

INDEX

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	SECTION	SHEET NO.	TOTAL SHEETS
7	IND.	377	1940	1	62

SECTION: C(3)

**STATE OF INDIANA
STATE HIGHWAY COMMISSION**

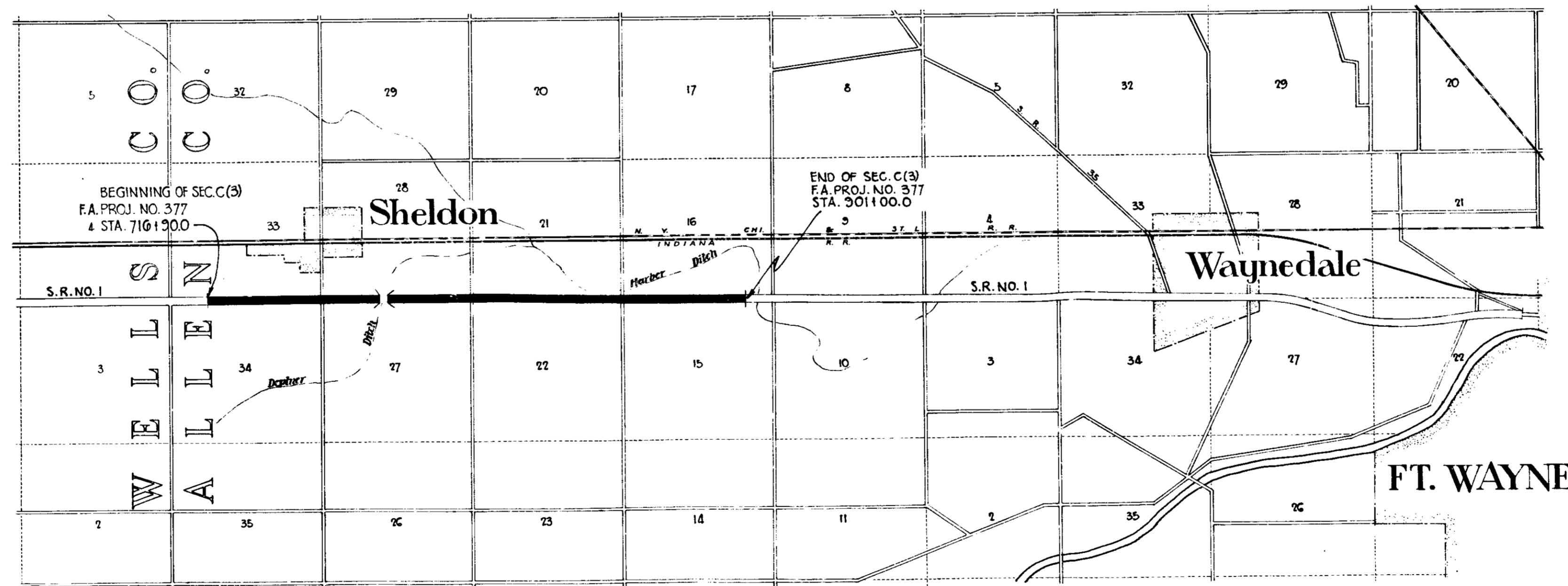
**PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY
F. A. PROJECT NO. 377 SEC. C(3) (1940)
BLUFFTON - FT. WAYNE ROAD**

BEGINNING AT A POINT APPROX. 1296' NORTH OF THE WELLS-ALLEN CO. LINE AND RUNNING IN A NORTHERLY DIRECTION ON S.R. NO. 1 TO A POINT APPROX. 1464' SOUTH OF THE NORTHWEST CORNER OF SECTION 15, T. 29 N., R. 12 E. IN ALLEN COUNTY.

GROSS LENGTH:- 3.486 MI.
NET LENGTH:- 3.452 MI.

SCALES:-
PLAN LONG:- 1"=100' TRANS:- 1"=100' PROFILE HORIZ:- 1"=100' VERT:- 1"=10'
MAX. GRADE 1.40%

SCALE:- 1"=3000'



APPROVED AND ADOPTED 7-21-39
BY STATE HIGHWAY COMMISSION OF INDIANA

J. A. Dixon
CHAIRMAN STATE HIGHWAY COMMISSION OF INDIANA

APPROVED 7-21-39
W. H. Keefe
CHIEF ENGINEER STATE HIGHWAY COMMISSION OF INDIANA

RECOMMENDED FOR APPROVAL
DISTRICT ENGINEER
PUBLIC ROADS ADMINISTRATION
FEDERAL WORKS AGENCY

APPROVED
COMMISSIONER
PUBLIC ROADS ADMINISTRATION
FEDERAL WORKS AGENCY

RECOMMENDED FOR APPROVAL 7-21-39

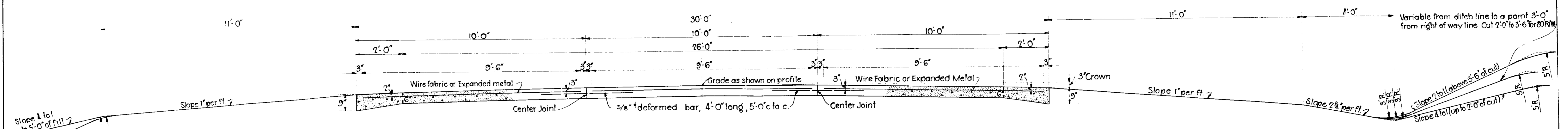
C. E. Vogelgesang
ENGINEER OF ROAD SECTION STATE HIGHWAY COMMISSION OF INDIANA

SHEET NO. 1	TITLE SHEET
SHEET NO. 2, 3	TYPICAL CROSS SECTIONS
SHEET NO. 4	STANDARD CROSS SECTIONS
SHEET NO. 5, 6	STD. EXPANSION JOINTS REV. MAY 8, 1939, JAN. 1, 1940
SHEET NO. 7-14	STD. PAVEMENT JOINTS JUNE 1939 REV. MAY 8, 1939, REV. JAN. 1, 1940 REV. M. 1, 1940
SHEET NO. 15	PLAN AND PROFILE
SHEET NO. 16, 17, 18	BRIDGE AND CULVERT DATA - ESTIMATE OF QUANTITIES
SHEET NO. 19, 20	MISCELLANEOUS STANDARDS SHEET A OCT. 1937 SHEET B REV. APRIL 9, 1939 SHEET C REV. FEB. 27, 1939
SHEET NO. 21	MISCELLANEOUS STANDARDS SHEET D REV. FEB. 27, 1939 SHEET E REV. JULY 25, 1939 AUG. 13, 1939
SHEET NO. 22	STD. REINF. CONC. CULV. - SLAB TOP TYPE UNDER FILL (10'-0" TO 18'-0" SPAN) FEB. 8, 1939
SHEET NO. 23	STD. REINF. CONC. CULV. - SLAB TOP TYPE WITHOUT FILL (10'-0" TO 18'-0" SPAN)
SHEET NO. 24	STD. REINF. CONC. CULV. - SLAB AND GIRDER WITHOUT FILL (20'-0" SPAN)
SHEET NO. 25	STD. REINF. CONC. CULV. - SLAB TOP TYPE WITHOUT FILL (10'-0" TO 20'-0" SPAN) 15' SKEW
SHEET NO. 26	STD. REINF. CONC. CULV. - SLAB TOP TYPE UNDER FILL (10'-0" TO 20'-0" SPAN) 15' SKEW REV. FEB. 8, 1939
SHEET NO. 27	STD. REINF. CONC. CULV. - SLAB TOP TYPE WITHOUT FILL (10'-0" TO 20'-0" SPAN) 30' SKEW
SHEET NO. 28	STD. REINF. CONC. CULV. - SLAB TOP TYPE UNDER FILL (10'-0" TO 20'-0" SPAN) 30' SKEW
SHEET NO. 29	STD. REINF. CONC. CULV. - SLAB TOP TYPE WITHOUT FILL (10'-0" TO 20'-0" SPAN) 45' SKEW
SHEET NO. 30	STD. REINF. CONC. CULV. - SLAB TOP TYPE UNDER FILL (10'-0" TO 20'-0" SPAN) 45' SKEW
SHEET NO. 31	STD. REINF. CONC. CULV. - SLAB TOP TYPE (5'-0" TO 10'-0" SPAN) GRAVITY TYPE
SHEET NO. 32	STD. REINF. CONC. CULV. - SLAB TOP TYPE (12'-0" TO 20'-0" SPAN) GRAVITY TYPE
SHEET NO. 33	STD. FLEXIBLE STEEL PLATE GUARD RAIL JAN. 1938
SHEET NO. 34	STD. RAILROAD GRAVE CROSSING
SHEET NO. 35	STD. STRUCTURE CONNECTIONS FOR EXTENSIONS MAY 1934
SHEET NO. 36	STD. PLAN FOR FILLING IN PEAT MARSHES
SHEET NO. 37	DATA FOR SUPER-ELEVATING AND WIDENING OF CURVES
SHEET NO. 38	STD. DETOUR SIGNS SHEET A REV. JULY 25, 1939 SHEET B REV. APRIL 27, 1939
SHEET NO. 39-62	CROSS SECTIONS

STATE HIGHWAY COMMISSION OF INDIANA.
STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION.
DATED MARCH, 1935, AND SUPPLEMENT DATED JUNE, 1936, ON FILE
WITH U. S. BUREAU OF PUBLIC ROADS TO BE USED WITH THESE PLANS.

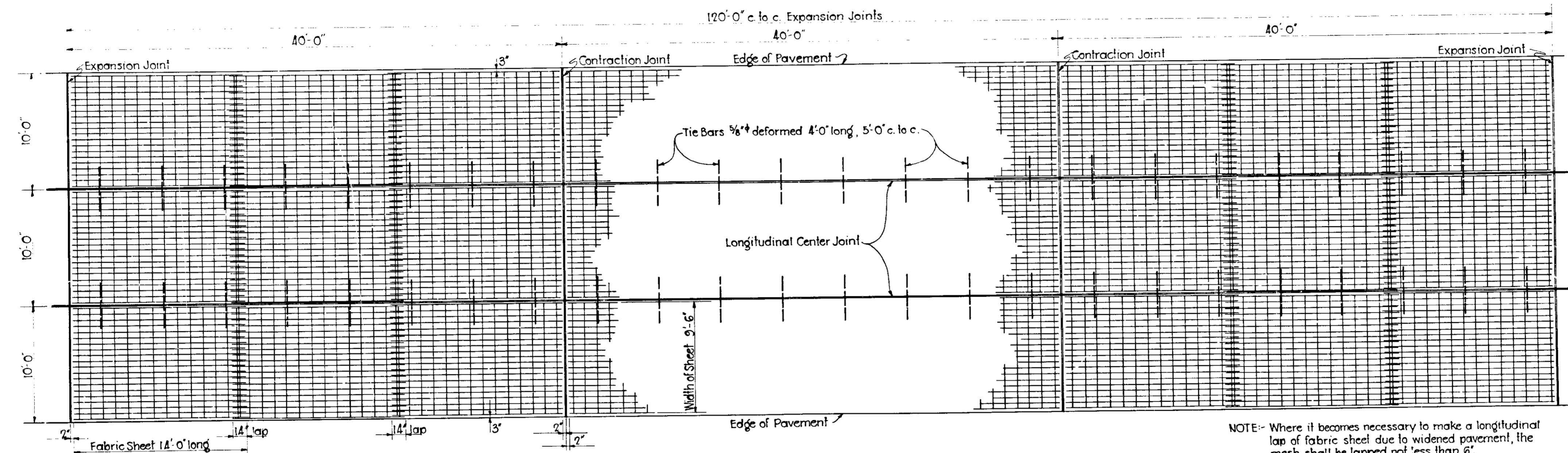
Copyright, 1939, by the State of Indiana, Department of Public Works, Bureau of Public Roads, Indianapolis, Indiana.

FEDERAL ROAD DISTRICT NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	377	1940	2	62



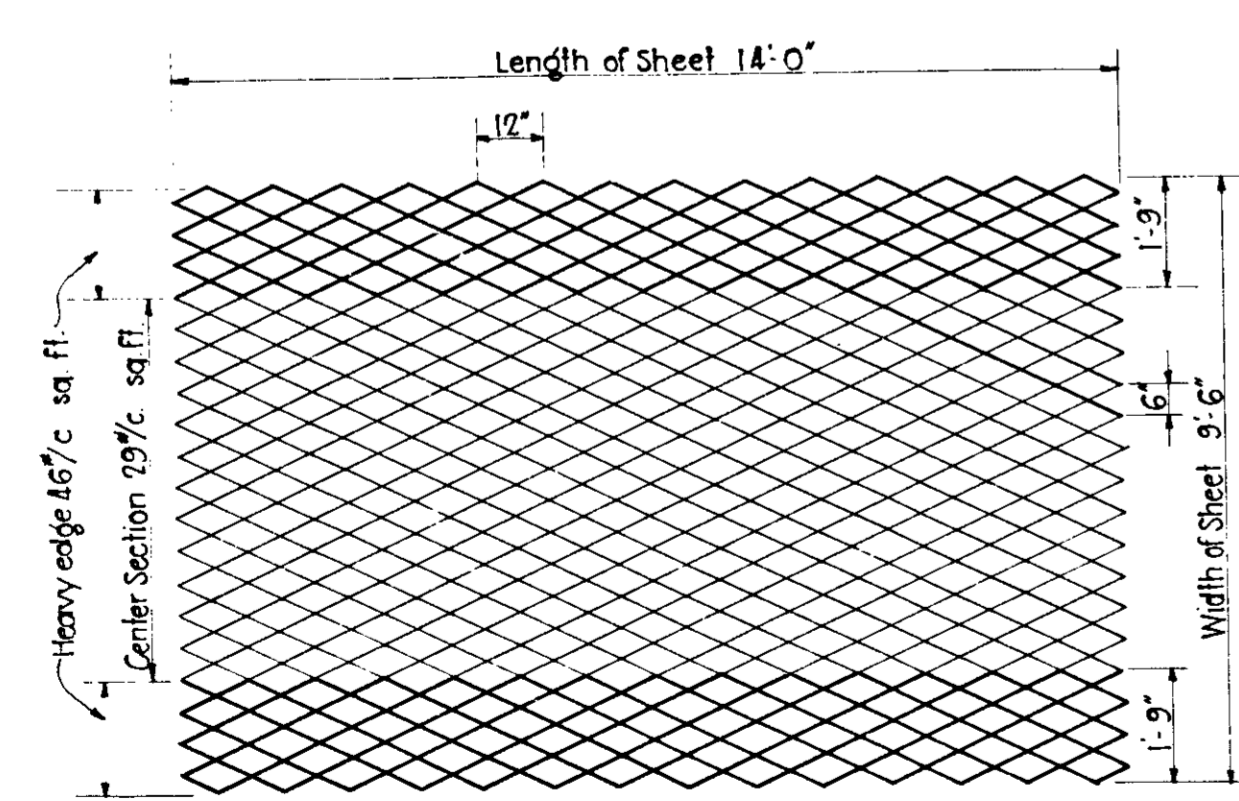
REINFORCED CONCRETE PAVEMENT

NOTE: Where any part of a fill requires a 2 to 1 slope, that slope shall be continued thruout the fill on that side.



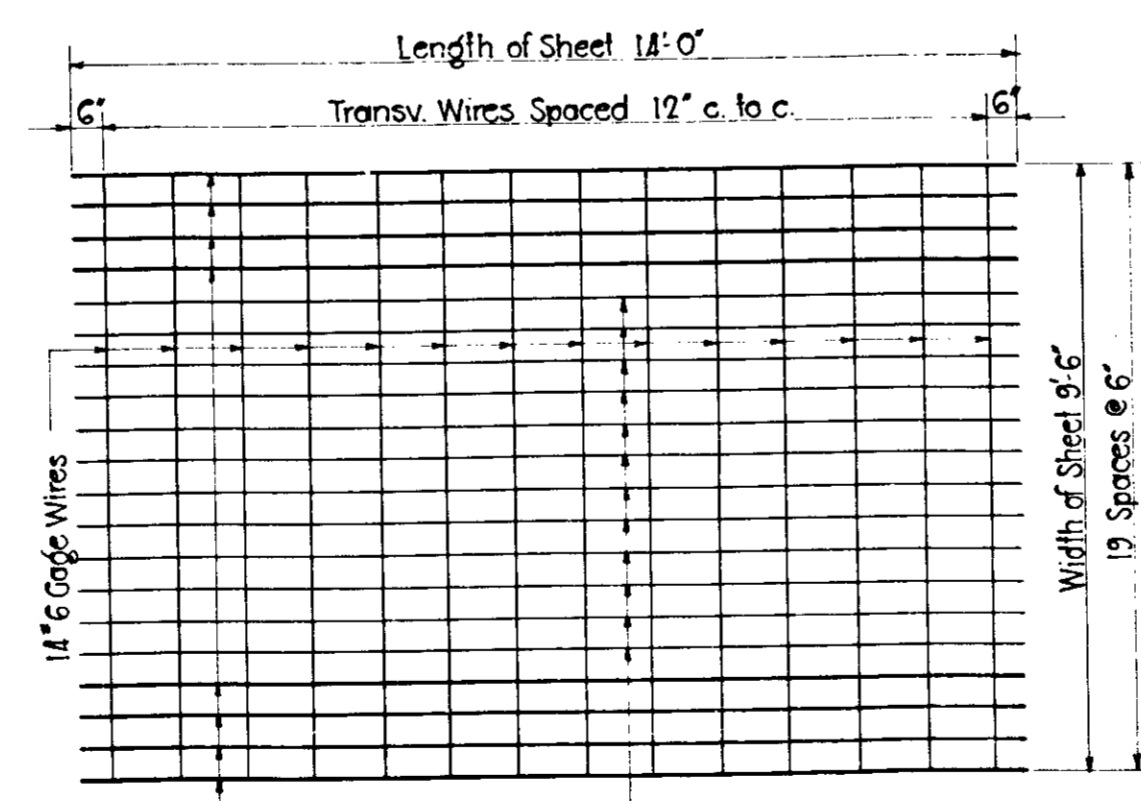
PLAN OF REINFORCED CONCRETE SLAB
SCALE: 3/8" = 1'-0"

NOTE: Where it becomes necessary to make a longitudinal lap of fabric sheet due to widened pavement, the mesh shall be lapped not less than 6'.



TYPICAL SHEET EXPANDED METAL FABRIC

SCALE: 3/8" = 1'-0"



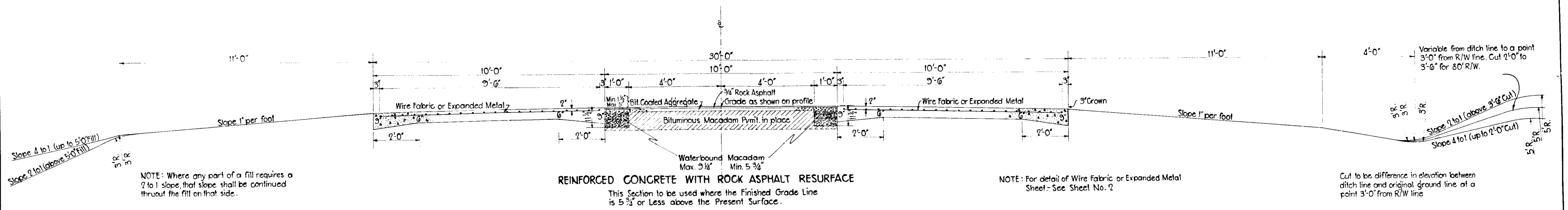
TYPICAL SHEET WIRE FABRIC

TYPICAL CROSS SECTION

APPROVED *m. Keefe* CHIEF ENGINEER - STATE HIGHWAY COMMISSION OF INDIANA
 APPROVED *J. A. Dicus* CHAIRMAN - STATE HIGHWAY COMMISSION OF INDIANA
 RECOMMENDED FOR APPROVAL 7-21-39
C. E. Vogelgesang ENGINEER OF ROAD DISTRICT

FEDERAL ROAD DISTRICT NO.	STATE	F.A. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	377	1940	3	62

SEC. C3

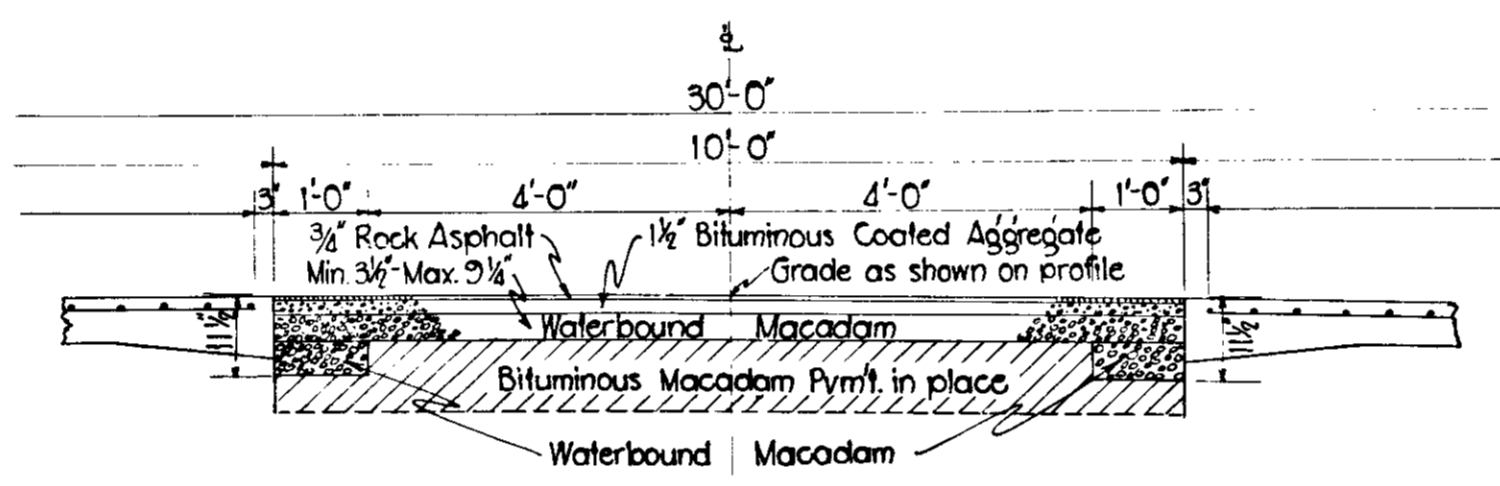


NOTE: Where any part of a fill requires a 2 to 1 slope, that slope shall be continued thruout the fill on that side.

REINFORCED CONCRETE WITH ROCK ASPHALT RESURFACE
 This Section to be used where the Finished Grade Line is $5\frac{3}{4}$ ' or Less above the Present Surface.

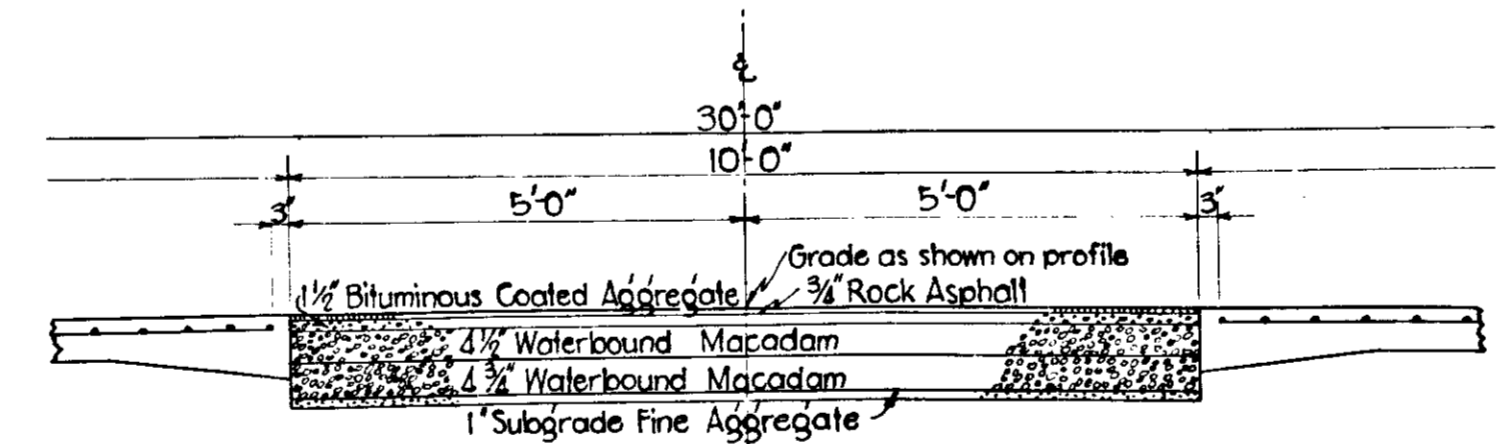
NOTE: For detail of Wire Fabric or Expanded Metal Sheet - See Sheet No. 2

Cut to be difference in elevation between ditch line and original ground line at a point 3'-0" from R/W line



REINFORCED CONCRETE WITH ROCK ASPHALT ON MACADAM BASE
 This Section to be used where the Finished Grade Line is More than $5\frac{3}{4}$ ' and Less than $11\frac{1}{2}$ ' above the Present Surface.

For R.C. Pavement and Shoulders See Above.



REINFORCED CONCRETE WITH ROCK ASPHALT ON MACADAM BASE
 This Section to be used where the Finished Grade Line is More than $11\frac{1}{2}$ ' above the Present Surface or where the Present Surface is to be Excavated.

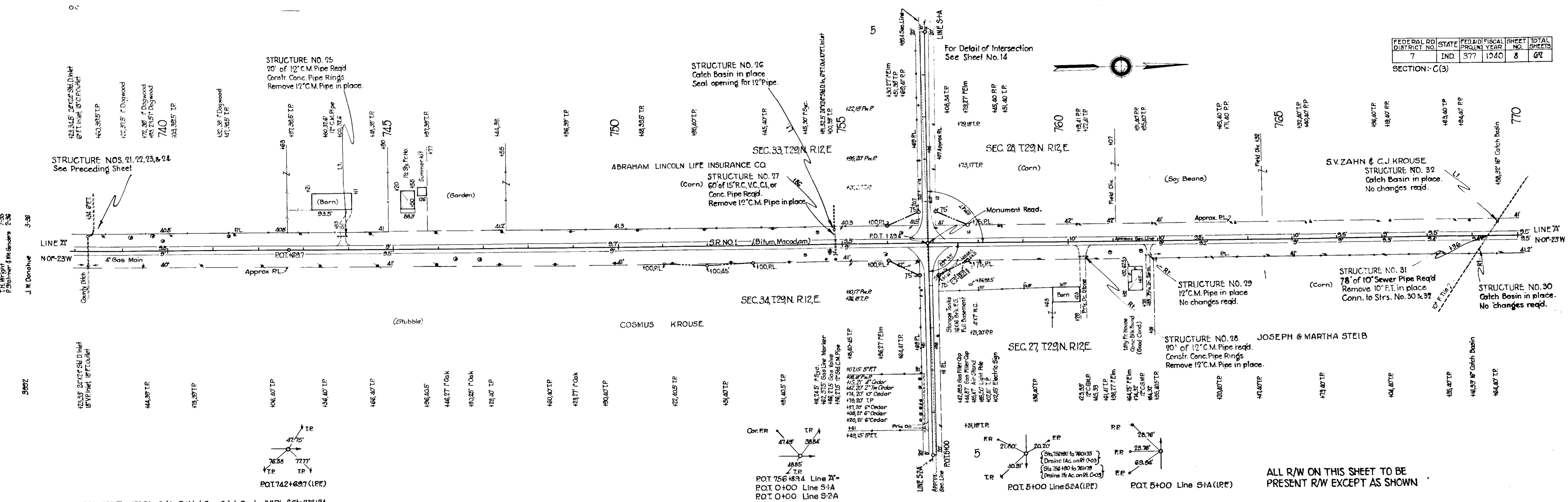
TYPICAL CROSS SECTIONS

SCALE $\frac{1}{2}$ "=1'

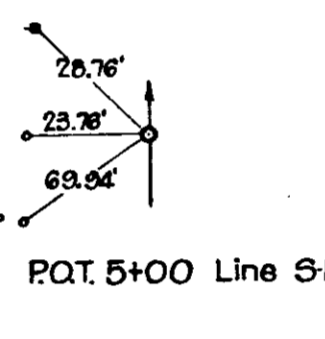
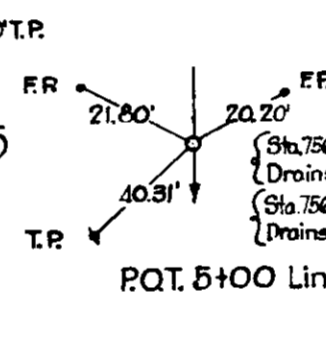
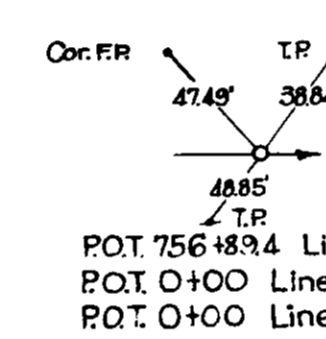
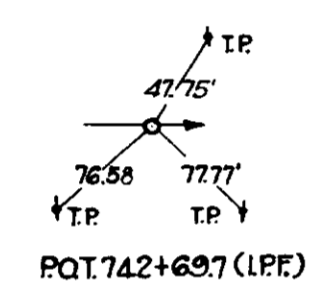
APPROVED: *J. B. Dixon* CHAIRMAN: STATE HIGHWAY COMMISSION OF INDIANA
 RECOMMENDED FOR APPROVAL: 7-21-39
 APPROVED: *W. H. Keefe* CHIEF ENGINEER: STATE HIGHWAY COMMISSION OF INDIANA
C. E. Vogelgesang ENGINEER OF ROAD DESIGN

FEDERAL RD DISTRICT NO.	STATE	FED AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND	377	1940	8	67

SECTION: C(3)

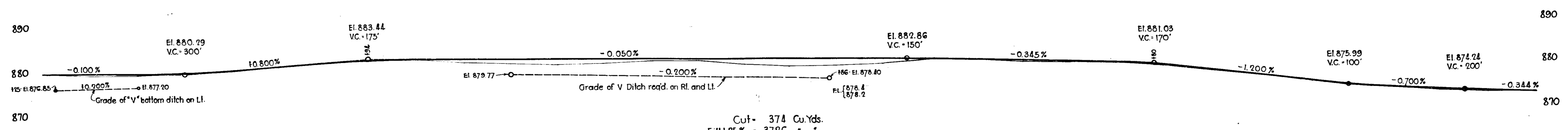


B.M. NO. 80 Elev. 878.24 = Cut in E Hdwl. Conc. Catch Basin 34' Rt. of Sta. 738+24
 B.M. NO. 81 Elev. 880.43 = Cut in Headwall (N. end) 27' Rt. of Sta. 754+85



ALL R/W ON THIS SHEET TO BE PRESENT R/W EXCEPT AS SHOWN

447 Sq. Yds. Sodding Req'd. on Lt.
 589 Sq. Yds. Sodding Req'd. on Rt.



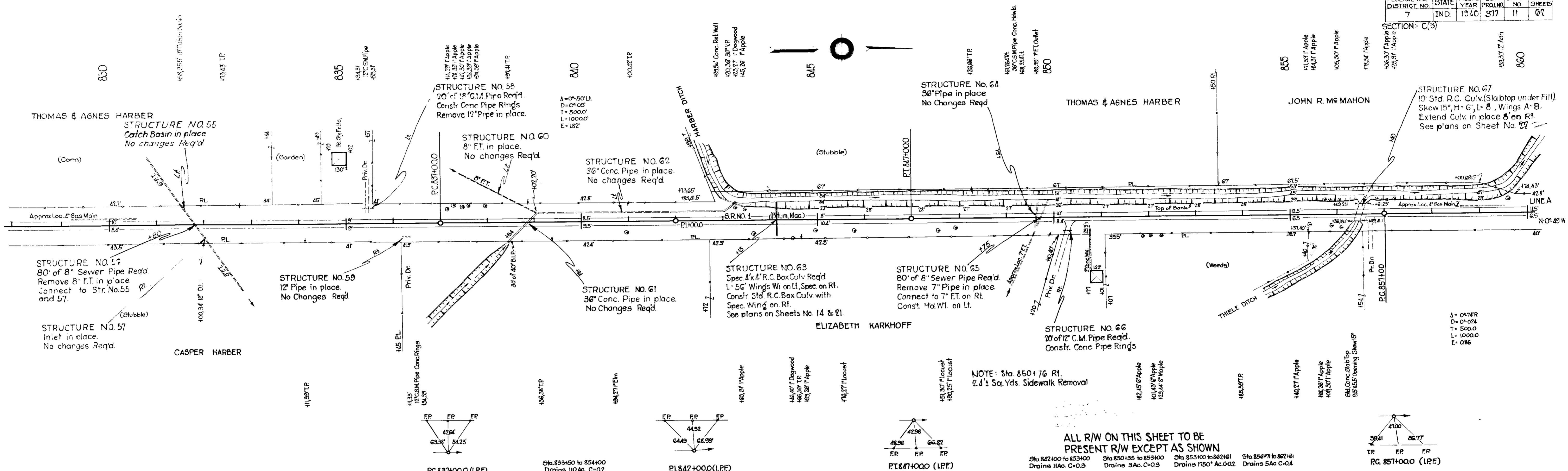
Cut = 266 Cu. Yds.
 Fill 125% = 634 . . .
 Sp. Borrow = 368 . . .

Cut = 374 Cu. Yds.
 Fill 125% = 3726 . . .
 Overhaul 1 Bal. = 668 . . .
 Overhaul 2 Bal. = 994 . . .
 Overhaul 3 Bal. = 1690 . . .
 Above Overhaul of 1 Balance to be obtained from Balance Sta. 762+00 to 782+00.
 Overhaul of 2 Balances to be obtained from Balance Sta. 782+00 to 802+00.
 Overhaul of 3 Balances to be obtained from Balance Sta. 802+00 to 815+00.

80.4	80.3	80.0	80.0	80.4	81.0	81.6	81.2	81.2	81.3	81.4	81.5	81.6	81.7	81.8	81.9	82.0	82.1	82.2	82.3	82.4	82.5	82.6	82.7	82.8	82.9	83.0	83.1	83.2	83.3	83.4	83.5	83.6	83.7	83.8	83.9	84.0	84.1	84.2	84.3	84.4	84.5	84.6	84.7	84.8	84.9	85.0	85.1	85.2	85.3	85.4	85.5	85.6	85.7	85.8	85.9	86.0	86.1	86.2	86.3	86.4	86.5	86.6	86.7	86.8	86.9	87.0	87.1	87.2	87.3	87.4	87.5	87.6	87.7	87.8	87.9	88.0	88.1	88.2	88.3	88.4	88.5	88.6	88.7	88.8	88.9	89.0	89.1	89.2	89.3	89.4	89.5	89.6	89.7	89.8	89.9	90.0	90.1	90.2	90.3	90.4	90.5	90.6	90.7	90.8	90.9	91.0	91.1	91.2	91.3	91.4	91.5	91.6	91.7	91.8	91.9	92.0	92.1	92.2	92.3	92.4	92.5	92.6	92.7	92.8	92.9	93.0	93.1	93.2	93.3	93.4	93.5	93.6	93.7	93.8	93.9	94.0	94.1	94.2	94.3	94.4	94.5	94.6	94.7	94.8	94.9	95.0	95.1	95.2	95.3	95.4	95.5	95.6	95.7	95.8	95.9	96.0	96.1	96.2	96.3	96.4	96.5	96.6	96.7	96.8	96.9	97.0	97.1	97.2	97.3	97.4	97.5	97.6	97.7	97.8	97.9	98.0	98.1	98.2	98.3	98.4	98.5	98.6	98.7	98.8	98.9	99.0	99.1	99.2	99.3	99.4	99.5	99.6	99.7	99.8	99.9	100.0
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SECTION - C(5)

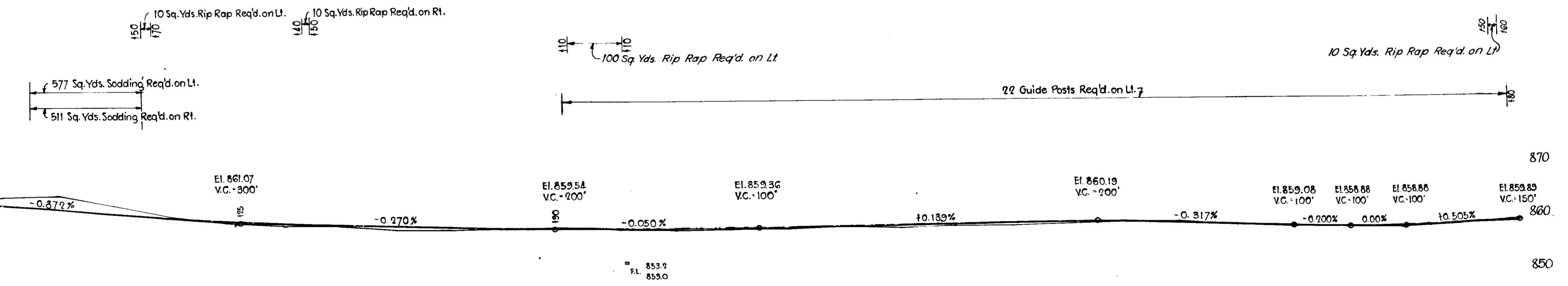
TH Weight
 3-36
 J.W. Donahue
 3992



B.M. NO. 87 Elev. 866.40 Post Spike in TP No. 227 39' Rt. of Sta. 832+79
 B.M. NO. 88 Elev. 855.16 oCut in N. End Hdwl. 32' Lt. of Sta. 843+50
 B.M. NO. 89 Elev. 858.59 " " " SE. Cor. 178' Rt. of Sta. 856+36

NOTE: Sta. 850+76 Rt. 2.4' Sq. Yds. Sidewalk Removal

ALL R/W ON THIS SHEET TO BE
 PRESENT R/W EXCEPT AS SHOWN
 Sta. 842+00 to 853+00 Drains 11Ac. C-0.3
 Sta. 850+35 to 853+00 Drains 3Ac. C-0.3
 Sta. 853+00 to 862+61 Drains 1750' Ac. C-0.2
 Sta. 856+71 to 862+01 Drains 5Ac. C-0.4



Cut = 3706 Cu. Yds.
 Fill @ 75% = 595 " "
 Waste = 3111 " "

1241 Cu. Yds. of above Waste to be overhauled
 1 Balance to Balance Sta. 845+00 to 865+00.
 and 1870 Cu. Yds. to be overhauled 2 Balances
 to Balance Sta. 865+00 to 882+00.

Cut = 633 Cu. Yds.
 Fill @ 75% = 1952 " "
 Overhaul & Bal. = 1241 " "
 Borrow = 78 " "
 Above Overhaul to be obtained from
 Balance Sta. 825+00 to 845+00.

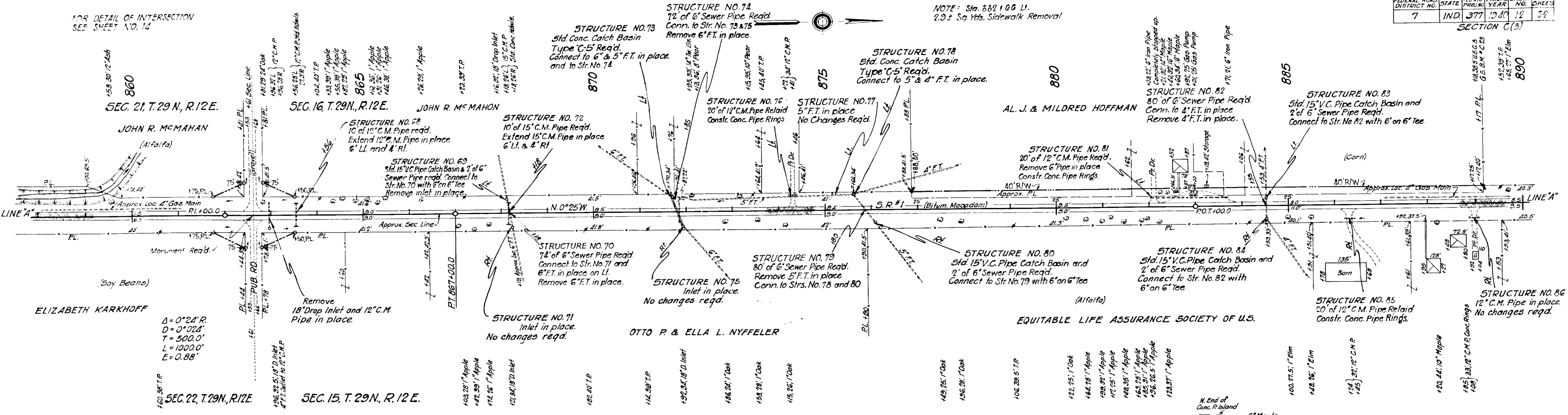
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866.9	866.8	867.2	866.3	866.0	866.4	866.5	864.7 864.0 863.7	862.3 862.1 861.9 861.6	861.0	860.5	860.4 860.4	860.4 860.1	859.6	859.5	859.9	859.8 859.5 859.5	859.3	859.2	859.2	859.1	859.6	859.2	859.3 859.3 859.3	859.4 859.4 859.5	859.9	860.1	860.0	859.2	858.9 858.9 858.9 858.1 858.1 858.0	858.7	859.2 859.4 859.5 859.5	860

FEDERAL ROAD DISTRICT NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND	377	1940	12	32

SECTION C (3)

FOR DETAIL OF INTERSECTION SEE SHEET NO. 12

NOTE: Sta. 882+00 G.G. LI. 2.0 ± Sq. Yds. Sidewalk Removal



Sta. 862+61 - 866+00 Drains 3 1/2 A. C=0.3

Sta. 866+00 - 870+50 Drains 7 A. C=0.3

Sta. 870+50 - 887 Drains 28 A. C=0.3

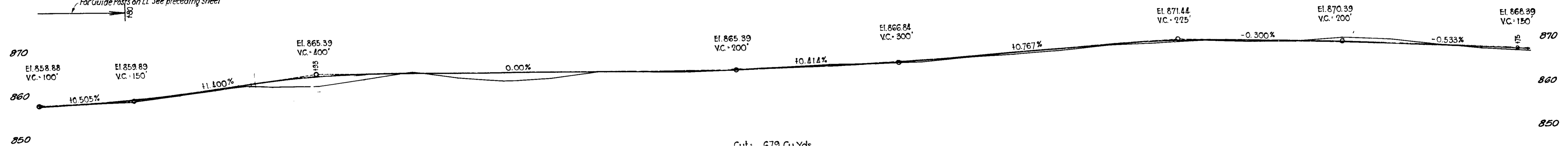
Sta. 886+25 - 887 Drains 1 A. C=0.3

ALL R/W ON THIS SHEET TO BE PRESENT R/W EXCEPT AS SHOWN

B.M. No. 90 El. 862.89 a Cut in S.E. Cor. Mahl. 26' Rt. Sta. 863+17
 B.M. No. 91 El. 868.96 Bl. Spike in T.P. #2533. 39' Rt. Sta. 880+40
 B.M. No. 92 El. 867.25 Brass Disk on Conc. Post, 38.3' Lt., Sta. 889+18 = (U.S.C. & G.S. B.M. #C-23 El. 798+443)

For Rip Rap on Lt. See Preceding Sheet.

For Guide Posts on LI See preceding sheet



Cut - 679 Cu. Yds.
 Fill + 25% - 2,628 " "
 Overhaul 2 Bal. - 1,870 " "
 Borrow - 79 " "
 Above Overhaul of 2 Balances to be obtained from Balance Sta. 825+00 to 845+00



58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90
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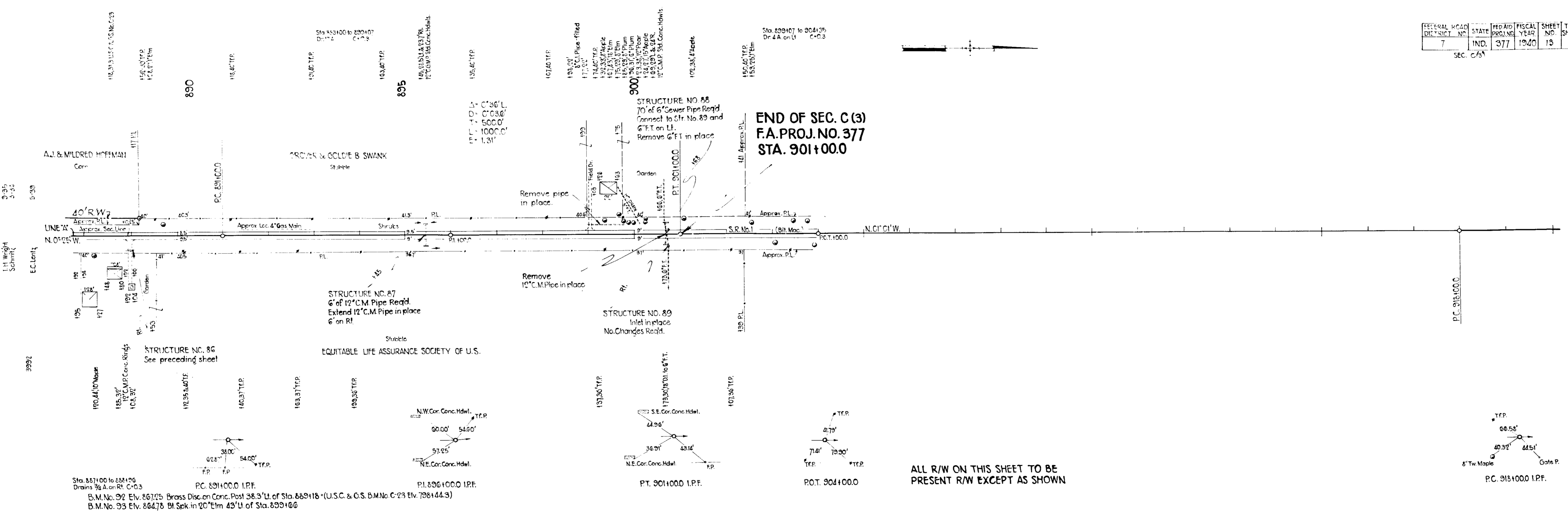
C.E.V. 7-21-39

FEDERAL ROAD DISTRICT NO.	STATE	FED AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	IND.	377	1940	13	62

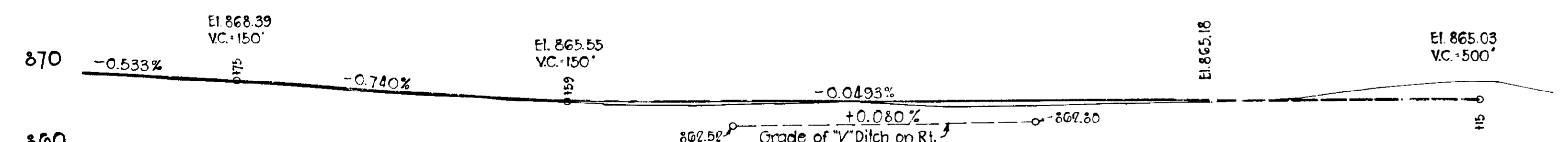
SEC. C(3)



END OF SEC. C (3)
F.A. PROJ. NO. 377
STA. 901+00.0

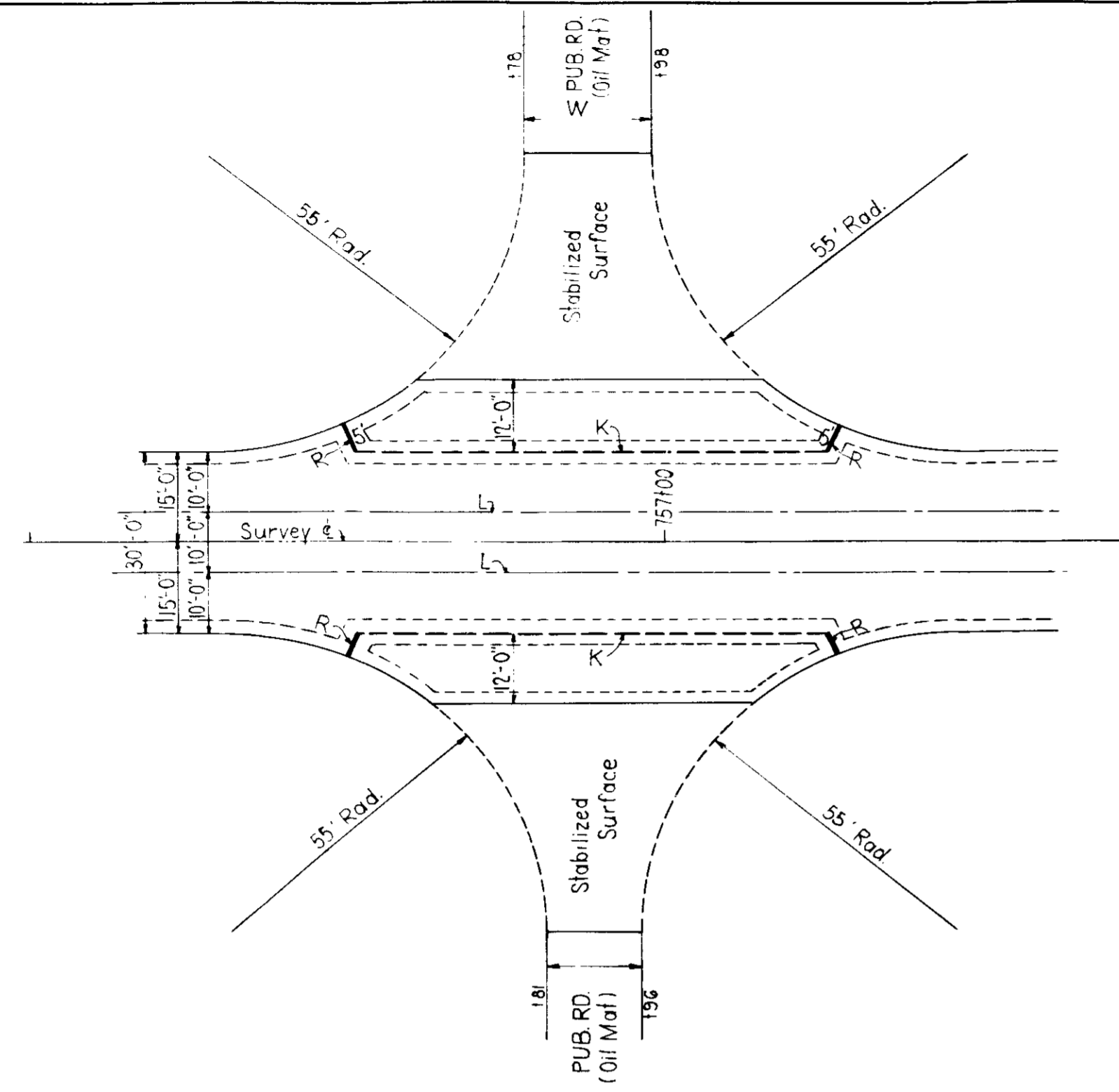


ALL R/W ON THIS SHEET TO BE PRESENT R/W EXCEPT AS SHOWN



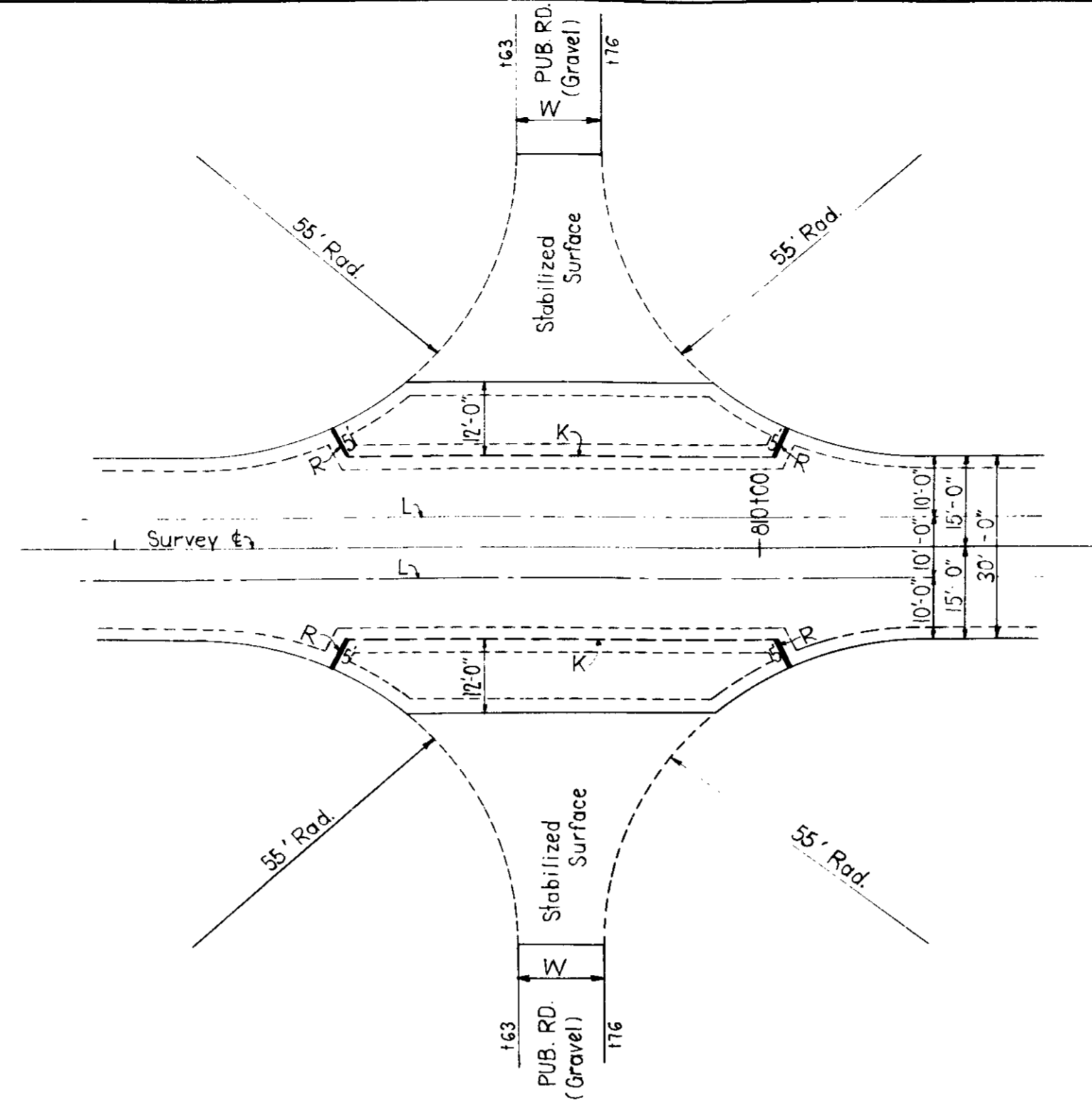
Cut = 1197 Cu Yds.
 Fill 125% = 1226 " "
 Borrow = 29 " "

88	89	90	91	92	93	94	95	96	97	98	99	100	101	102	103	104	105	106	107	108	109	110	111	112	113	114	115	116	117	118	119	120	121	122	123	124	125	126	127	128	129	130	131	132	133	134	135	136	137	138	139	140	141	142	143	144	145	146	147	148	149	150	151	152	153	154	155	156	157	158	159	160	161	162	163	164	165	166	167	168	169	170	171	172	173	174	175	176	177	178	179	180	181	182	183	184	185	186	187	188	189	190	191	192	193	194	195	196	197	198	199	200
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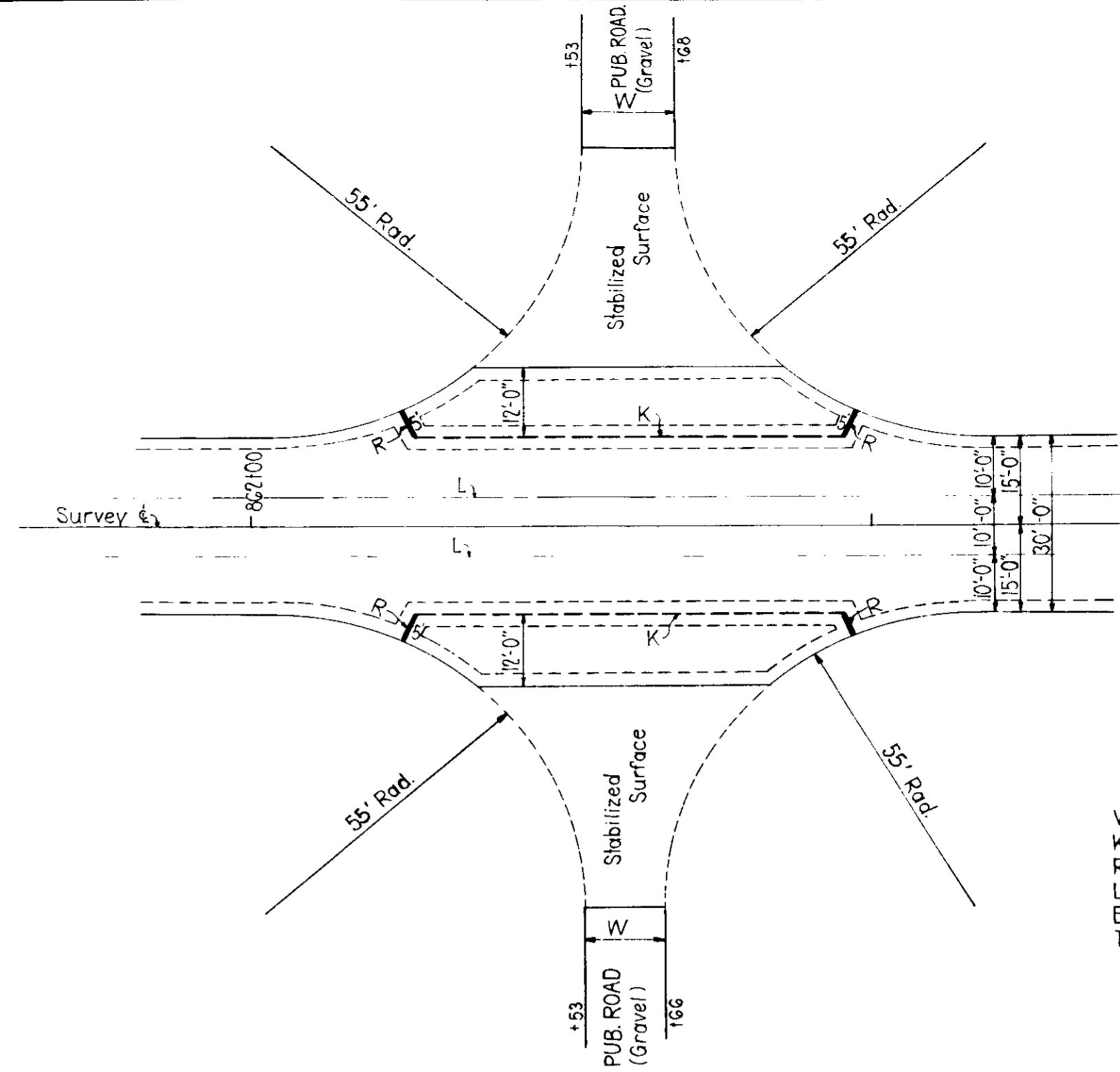
DETAIL OF INTERSECTION STA. 75G+88

ADDITIONAL QUANTITIES
 Pavement----- 195 S.yds.
 Expansion Joint 1" Cork, Rubber or Fiber-- 18 Lin. ft.
 Stabilized Surface----- 240 S.yds.



DETAIL OF INTERSECTION STA. 809+70

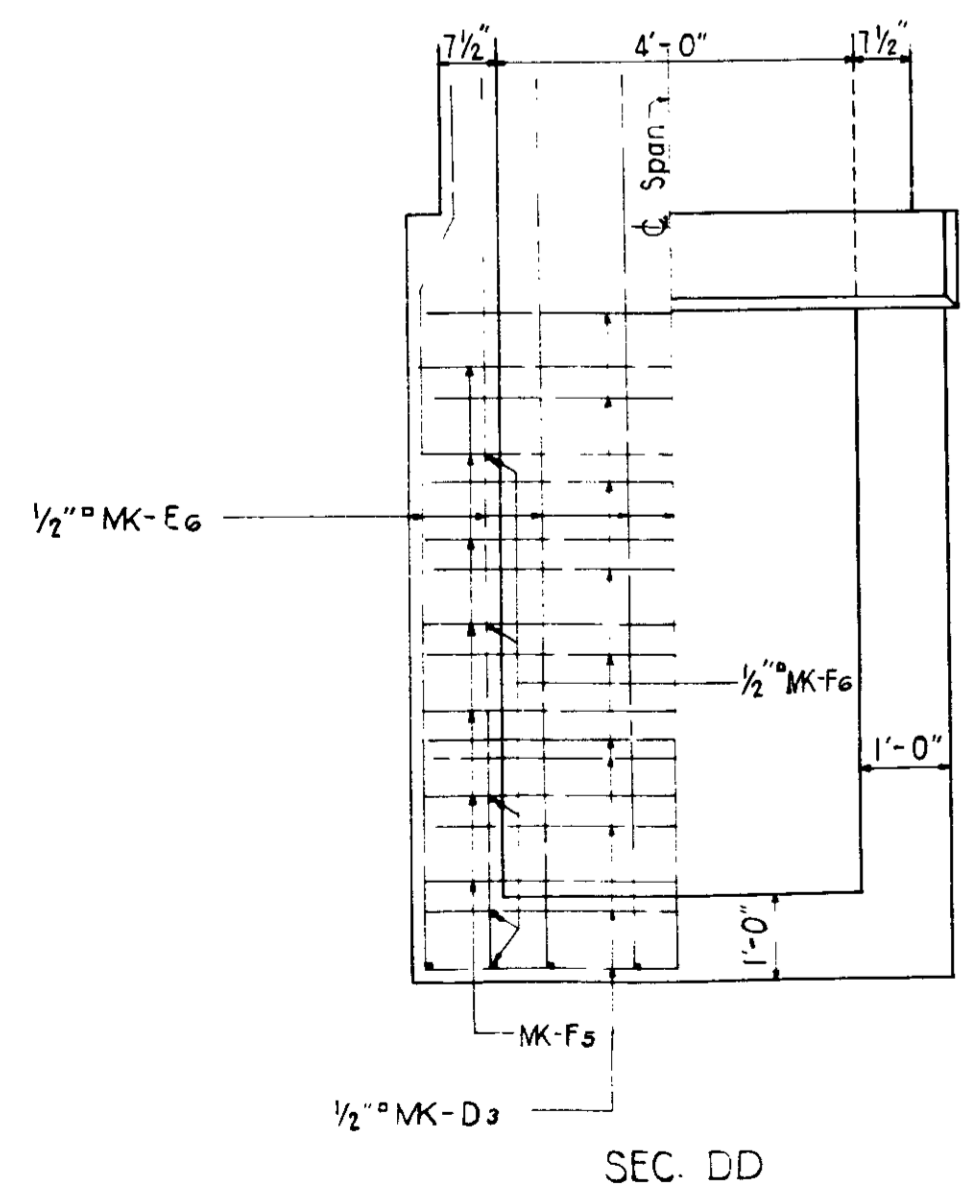
ADDITIONAL QUANTITIES
 Pavement----- 182 S.yds.
 Expansion Joint 1" Cork, Rubber or Fiber-- 20 Lin. ft.
 Stabilized Surface----- 203 S.yds.



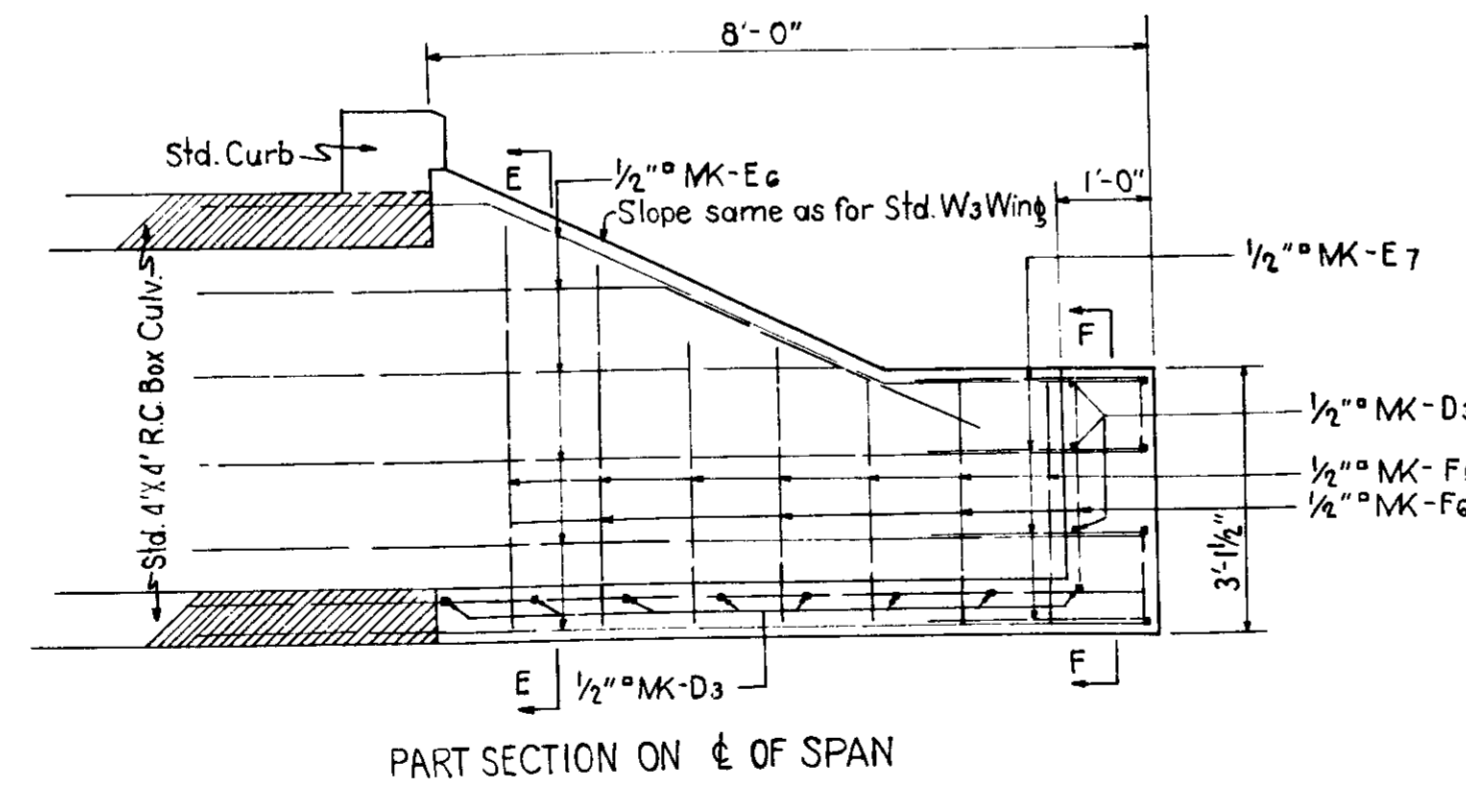
DETAIL OF INTERSECTION STA. 862+60

ADDITIONAL QUANTITIES
 Pavement----- 185 S.yds.
 Expansion Joint 1" Cork, Rubber or Fiber-- 19 Lin. ft.
 Stabilized Surface----- 209 S.yds.

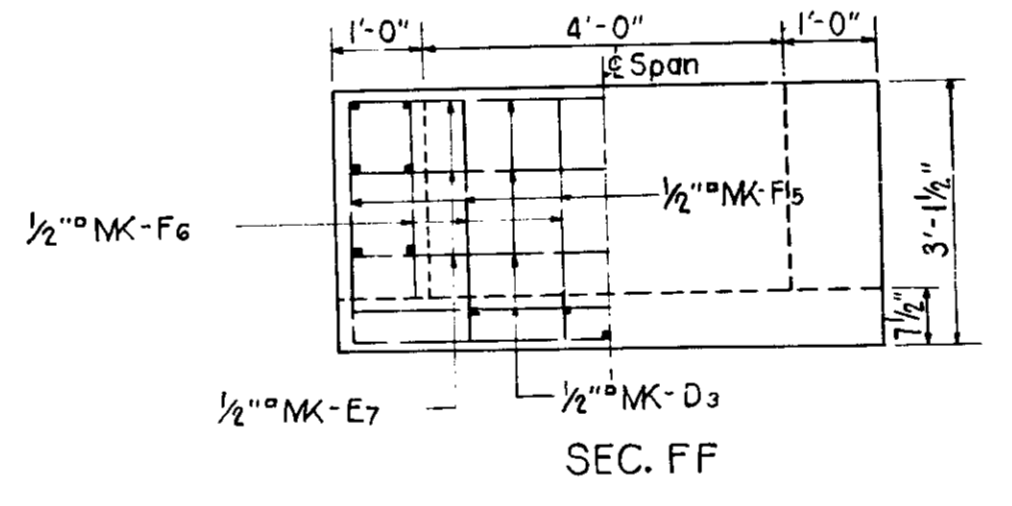
SYMBOLS
 W = Width of Surface in place.
 K = Construction Joint.
 R = 1" Cork, Rubber or Fiber Expansion Joint.
 L = Std. Longitudinal Joint.
 Edges adjacent to dashed lines to be Std. Thickened Edges.



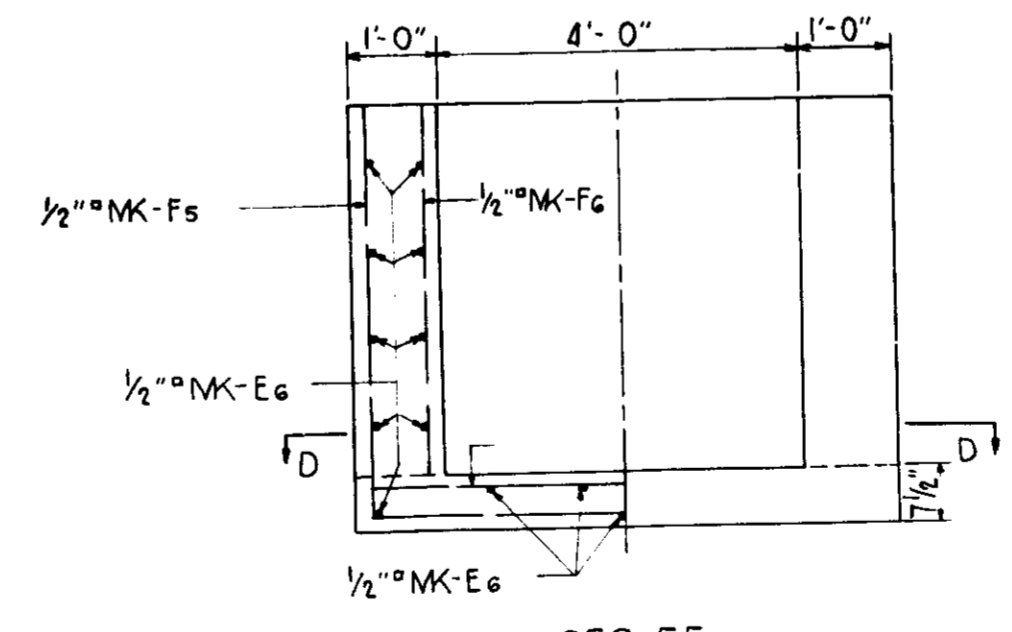
SEC. DD



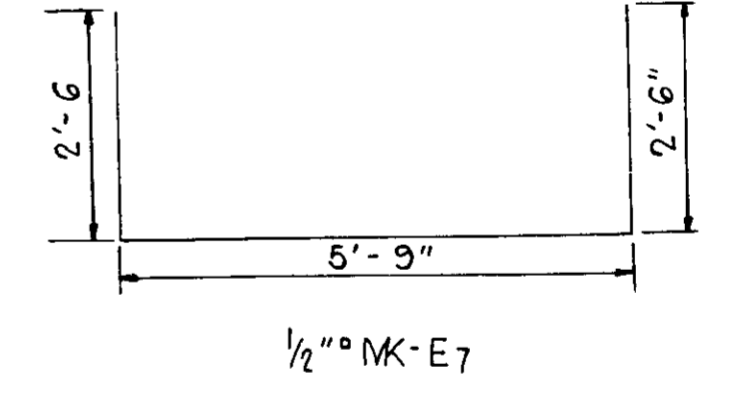
PART SECTION ON C OF SPAN



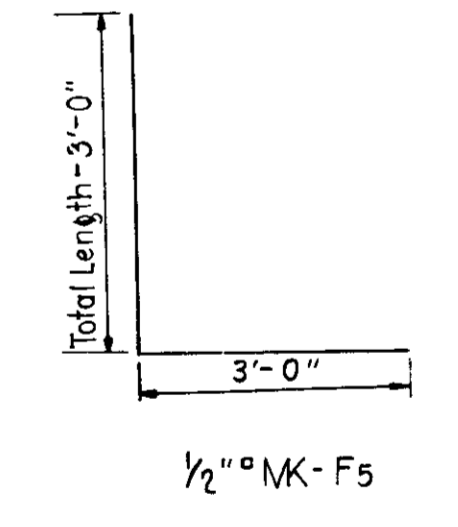
SEC. FF



SEC. EE



1/2" MK-E7



1/2" MK-F5

REINFORCING BARS																Total Steel Lbs.	Conc. Cyds.							
Bars D3		Bars E6		Bars E7		Bars G2		Bars F5						Bars F6										
No.	Len.	No.	Len.	No.	Len.	No.	Len.	No.	Len.	No.	Len.	No.	Len.	No.	Len.	No.	Len.							
15	5'-6"	26	10'-6"	3	10'-9"	3	4'-0"	2	7'-9"	2	7'-3"	4	6'-3"	12	5'-9"	2	3'-9"	2	2'-9"	1	8	2'-3"	471	4.04

NOTE:-- All Bars 1/2" Spaced 1'-0" C. to C. Except Bars F6 Spaced 2'-0" C. to C.

PLAN OF SPECIAL WING ON RT. FOR STR. NO. G3

INTERSECTION DETAILS

Scale: 1" = 20'-0"

