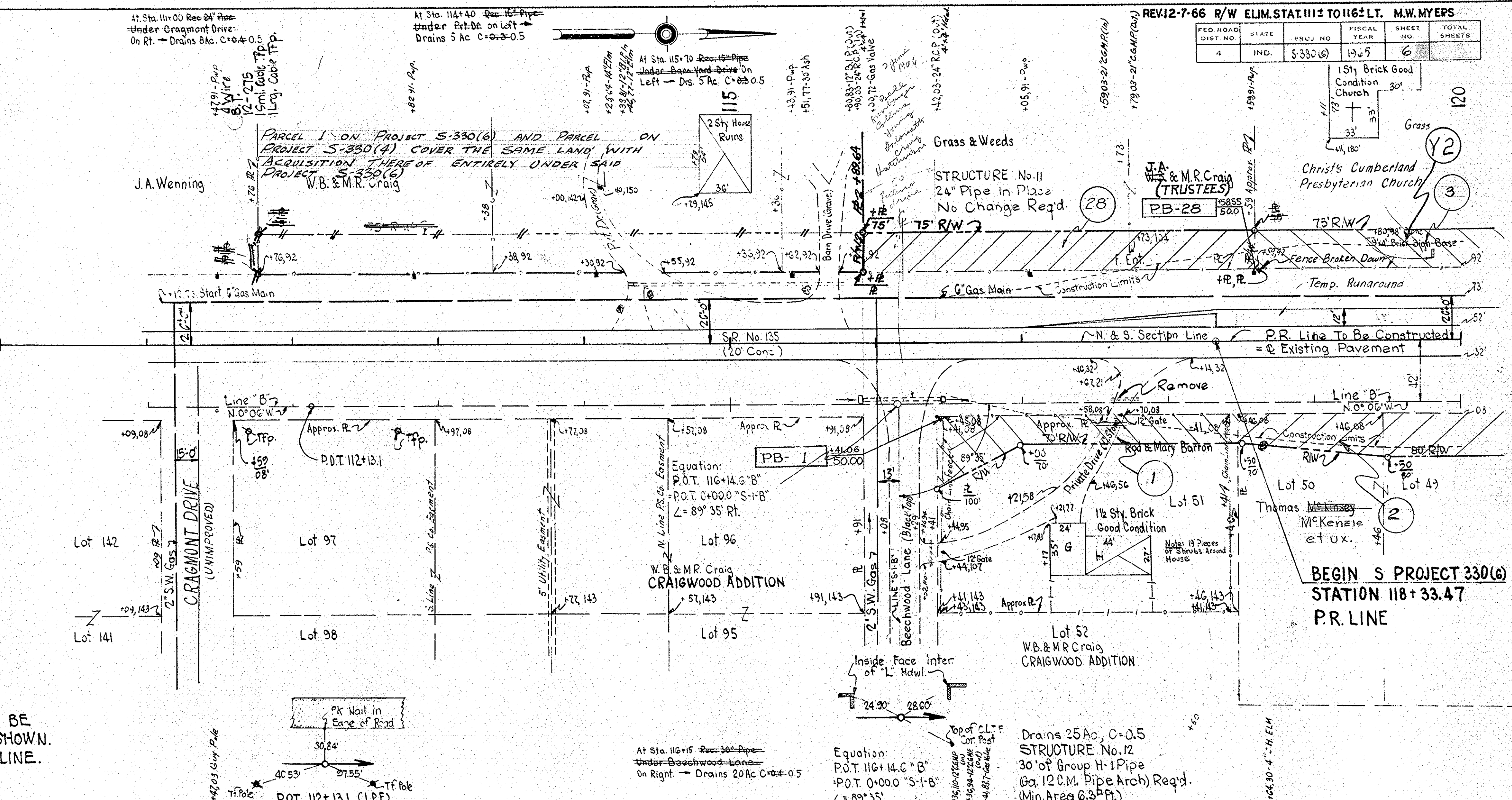
PLAN  
NOTE BOOK ALIGNED CHECKED  
no 128567  
DATE: 1-25-65  
BY: J. W. Myers  
CHECKED: J. W. Myers

Utilities  
Power Lines Owned By Public Service Co.  
Telephone Lines Owned By Indiana Bell Telephone Co.  
Gas Line Owned By Citizens Gas and Coke Utility Co.  
Indianapolis Sanitary District

**LEGEND**

- R/W - RIGHT OF WAY
- L.A. R/W - LIMITED ACCESS RIGHT OF WAY
- A.C.L. - ACCESS CONTROL LINE
- C.L.T. FENCE - CHAIN LINK TYPE FENCE

ALL R/W ON THIS SHEET TO BE  
PRESENT R/W EXCEPT AS SHOWN.  
R/W MEASURED FROM 'PR' LINE.



**FIRST SHEET NOTES**

Standard Cross Section E-11-1F Revised 4-17-64 As Shown on Sheet No. 2 To Be Used on This Project. Typical Cross Sections As Shown on Sheet No. 3&4 To Be Used on This Project.

State Highway Commission of Indiana Standard Specifications Dated 1963 To Be Used With These Plans.

Standards Under Dates As Listed in Index on Title Sheet To Be Used on This Project. Grade Line As Shown on Profile Represents Top of Finished Surface As Indicated on the Standard or Typical Cross Sections.

All Ditches of 1% and Over Shall Be Sodded Except Where Ditch is in Rock Cut or Where Paved Side Ditch is To Be Constructed.

All Shoulders, Cut and Fill Slopes, Shall Be Plain or Mulched Seeded Except Where Sodding is Specified.

Shoulders Are To Be Sodded As Shown on Miscellaneous Standard Sheet "B" or As Indicated on the Typical Cross Sections.

Sodding Shall Be Placed Along Paved Side Ditch As Shown on Miscellaneous Standard Sheet "E".

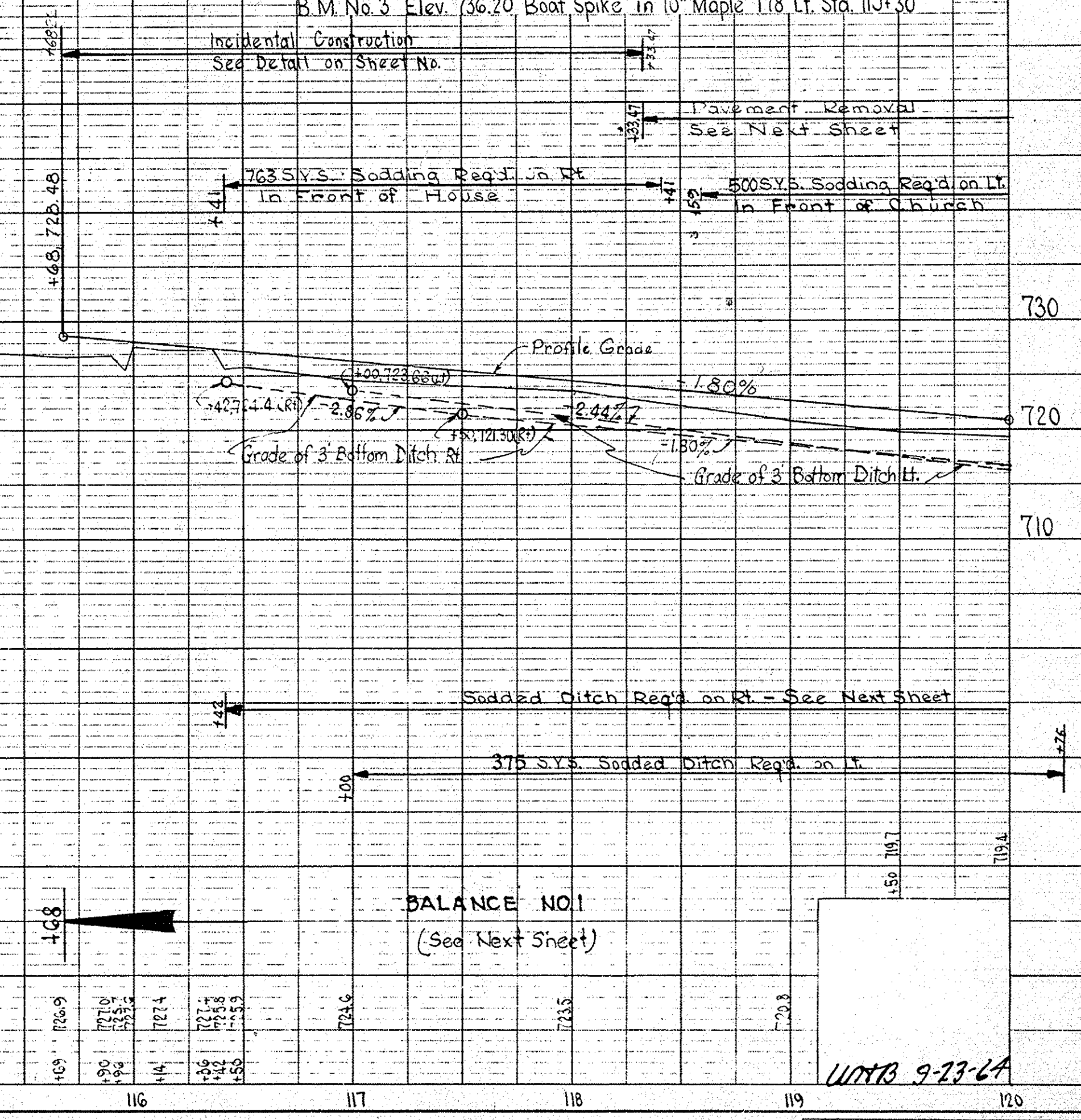
Excavation Quantities As Shown on Plan and Profile Sheets Include Estimated Excavation for Private and Public Approaches. See Table on Sheet No. Paper Relocation on Thomson Road To Be Cross-Sectioned by the Project Engineer Before Construction Where Existing Surface is Located Outside the Limits of New Construction Left of Station 137+40 P.R. and Station 140+00 P.R. The Contractor Will Be Required to Remove the Present Roadway Surface as Directed by Engineer. Contraction Joints Shall Be Placed at All Manholes Within Pavement Limits.

Curves Over 0°-13' Shall Be Super-elevated According to the Standards of January, 1961.

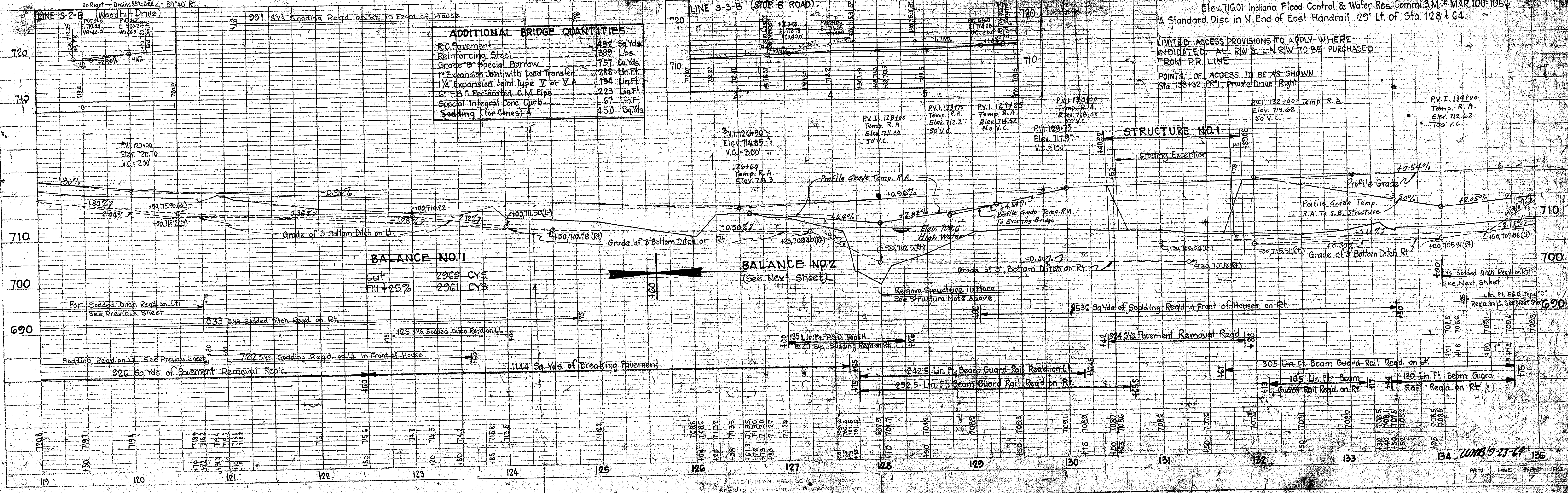
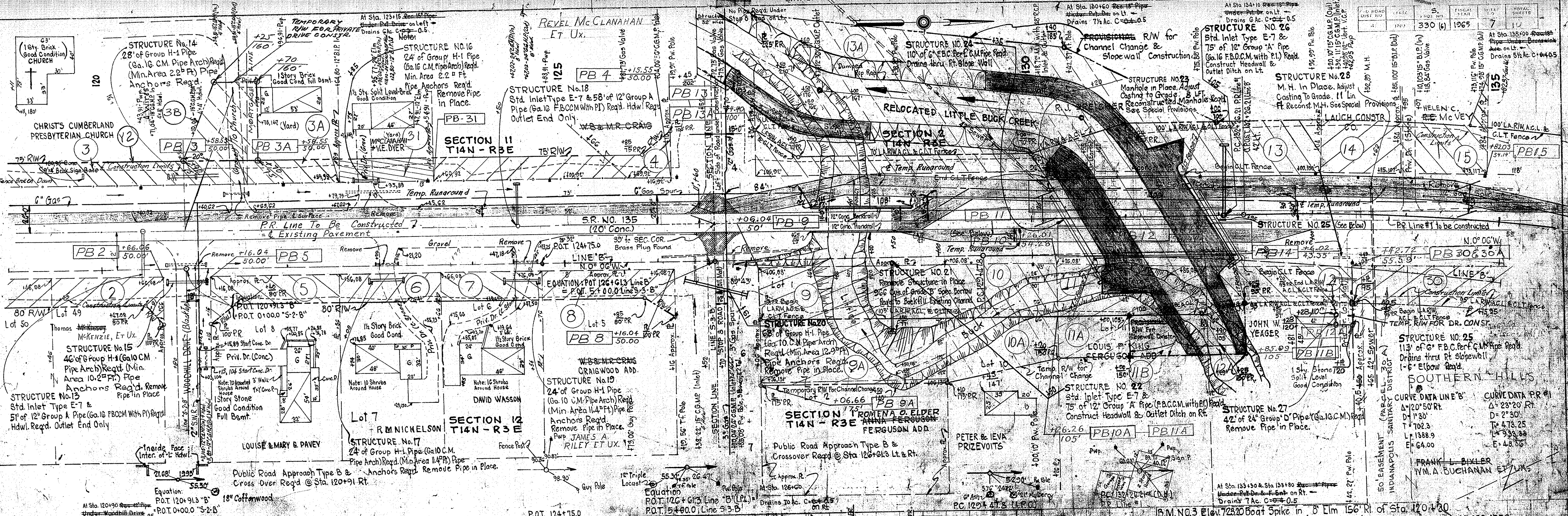
Public Road Approaches Shall Be Constructed As Shown on Miscellaneous Standards Sheet "I" and As Indicated in the Approach Table on Sheet No. 24 & 25.

Private Drive, Commercial Drive and Mail Box Approaches Shall Be Constructed As Shown by Section on Miscellaneous Sheet "B" and "H" and As Indicated in the Approach Table on Sheet No. 24 & 25.

Quantities for Headwalls Are Based on Using Pipe Culvert Headwalls for Retaining 2:1 Slopes and Private Entrance Headwalls for Retaining 4:1 Slopes for kinds of pipe permitted for each size and classification as shown in Structure Notes. See Miscellaneous Standard Sheet "P" & "Q" County Roads To Be Turned Back to County Beyond R/W Markers As Shown on Plans.







PLAN  
 NOTE BOOK, JEFFREY CHICKO, No. 18667 P.P. OF WAY CHECKED, W.G. ZELLER, 3-55

PROFILE  
 NOTE BOOK, JEFFREY CHICKO, No. 18667 P.P. OF WAY CHECKED, W.G. ZELLER, 3-55



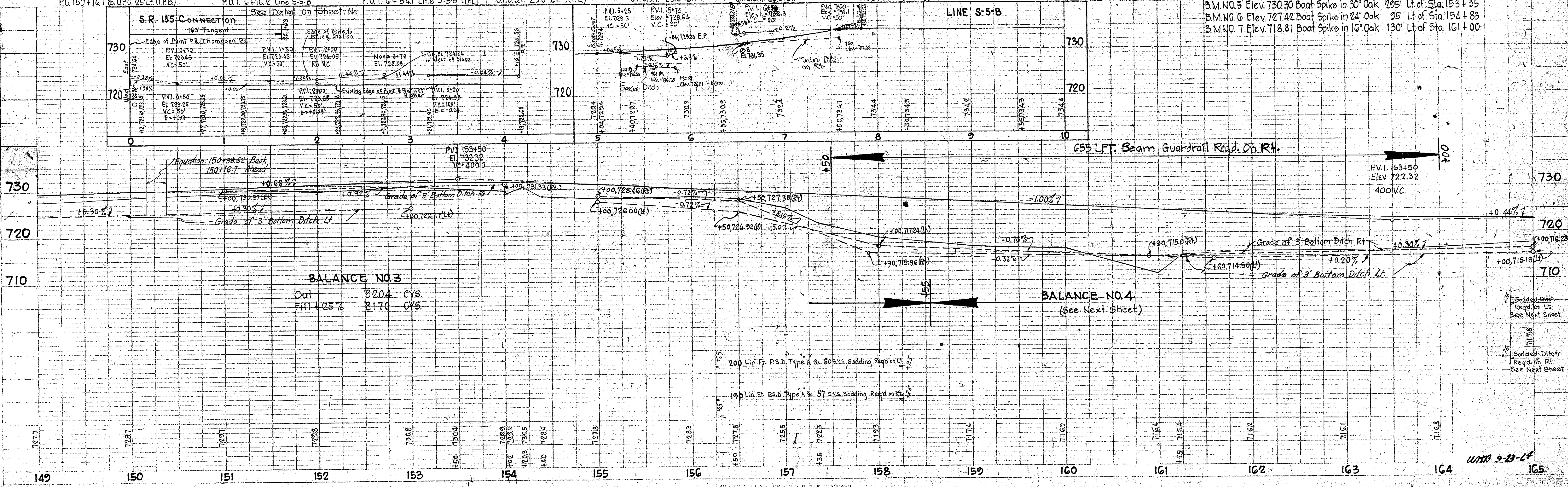
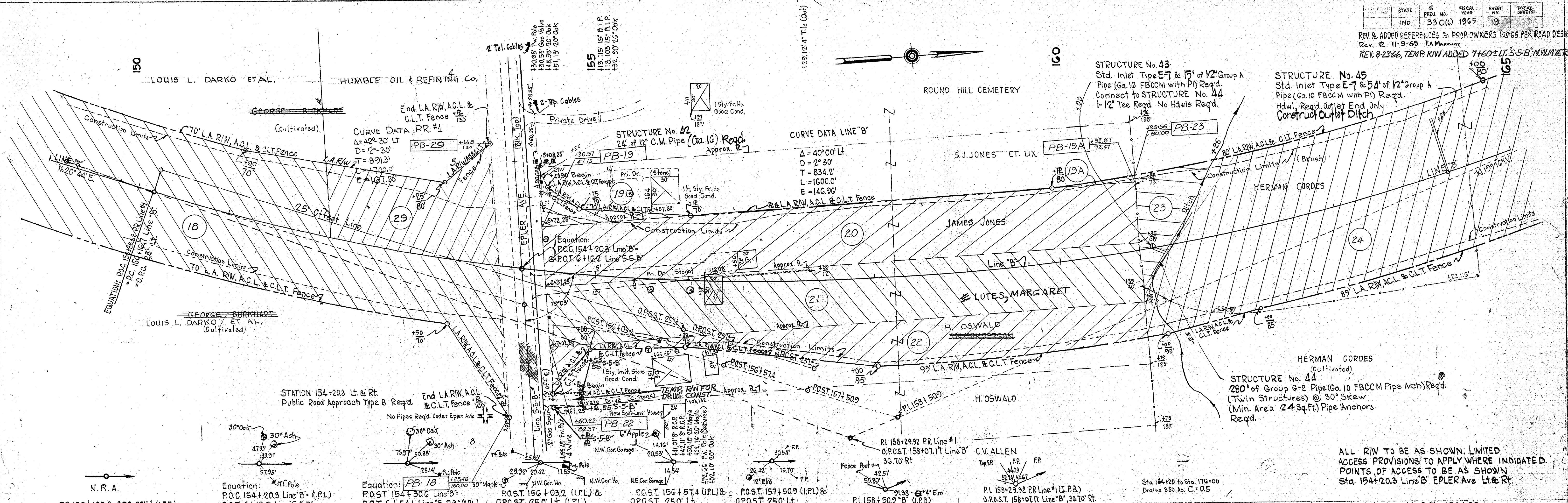




REV. & ADDED REFERENCES BY PROP. OWNERS 12-65 PER ROAD DESIGN  
REV. 11-9-65 TAM:man  
REV. 8-23-66, TEMP. R/W ADDED 7'60" LT. 5'-5" B. 7' MIN. WYETHS

PLAN  
NOTED BOOK PORTER, C.H. DIX  
No. 78671  
DATE 10-1-65  
DESIGNED BY W.G. ZOLLER  
INKED BY W.G. ZOLLER

PROFILE  
NOTED BOOK PORTER, C.H. DIX  
No. 78671  
DATE 10-1-65  
DESIGNED BY W.G. ZOLLER  
INKED BY W.G. ZOLLER



ALL R/W TO BE AS SHOWN. LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED. POINTS OF ACCESS TO BE AS SHOWN Sta. 154+20.3 Line B' Epler Ave Lt. & Rt.

B.M. NO. 5 Elev. 730.30 Boat Spike in 30" Oak 295' Lt. of Sta. 153+35  
B.M. NO. 6 Elev. 727.42 Boat Spike in 24" Oak 25' Lt. of Sta. 154+83  
B.M. NO. 7 Elev. 718.81 Boat Spike in 16" Oak 130' Lt. of Sta. 161+00

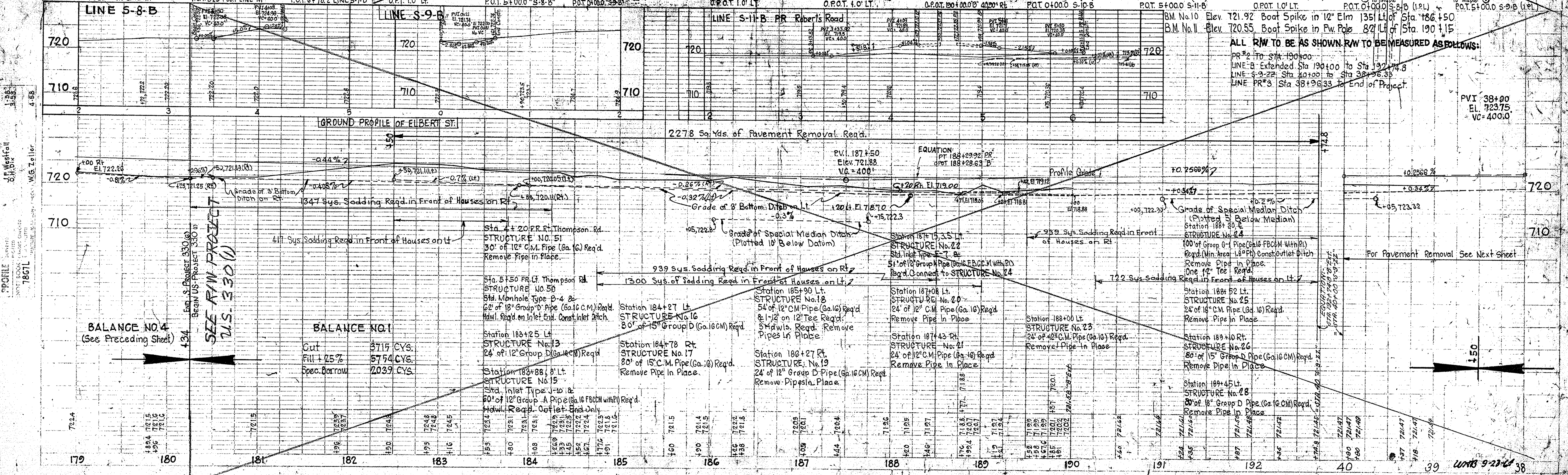
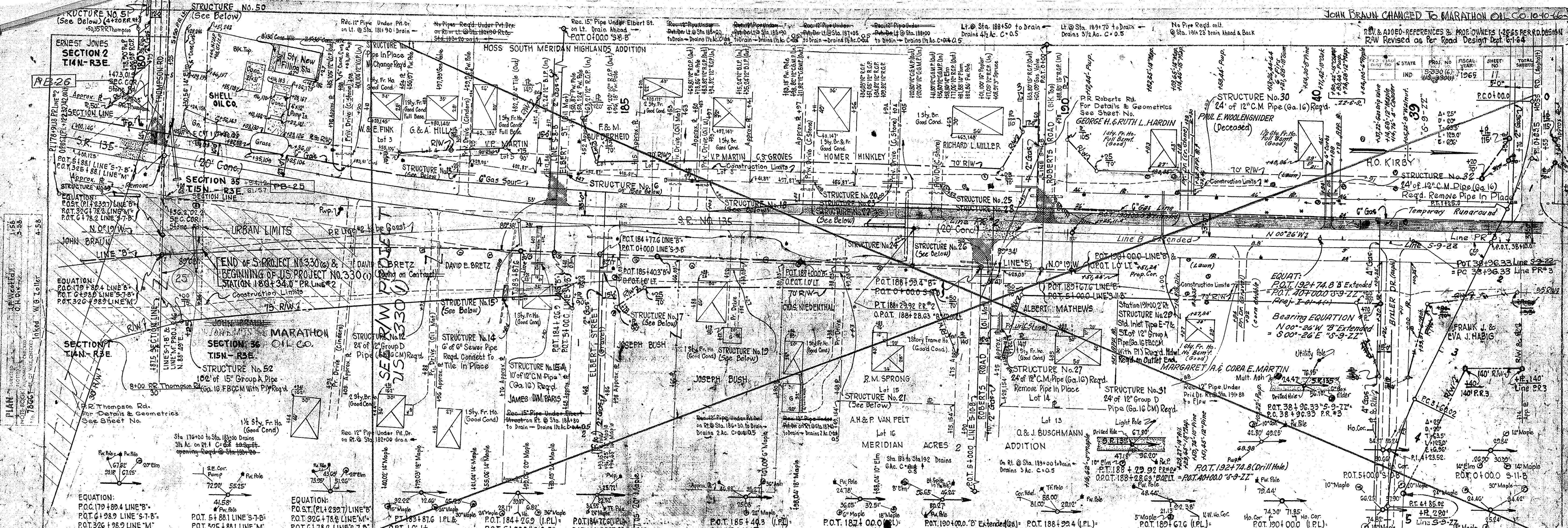
WETA 9-23-64







STATE	IND	PROJ. NO.	FISCAL YEAR	SHEET	TOTAL SHEETS
IND		330 (6)	1965	11	40



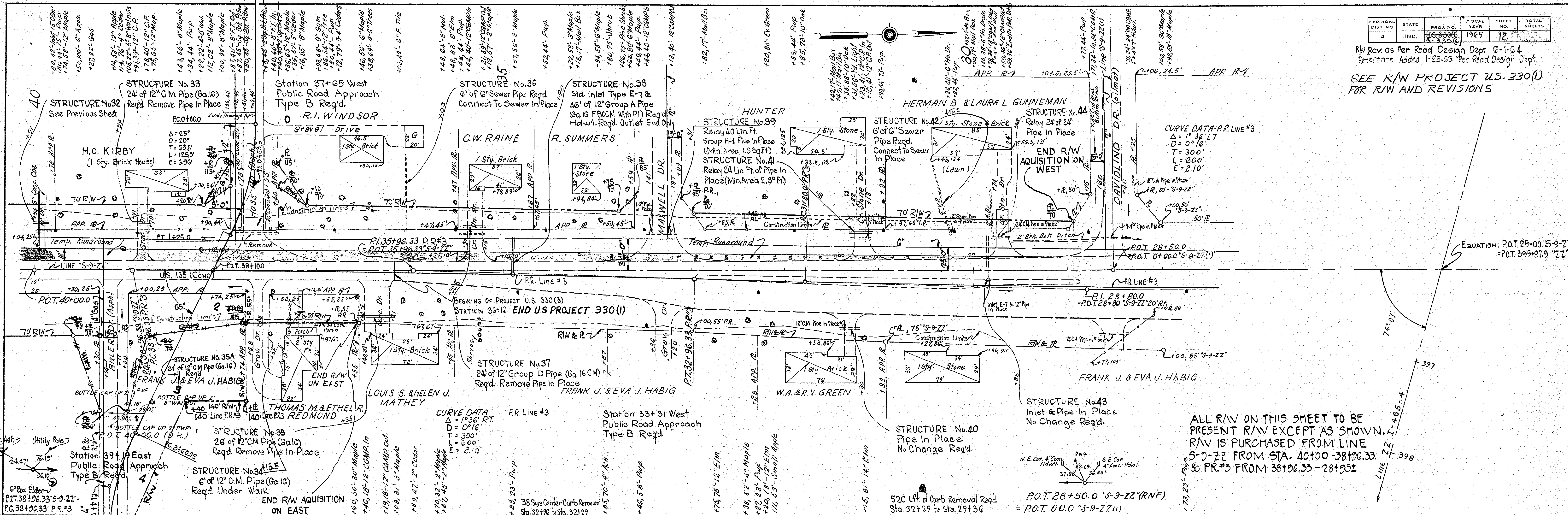


FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	15-330(4)	1965	12	

R/W ROW as Per Road Design Dept. G-1-G-4  
Reference Added 1-25-65 Per Road Design Dept.  
SEE R/W PROJECT U.S. 330(I)  
FOR R/W AND REVISIONS

PLAN	DATE	BY
SURVEYED	3/27/63	SHAWLEY
NOTED		DAWSON
GRADES CHECKED		DAWSON
STRUCTURE NOTATIONS CHECKED		DAWSON

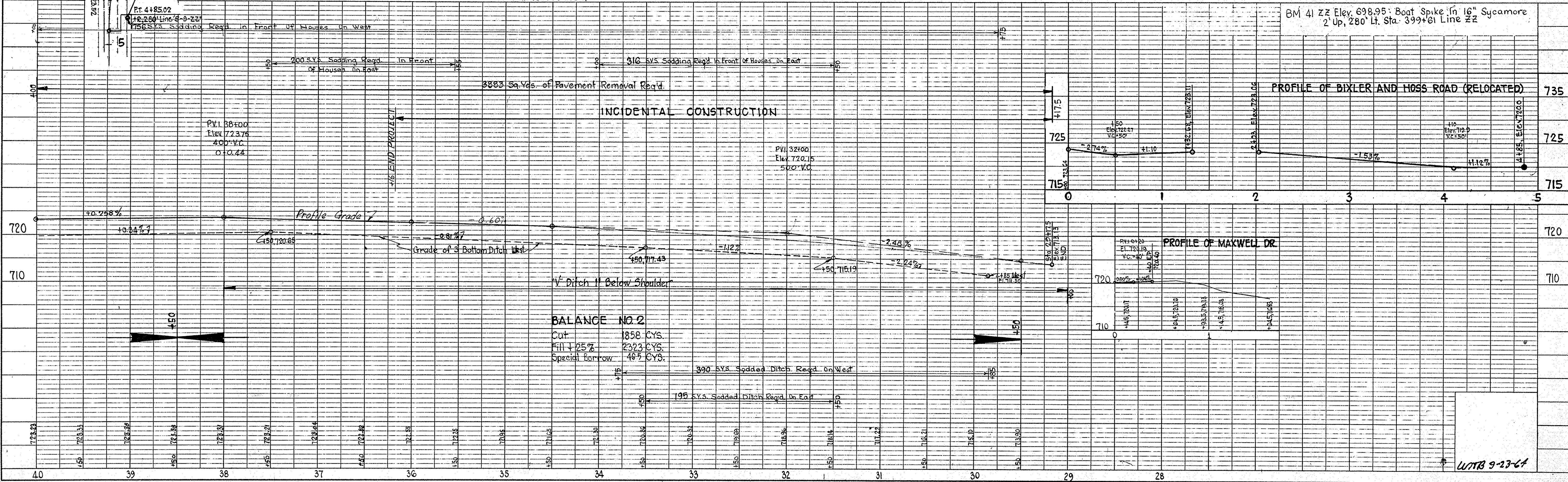
PROFILE	DATE	BY
SURVEYED	7/19/63	DAWSON
NOTED		DAWSON
GRADES CHECKED		DAWSON
STRUCTURE NOTATIONS CHECKED		DAWSON



CURVE DATA - P.R. LINE #3  
 $\Delta = 1^\circ 36' 17''$   
 $D = 0^\circ 16'$   
 $T = 300'$   
 $L = 600'$   
 $E = 2.10'$

EQUATION: P.O.T. 25+00 'S-9-ZZ'  
 = P.O.T. 395+97.9 'ZZ'

ALL R/W ON THIS SHEET TO BE  
PRESENT R/W EXCEPT AS SHOWN.  
R/W IS PURCHASED FROM LINE  
S-9-ZZ FROM STA. 40+00-38+06.33  
& P.R.#3 FROM 38+06.33-28+95

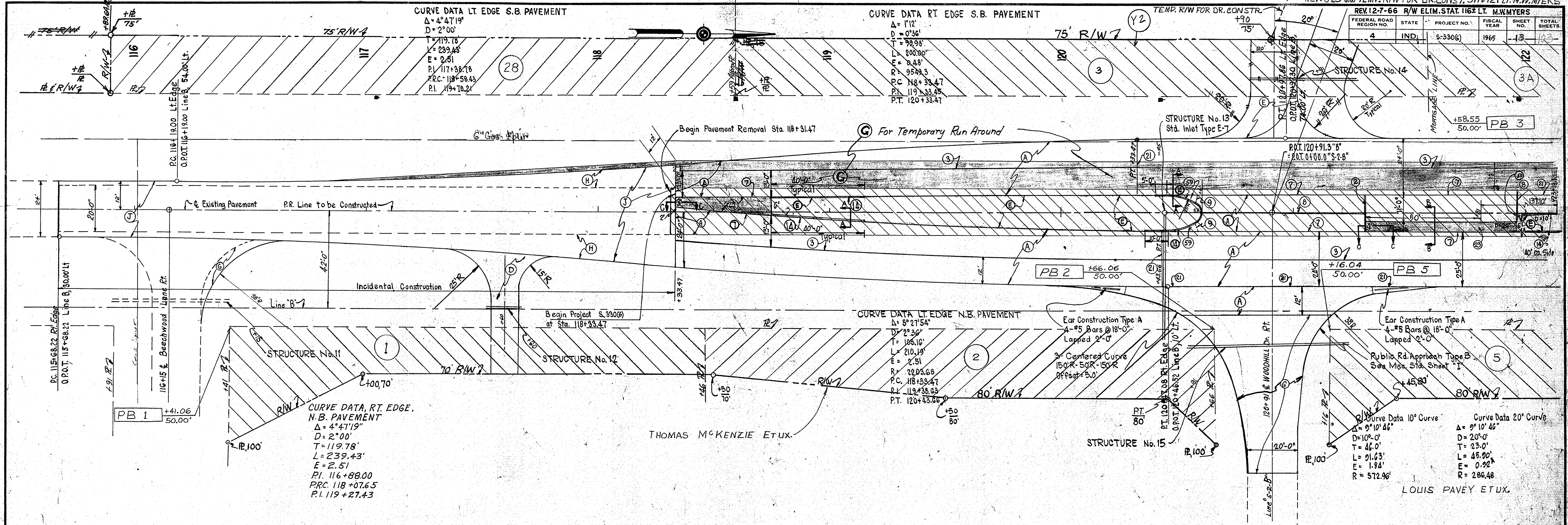


WTRB 9-23-64

PROJ.	LINE	SHEET	FILE
15-330(4)		12	

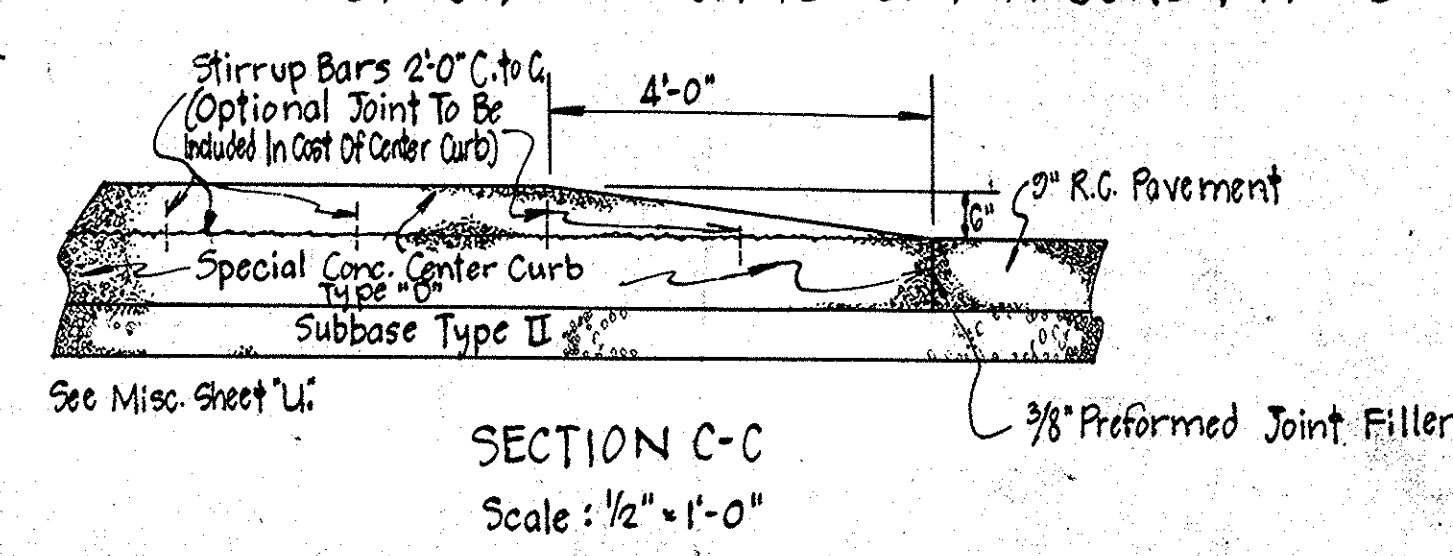
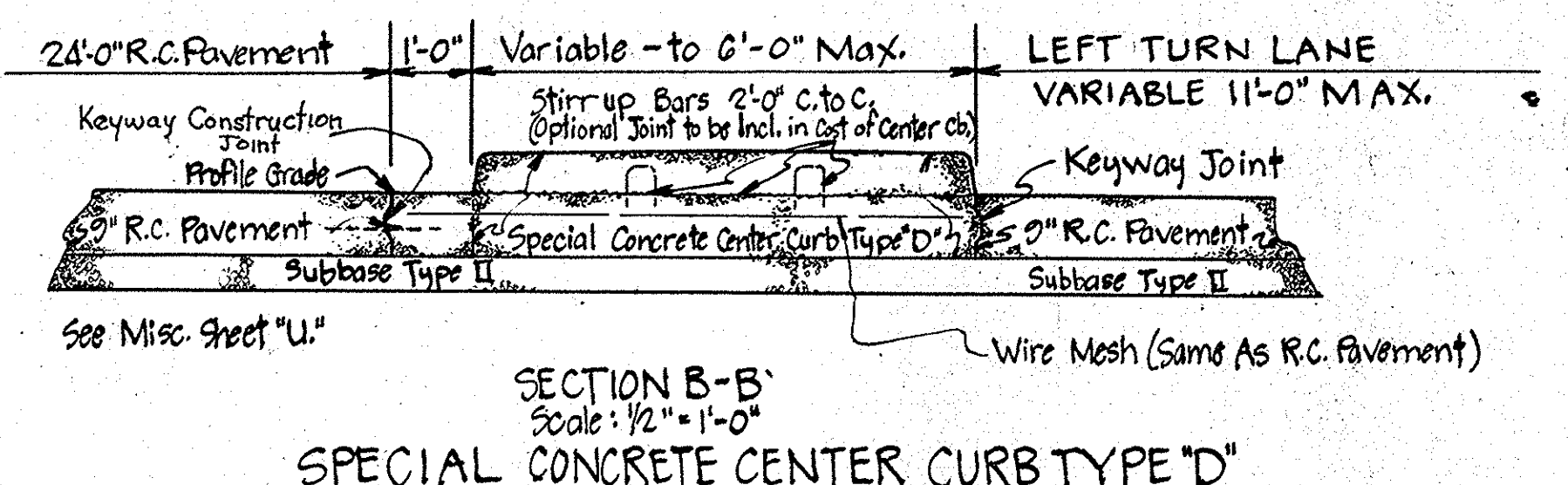
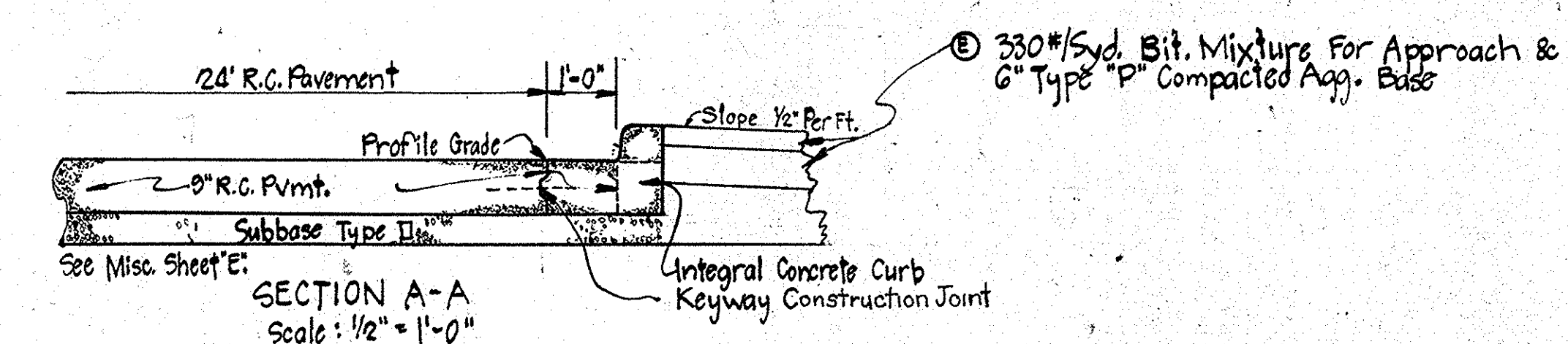


FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	S-330(6)	1965	13	13



THOMAS MCKENZIE ETUX.

LOUIS PAVEY ETUX.



330#/Sqd. Bit. Mixture For Approach & 6" Type "P" Compacted Agg. Base

- LEGEND**
- ① Contraction Joint
  - ② Longitudinal Joint
  - ③ Keyway Joint
  - ④ 1" Preformed Expansion Joint With Load Transfer
  - ⑤ 1" Preformed Joint Filler
  - ⑥ Integral Concrete Curb
  - ⑦ Keyway Construction Joint
  - ⑧ Special Concrete Center Curb Type "D"
  - ⑨ Ear Construction Type B-6 #5 Bars @ 19'-0" Lapped 2'-0"
  - ⑩ Ear Construction Type B-2 #5 Bars @ 18'-0" (Typ)
  - ⑪ 9" Standard Reinforced Concrete Pavement
  - ⑫ 330#/sqd. Bit. Mixture For Approach & 3" Type P Compacted Agg. Base
  - ⑬ 330#/sqd. Bit. Mixture For Approach & 6" Type "P" Compacted Agg. Base
  - ⑭ 330#/sqd. Bit. Mixture For Approach & 9" Type P Compacted Agg. Base
  - ⑮ 8" Concrete Base
  - ⑯ 330#/sqd. Bituminous Mixture For Approaches

▨ Pavement Removal

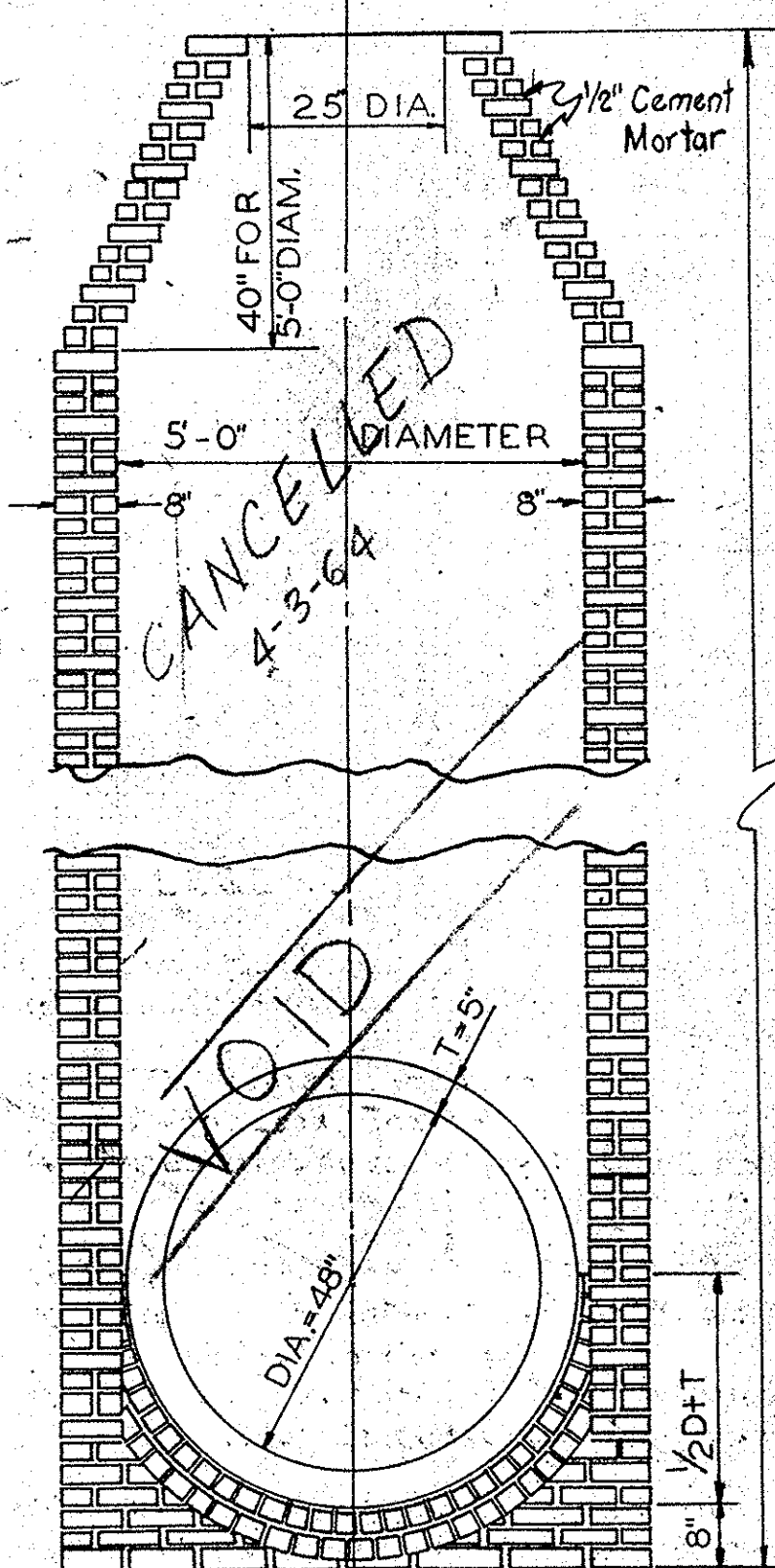
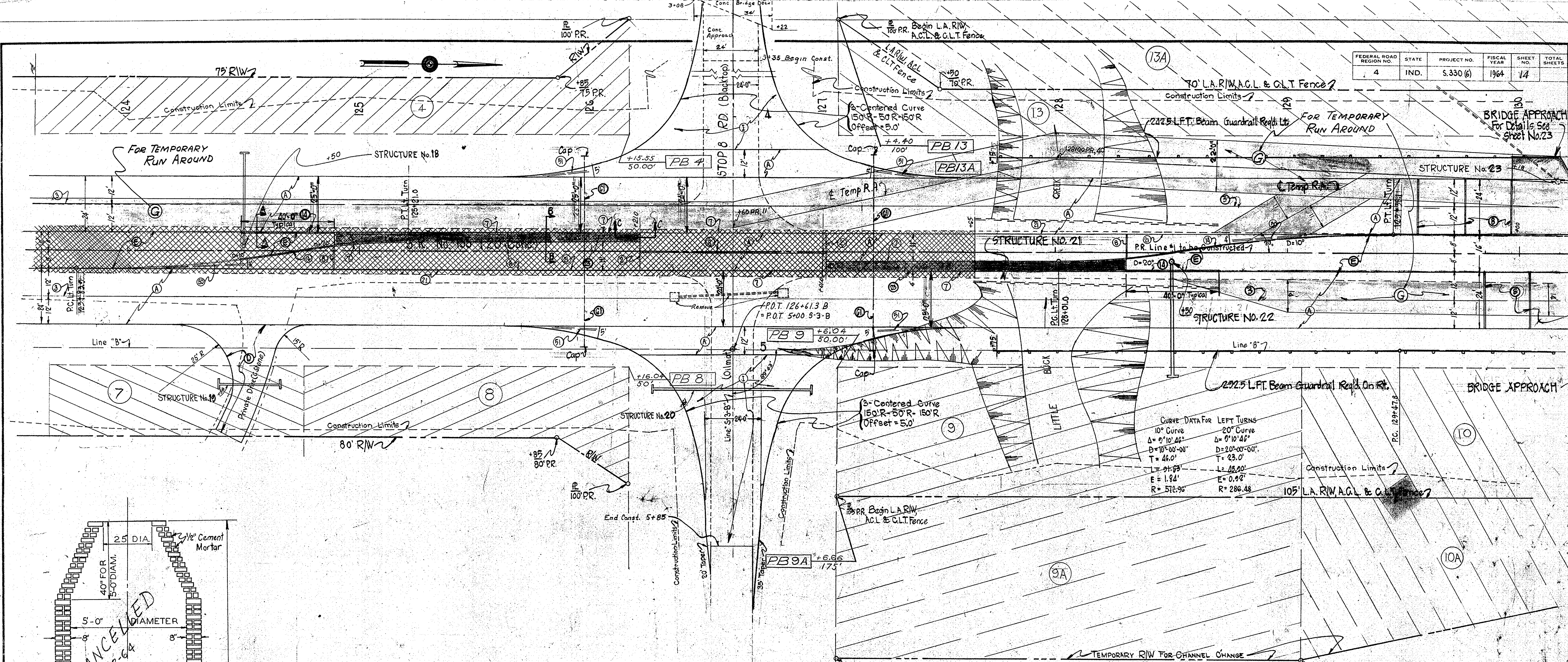
# WOODHILL DR. DETAILS

Scale: 1" = 20'-0"

WNB 9-23-64



FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	S.330 (6)	1964	14	



- LEGEND**
- ⊕ Contraction Joint
  - ⊙ Longitudinal Joint
  - ⊖ Keyway Joint
  - ⊗ Prefomed Expansion Joint With Load Transfer
  - ⊘ Integral Concrete Curbs
  - ⊙ Keyway Construction Joint
  - ⊚ Special Concrete Center Curb Type "D"
  - ⊛ Ear Construction Type B'-C'-#5 Bars @ 19'-0" Lapped 2'-0"
  - ⊜ Ear Construction Type A-4-#5 Bars @ 18'-0" Lapped 2'-0"
  - ⊝ 12" Conduit
  - ⊞ 9" Standard Reinforced Concrete Pavement
  - ⊟ 330# Syd. Bit. Mixt. for Approach & 3" Type "P" Compacted Agg. Base
  - ⊠ 440# Syd. Bit. Mixt. for Approach & 9" Type "P" Compacted Agg. Base
  - ⊡ 330# Syd. Bit. Mixt. for Approach & 6" Type "P" Comp. Agg. Base
  - ⊢ Pavement Removal
  - ⊣ Pavement Breaking

Note:  
For Sections A-A, B-B & C-C  
See Sheet No. 13

# STOP 8 RD. DETAILS

Scale: 1"=20'-0"

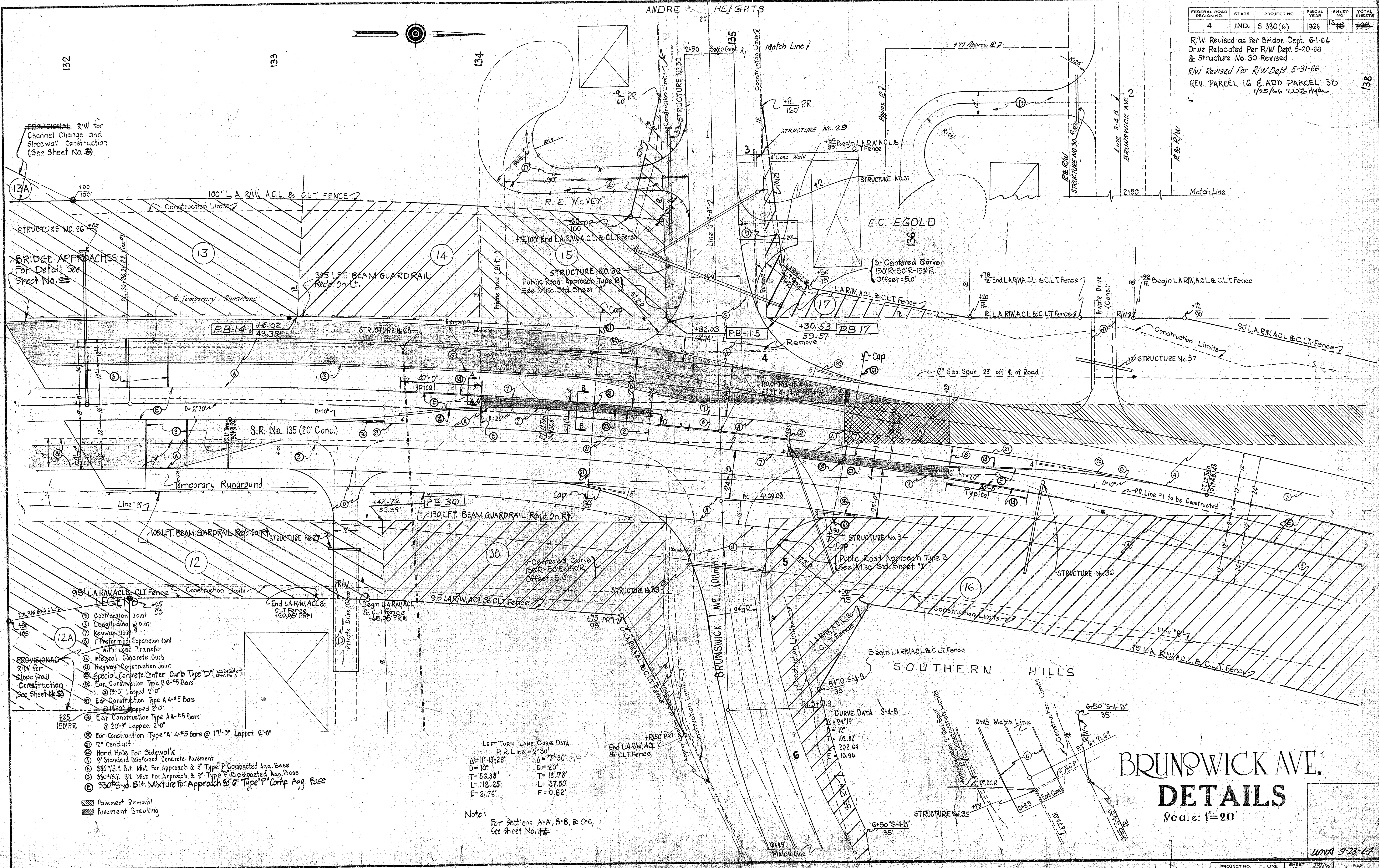
SCALE: 1/2"=1'-0"  
NOTE:  
FOR DETAILS NOT SHOWN  
SEE STD. MANHOLE TYPE "A"

PROJECT NO.	LINE	SHEET	TOTAL SHEETS	FILE
S.330 (6)		14		



FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	S 330(6)	1965	15	16

R/W Revised as Per Bridge Dept. 6-1-64  
Drive Relocated Per R/W Dept 5-20-66  
& Structure No. 30 Revised.  
R/W Revised Per R/W Dept. 5-31-66.  
REV. PARCEL 16 & ADD PARCEL 30  
1/25/66 RWS/HJA



PROVISIONAL R/W for Channel Change and Slope Wall Construction (See Sheet No. 3)

BRIDGE APPROACHES For Detail See Sheet No. 3

- LEGEND**
- ① Contraction Joint
  - ② Longitudinal Joint
  - ③ Keyway Joint
  - ④ Prefabricated Expansion Joint With Lead Transfer
  - ⑤ Integral Concrete Curb
  - ⑥ Keyway Construction Joint
  - ⑦ Special Concrete Center Curb Type "D"
  - ⑧ Ear Construction Type B 8-#5 Bars @ 19'-0" Lapped 2'-0"
  - ⑨ Ear Construction Type A 4-#5 Bars @ 15'-0" Lapped 2'-0"
  - ⑩ Ear Construction Type A 4-#5 Bars @ 20'-9" Lapped 2'-0"
  - ⑪ 2" Conduit
  - ⑫ Hand Hole For Sidewalk
  - ⑬ 9" Standard Reinforced Concrete Parapet
  - ⑭ 530#/S.Y. Bit. Mixt. For Approach & 3" Type "P" Compacted Agg. Base
  - ⑮ 330#/S.Y. Bit. Mixt. For Approach & 9" Type "P" Compacted Agg. Base
  - ⑯ 330#/S.Y. Bit. Mixture For Approach & 6" Type "P" Comp. Agg. Base
- Pavement Removal  
 Pavement Breaking

LEFT TURN LANE CURVE DATA  
P.R. Line = 2'-30"

Station	Delta	D	T	L	E
11+53.28	7°30'	10'	18.78'	112.25'	2.76'
7+30'	7°30'	20'	18.78'	37.50'	0.62'

Note:  
For Sections A-A, B-B, & C-C,  
See Sheet No. 3

# BRUNSWICK AVE. DETAILS

Scale: 1"=20'

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
S 330 (6)		15	16	

UNTS 9-23-61



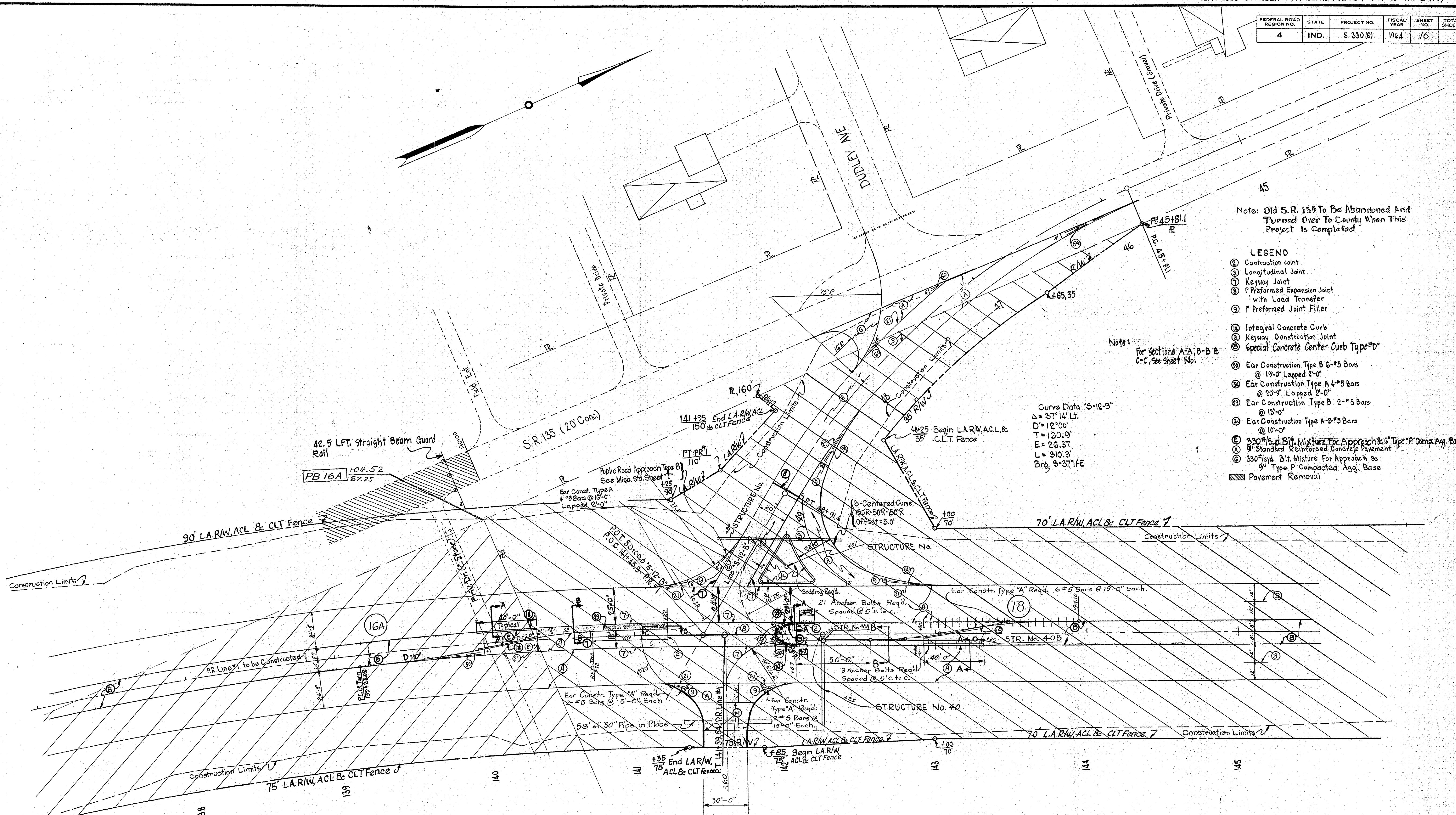
FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	S. 330 (S)	1964	16	

Note: Old S.R. 135 To Be Abandoned And Turned Over To County When This Project Is Completed

- LEGEND**
- ② Contraction Joint
  - ③ Longitudinal Joint
  - ④ Keyway Joint
  - ⑤ Preformed Expansion Joint with Load Transfer
  - ⑥ Preformed Joint Filler
  - ⑦ Integral Concrete Curb
  - ⑧ Keyway Construction Joint
  - ⑨ Special Concrete Center Curb Type "D"
  - ⑩ Ear Construction Type B 6-#5 Bars @ 19'-0" Lapped 2'-0"
  - ⑪ Ear Construction Type A 4-#5 Bars @ 20'-9" Lapped 2'-0"
  - ⑫ Ear Construction Type B 2-#5 Bars @ 13'-0"
  - ⑬ Ear Construction Type A 2-#5 Bars @ 10'-0"
  - ⑭ 330# Syd. Bit. Mixture For Approach & Type "P" Comp. Agg. Base
  - ⑮ 9" Standard Reinforced Concrete Pavement II
  - ⑯ 330# Syd. Bit. Mixture For Approach & 9" Type P Compacted Agg. Base
  - ⑰ Pavement Removal

Note: For Sections A-A, B-B & C-C, See Sheet No.

Curve Data "S-12-B"  
 $\Delta = 37^{\circ}14'$  LT.  
 $D = 12^{\circ}00'$   
 $T = 160.9'$   
 $E = 26.87'$   
 $L = 310.3'$   
 $Brg. S-37^{\circ}14'E$



CURVE DATA PR#1  
 $\Delta = 23^{\circ}20'$  RT.  
 $D = 2^{\circ}30'$   
 $T = 473.25'$   
 $L = 933.33'$   
 $E = 48.36'$

LEFT TURN LANE CURVE DATA  
 PR. Line = 2'-30"  
 $\Delta = 11^{\circ}13'28''$   
 $D = 10^{\circ}00''$   
 $T = 56.39'$   
 $L = 112.25'$   
 $E = 2.76'$

LEFT TURN LANE CURVE DATA  
 PR. LINE #1 STA. 142+57 TO STA. 143+94.10  
 $\Delta = 9^{\circ}10'46''$   
 $D = 10^{\circ}00''$   
 $T = 46.00'$   
 $L = 91.63'$   
 $E = 01.84'$

# LINE "S-12-B"

## DETAILS

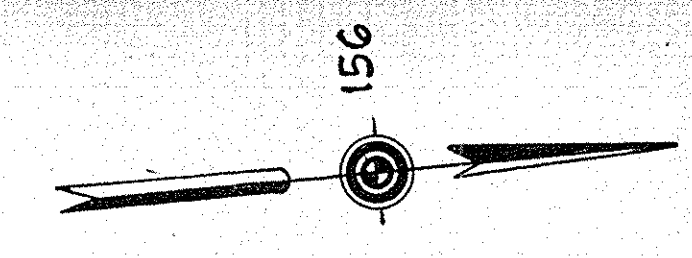
Scale: 1"=30'-0"

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
US 330 (S)	PR#1	16	16	

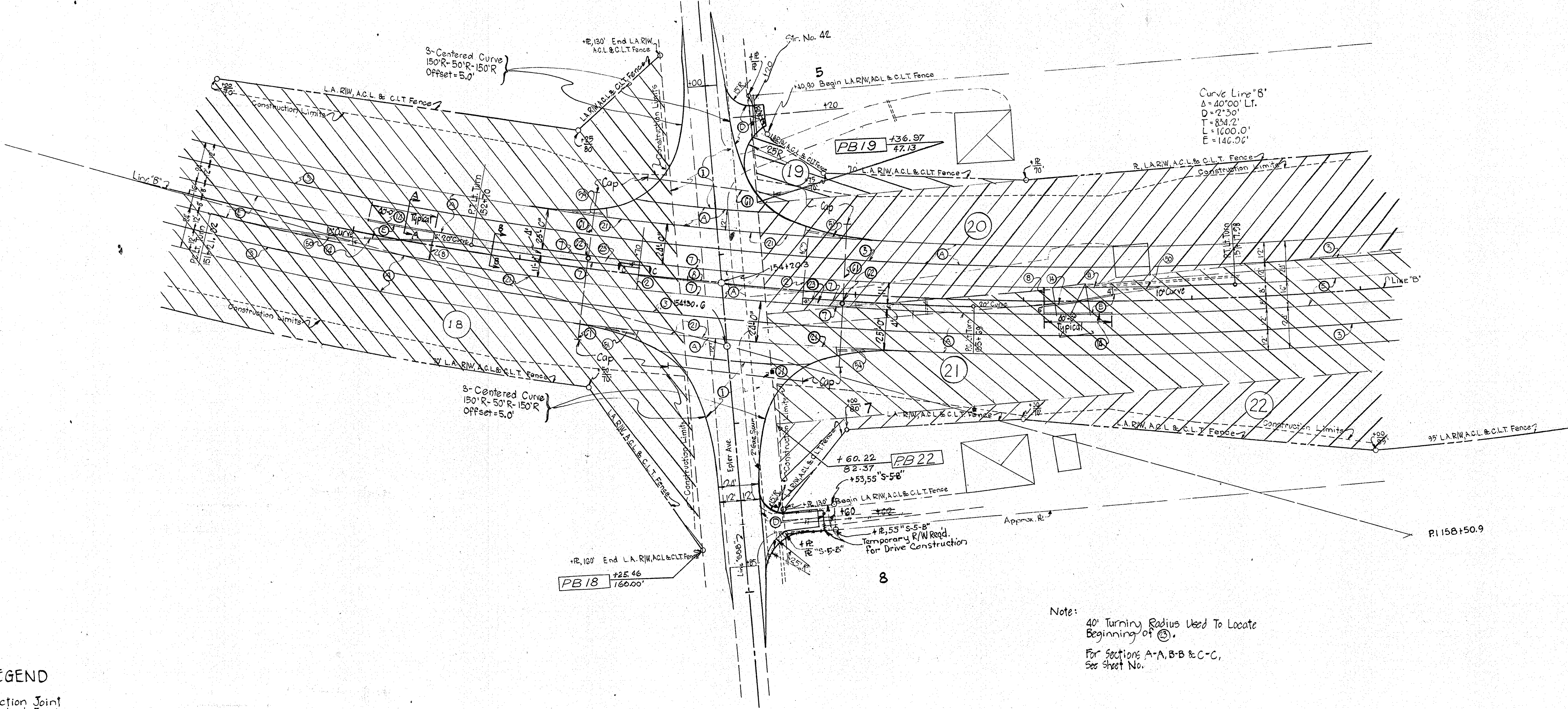


FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	5330(6)	1965	17	

Revised 8-7-66: Revised Dr. Appr. + Added Temp. R/W Lt.  
 Station 7+00 "S-5-B". Per R/W Dept.



151      152      153      154      155      156      157      158



Curve Line "B"  
 $\Delta = 40^{\circ}00'$  LI.  
 $D = 2^{\circ}30'$   
 $T = 834.2'$   
 $L = 1600.0'$   
 $E = 146.06'$

3-Centered Curve  
 $150^{\circ}R - 50^{\circ}R - 150^{\circ}R$   
 Offset = 5.0'

Lt. Turn Lane Curve Data  
 Line "B" = 2°30'  
 $\Delta = 11^{\circ}13'28''$        $\Delta = 7^{\circ}30'$   
 $D = 10^{\circ}00'00''$        $D = 20^{\circ}00'00''$   
 $T = 56.39'$        $T = 19.78'$   
 $L = 112.25'$        $L = 37.50'$   
 $E = 2.76'$        $E = 0.02'$

Note:  
 40' Turning Radius Used To Locate Beginning of (3).  
 For sections A-A, B-B & C-C, See sheet No.

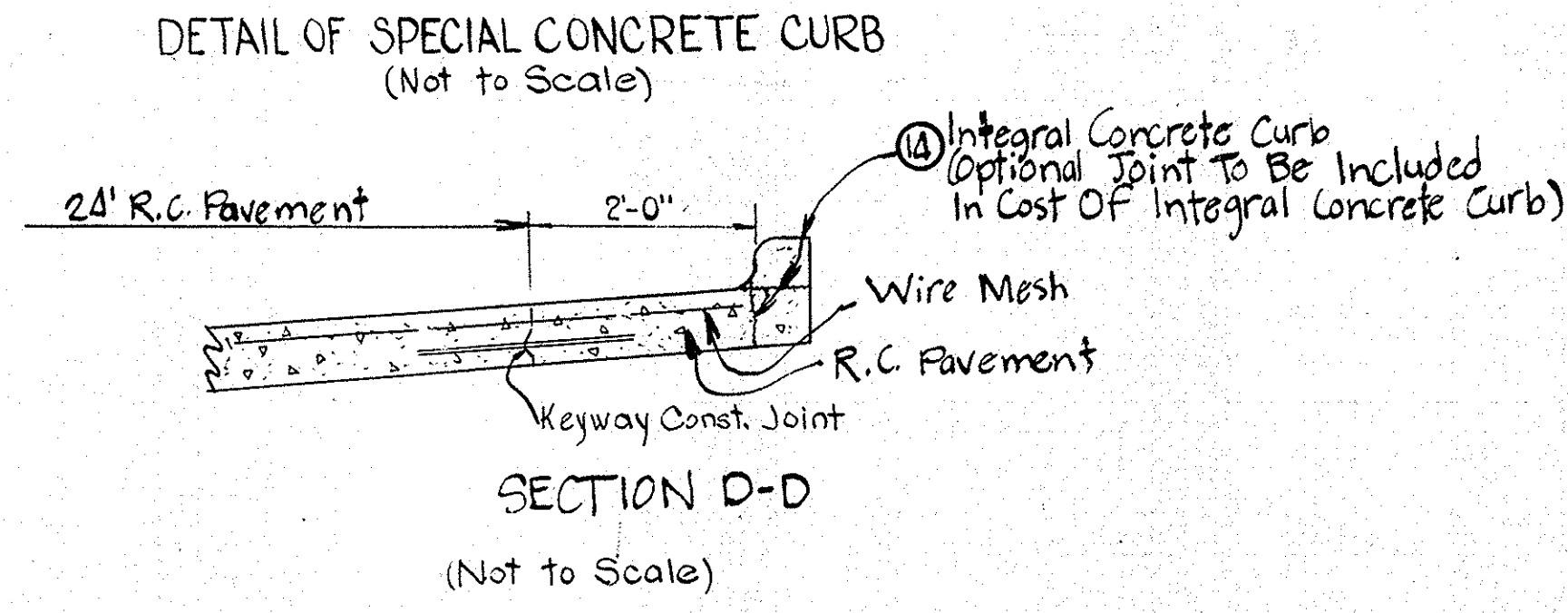
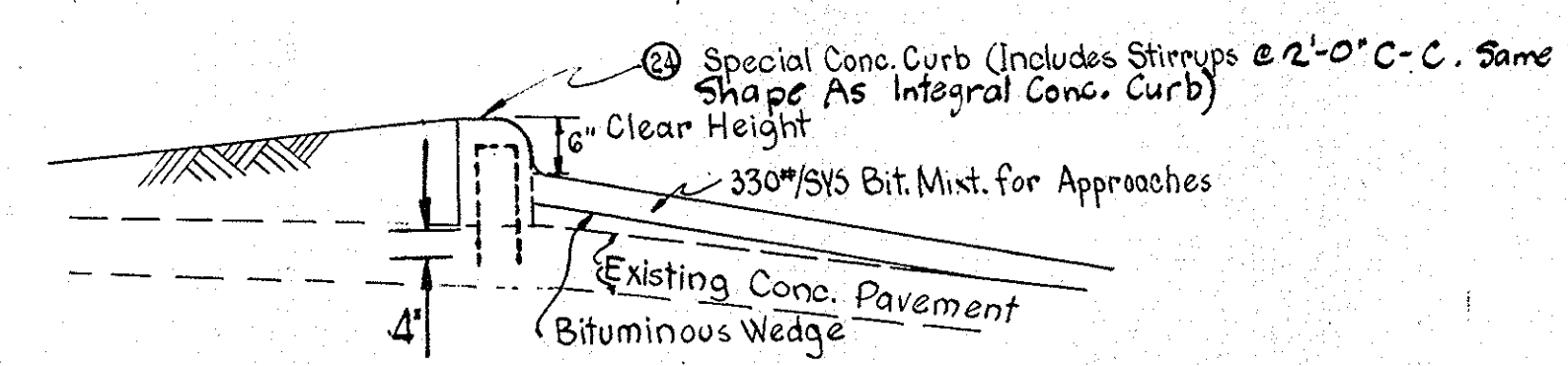
**LEGEND**

- (C) Contraction Joint
- (L) Longitudinal Joint
- (K) Keyway Joint
- (E) 1" Prefabricated Expansion Joint w/ Load Transfer
- (I) Integral Concrete Curb
- (S) Keyway Construction Joint
- (D) Special Concrete Center Curb Type "D" (Detail on sheet No. 14)
- (B) Ear Construction Type "B" 6-#5 Bars @ 19'-0" Lapped 20"
- (A) Ear Construction Type "A" 4-#5 Bars @ 18'-0" Lapped 2'-0"
- (4) Ear Construction Type "A" 4-#5 Bars @ 20'-9" Lapped 2'-0"
- (R) 3" Standard Reinforced Concrete Pavement
- (3) 330#/SY. Bit. Mixt. For Approach & 3" Type "P" Compacted Agg. Base
- (5) 530#/SY. Bit. Mixt. For Approach & 5" Type "P" Compacted Agg. Base
- (4) 440#/SY. Bit. Mixt. For Approach & 4" Type "P" Compacted Agg. Base
- (3) 330#/SY. Bit. Mixt. For Approach & 3" Type "P" Compacted Agg. Base
- (C) 2" Conduit
- (M) Man Hole For sidewalk

Epler Ave.  
 Sta. 154+20.3  
**DETAILS**  
 Scale: 1"=30'-0"

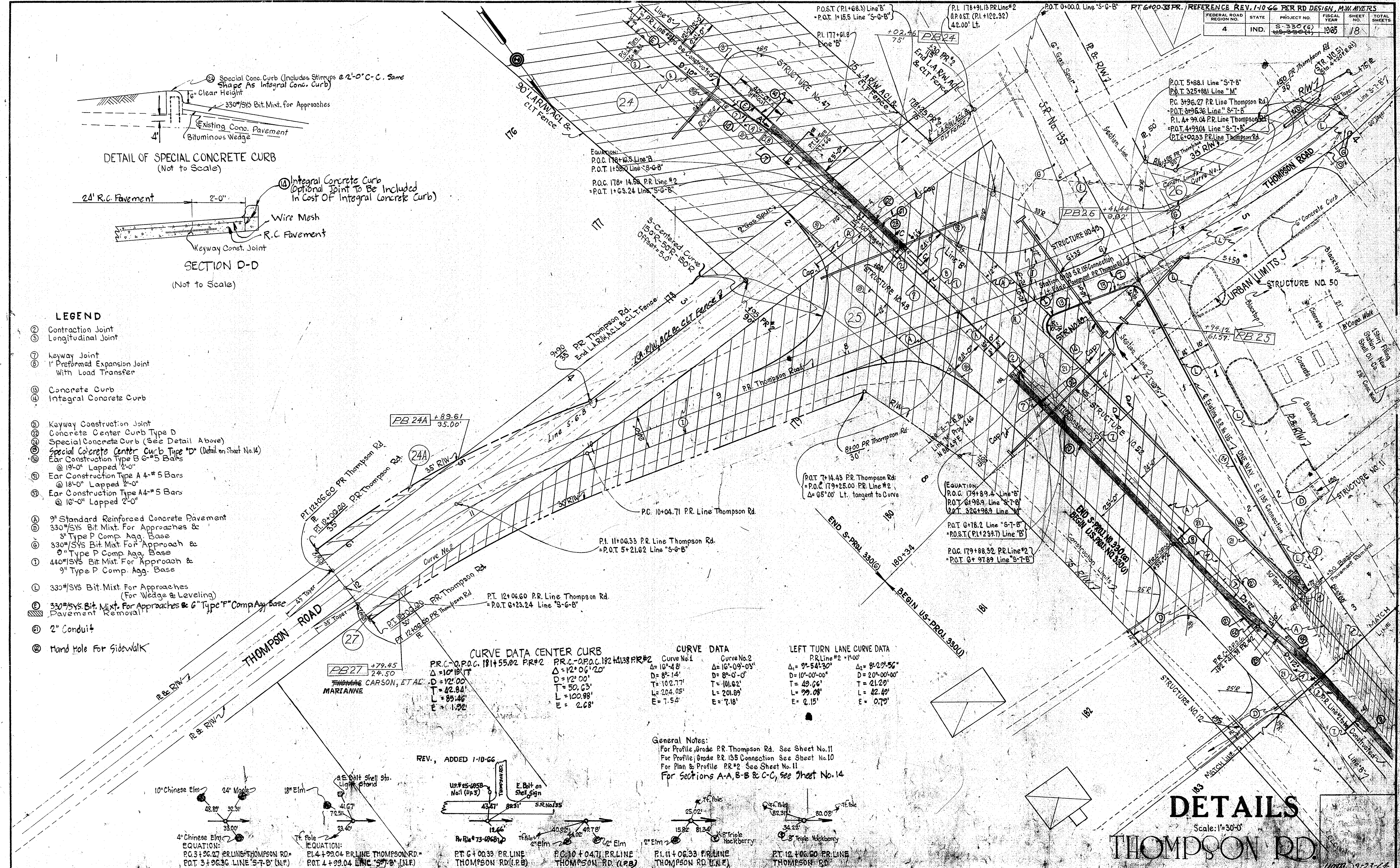


FEDERAL ROAD DISTRICT NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	S-330(6)	1965	18	



LEGEND

- ② Contraction Joint
- ③ Longitudinal Joint
- ⑦ Keyway Joint
- ⑧ 1" Preformed Expansion Joint With Load Transfer
- ⑩ Concrete Curb
- ⑭ Integral Concrete Curb
- ⑳ Keyway Construction Joint
- ㉑ Concrete Center Curb Type D
- ㉒ Special Concrete Curb (See Detail Above)
- ㉓ Special Concrete Curb Type "D" (Detail on Sheet No.14)
- ㉔ Ear Construction Type B 6-#5 Bars @ 19'-0" Lapped 2'-0"
- ㉕ Ear Construction Type A 4-#5 Bars @ 18'-0" Lapped 2'-0"
- ㉖ Ear Construction Type A4-#5 Bars @ 18'-0" Lapped 2'-0"
- ㉗ 9" Standard Reinforced Concrete Pavement
- ㉘ 330#/5YS Bit. Mixt. For Approaches & 3" Type P Comp. Agg. Base
- ㉙ 330#/5YS Bit. Mixt. For Approach & 0" Type P Comp. Agg. Base
- ㉚ 440#/5YS Bit. Mixt. For Approach & 9" Type P Comp. Agg. Base
- ㉛ 330#/5YS Bit. Mixt. For Approaches (For Wedge & Leveling)
- ㉜ 330#/5YS Bit. Mixt. For Approaches & 6" Type P Comp. Agg. Base
- ㉝ Pavement Removal
- ㉞ 2" Conduit
- ㉟ Hand Hole For Sidewalk



**CURVE DATA CENTER CURB**

Curve No.1	Curve No.2
Δ = 10° 48'	Δ = 10° 09' 05"
D = 8° 14'	D = 8° 0' 0"
T = 102.77'	T = 101.62'
L = 204.05'	L = 201.89'
E = 7.54'	E = 7.18'

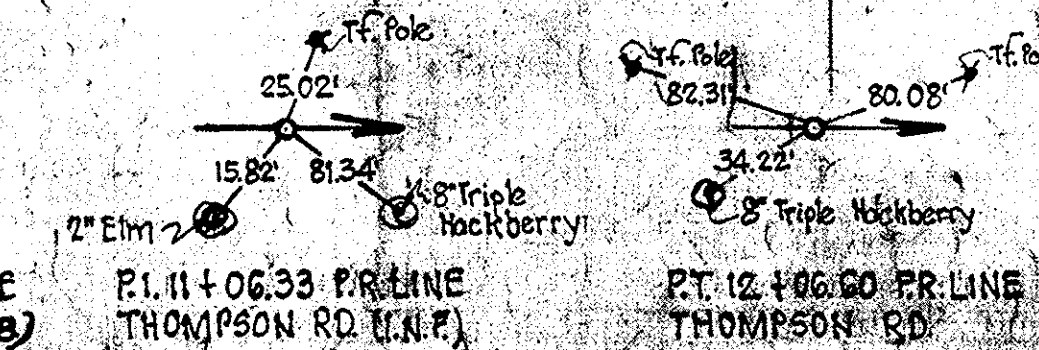
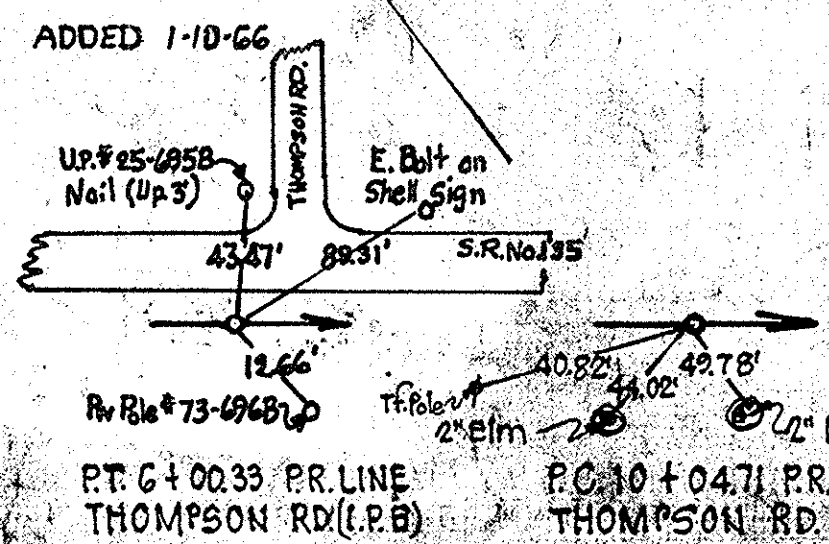
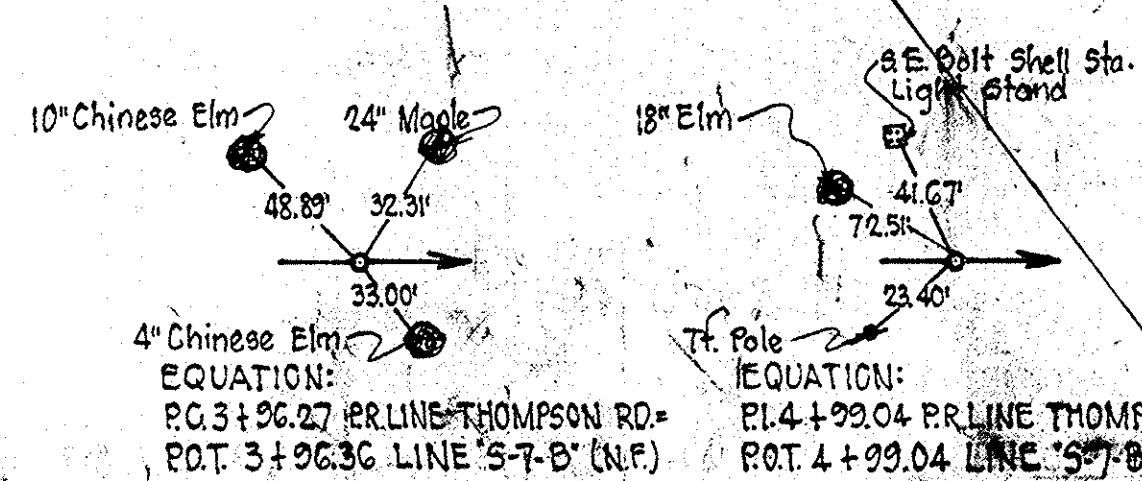
**CURVE DATA**

Curve No.1	Curve No.2
Δ = 10° 48'	Δ = 10° 09' 05"
D = 8° 14'	D = 8° 0' 0"
T = 102.77'	T = 101.62'
L = 204.05'	L = 201.89'
E = 7.54'	E = 7.18'

**LEFT TURN LANE CURVE DATA**

PR Line #2 - 11° 00'	PR Line #2 - 11° 00'
Δ = 9° 54' 30"	Δ = 8° 22' 56"
D = 10° 00' 00"	D = 20° 00' 00"
T = 49.66'	T = 21.29'
L = 99.09'	L = 42.49'
E = 2.15'	E = 0.79'

**General Notes:**  
 For Profile, Grade PR Thompson Rd. See Sheet No.11  
 For Profile, Grade PR. 135 Connection See Sheet No.10  
 For Plan & Profile PR.#2 See Sheet No.11  
 For Sections A-A, B-B & C-C, See Sheet No.14



**DETAILS**  
 Scale: 1"=30'-0"  
**THOMPSON RD.**

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
US-330(6)		18		



















Table with columns: STRUCTURE NUMBER, LOCATION, DESCRIPTION, LENGTH, SKEW, COVER, FLOW LINE (UP/DOWN STREAM ELEV.), CONCRETE CLASS, SPECIAL BORROW GRADE, REINFORCING STEEL, REMARKS, PLANS ON SHEET NO.

Table with columns: STRUCTURE NUMBER, LOCATION, DESCRIPTION, LENGTH, SKEW, COVER, FLOW LINE (UP/DOWN STREAM ELEV.), CONCRETE CLASS, SPECIAL BORROW GRADE, REINFORCING STEEL, REMARKS, PLANS ON SHEET NO.