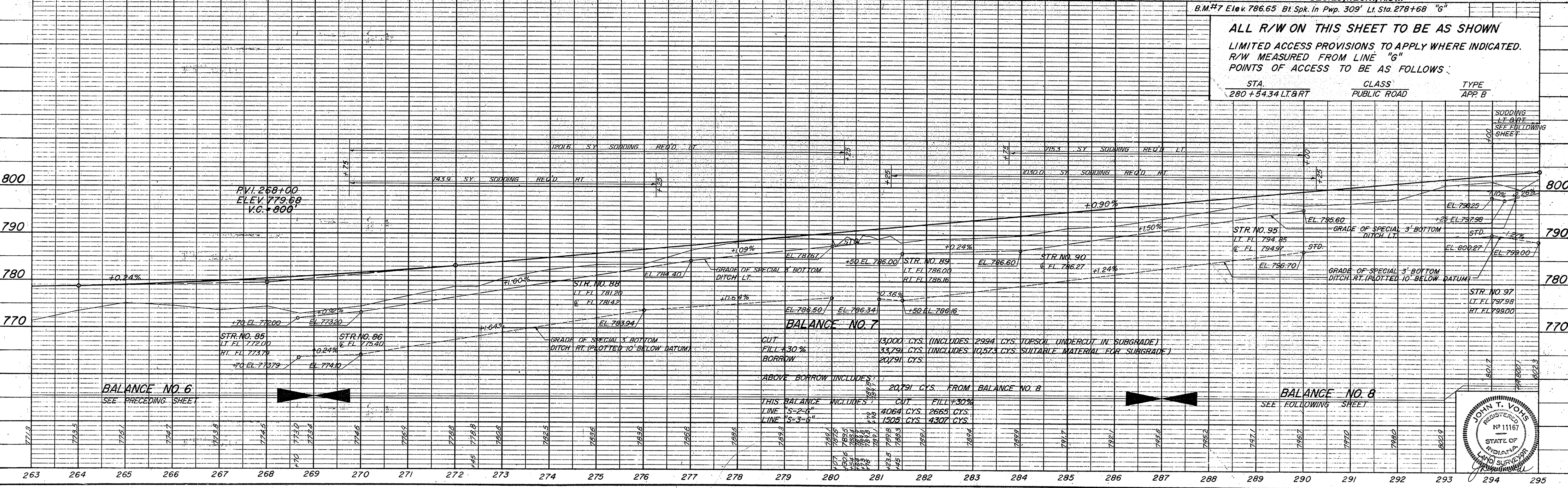
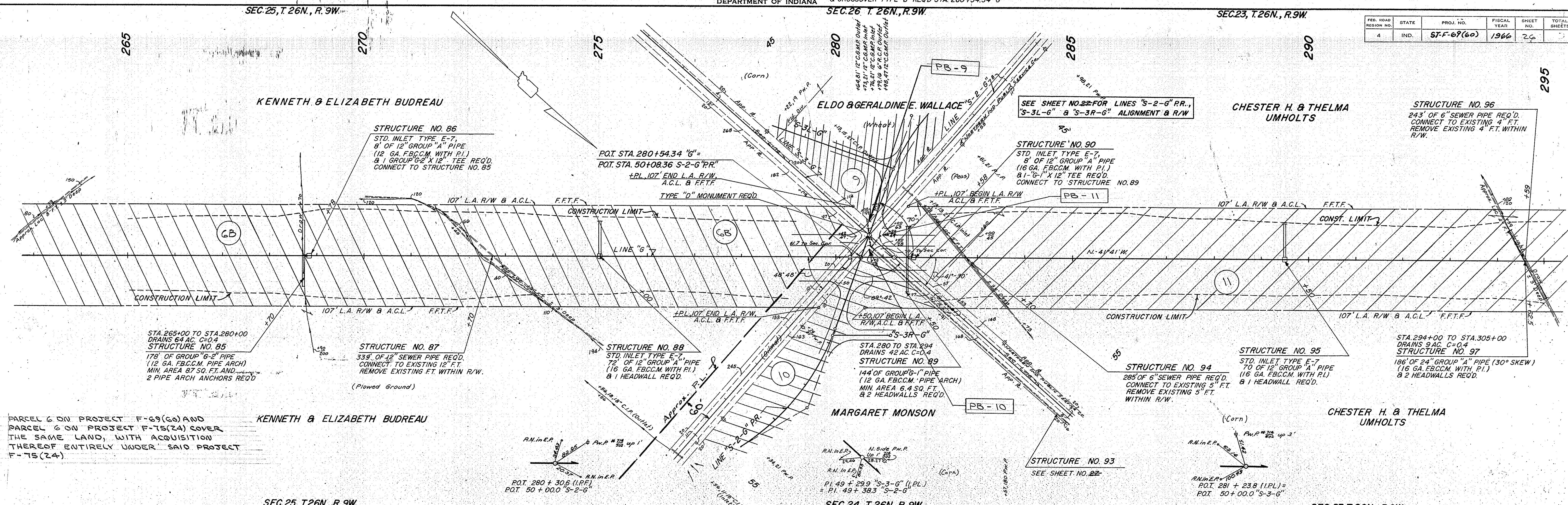


FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-69(60)	1966	26	26

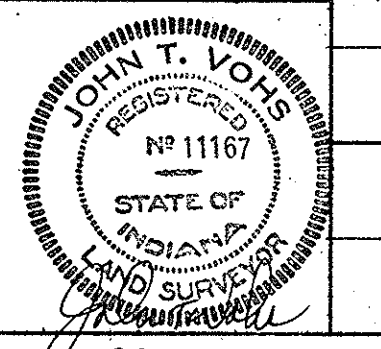
DATE	BY
6-1-68	U.S.M.C.
12-1-68	J.A.S.J.
12-1-68	E.L.C.R.H.
4-1-69	E.L.C.R.H.
7-1-69	E.L.C.R.H.

DATE	BY
6-1-68	U.S.M.C.
12-1-68	J.A.S.J.
12-1-68	E.L.C.R.H.
4-1-69	E.L.C.R.H.
7-1-69	E.L.C.R.H.



ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
R/W MEASURED FROM LINE "G"
POINTS OF ACCESS TO BE AS FOLLOWS:

STA.	CLASS	TYPE
280+54.34 LT. & RT.	PUBLIC ROAD	APP. B



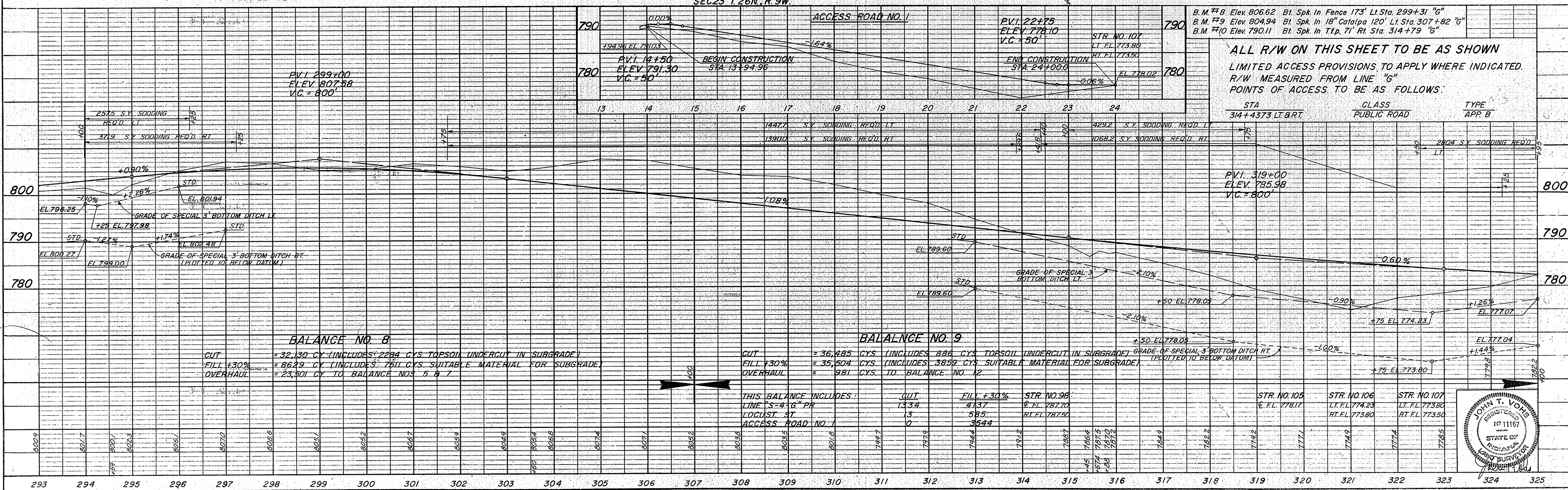
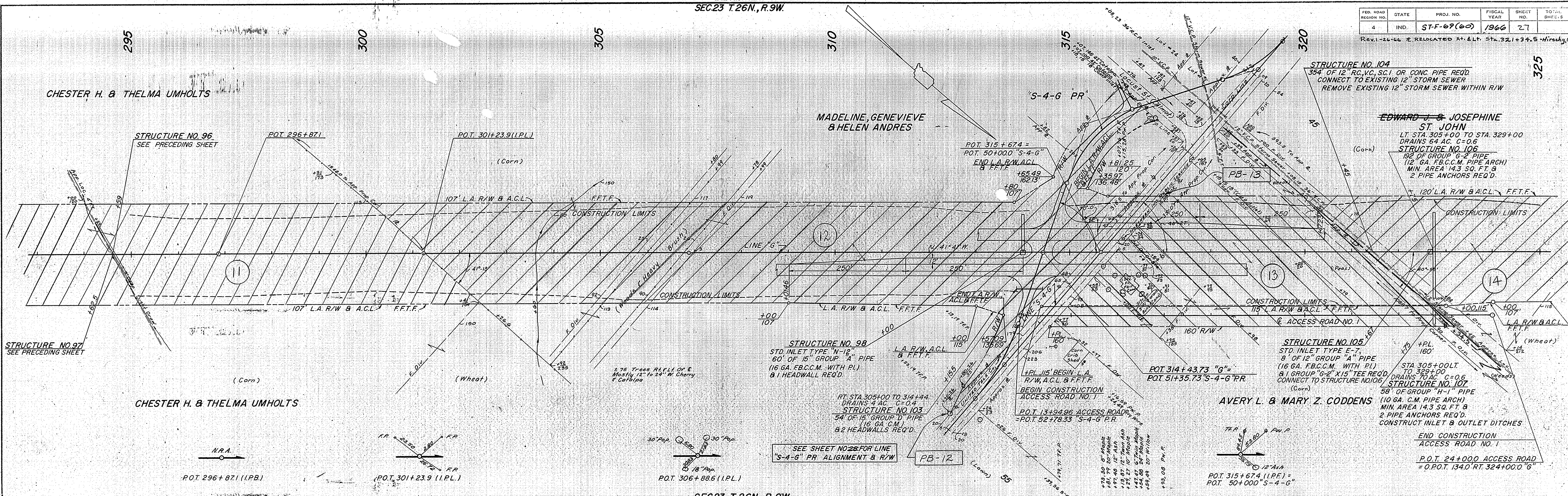
STATE HIGHWAY
DEPARTMENT OF INDIANA
SEC.23 T.26N., R.9W.

TYPE "B" ROAD APPROACH RT. & LT.
& TYPE "B" CROSSOVER REQ'D STA. 314+43.73

FED. ROAD REGION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-67(60)	1966	27	

DATE	BY
6-63	L.S.L.C.
12-63	S.M.
4-65	F.R.M. V.T.V.

DATE	BY
6-63	L.S.H.G.
12-63	T.F.F.-E.R.
9-65	T.J.P.-E.B.



ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
R/W MEASURED FROM LINE "G"
POINTS OF ACCESS TO BE AS FOLLOWS:

STA	CLASS	TYPE
314+43.73 LT. & RT.	PUBLIC ROAD	APP. B

BALANCE NO. 8
CUT = 32,130 CY (INCLUDES 2284 CYS TOPSOIL UNDERCUT IN SUBGRADE)
FILL +30% OVERHAUL = 8629 CY (INCLUDES 761 CYS SUITABLE MATERIAL FOR SUBGRADE)
OVERHAUL = 23,301 CY TO BALANCE NOS. 5 & 7

BALANCE NO. 9
CUT = 36,485 CYS (INCLUDES 886 CYS TOPSOIL UNDERCUT IN SUBGRADE)
FILL +30% OVERHAUL = 35,504 CYS (INCLUDES 3859 CYS SUITABLE MATERIAL FOR SUBGRADE)
OVERHAUL = 981 CYS TO BALANCE NO. 12

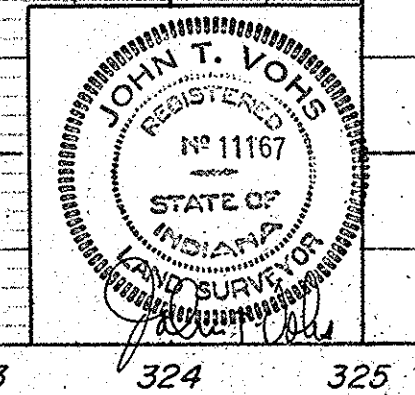
THIS BALANCE INCLUDES:
CUT 13.34
FILL +30% 4.137
LOCUST ST 3
ACCESS ROAD NO. 1 0

STR. NO. 98
EL. 787.70
RT. EL. 787.50

STR. NO. 105
EL. 778.17

STR. NO. 106
LT. EL. 774.23
RT. EL. 773.80

STR. NO. 107
LT. EL. 773.50
RT. EL. 773.50



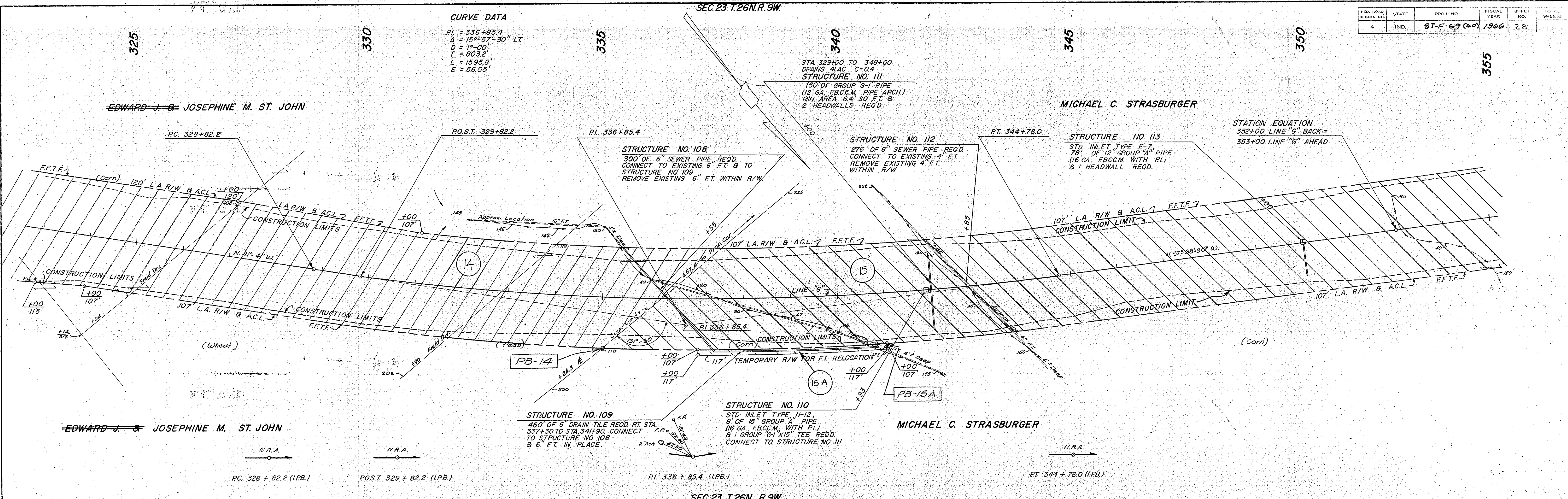
STATE HIGHWAY
DEPARTMENT OF INDIANA
SEC. 23 T. 26N. R. 9W

FED. ROAD REGION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69 (60)	1966	28	

CURVE DATA
 PI = 336+85.4
 Δ = 153°-57'-30" LT
 D = 1°-00'
 T = 803.2'
 L = 1595.8'
 E = 56.05'

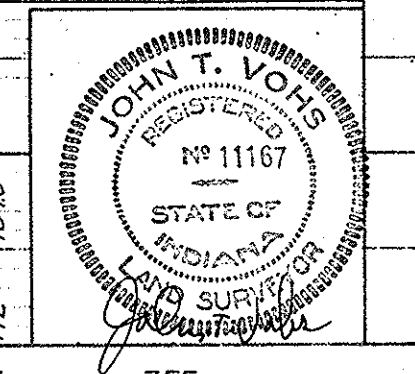
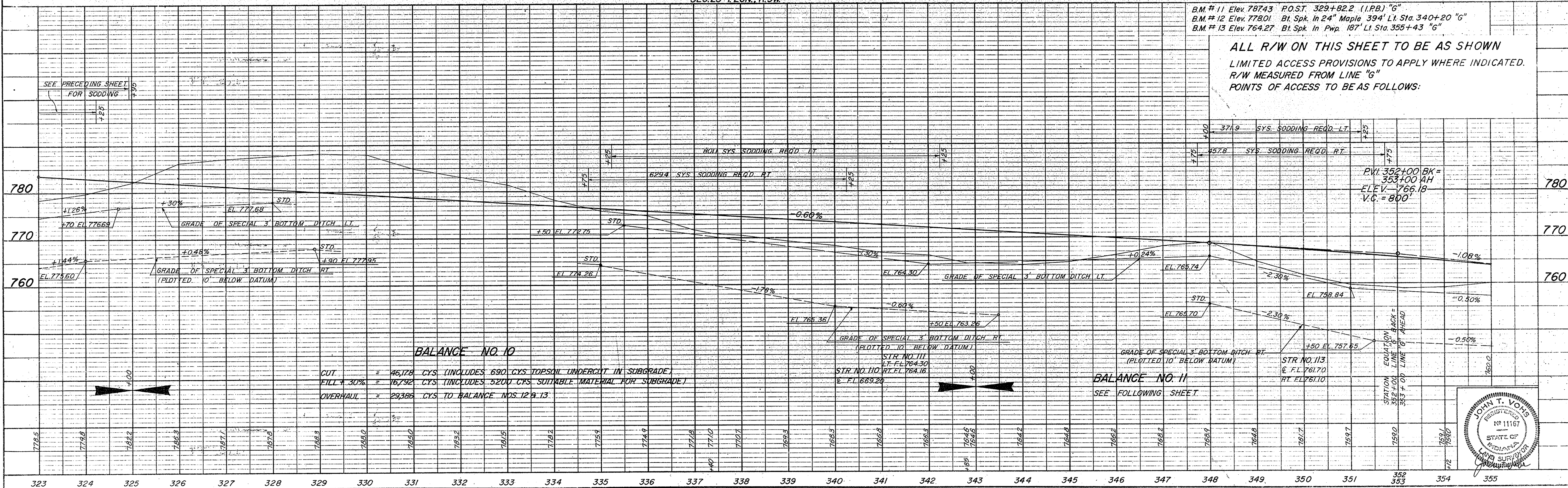
DATE	BY
6-23-63	J.S.C. E.R.
12-6-63	T.P. E.R.
9-5-65	E.R.H. J.T.V.

NO. 8928-7



DATE	BY
6-23-63	J.S.C. E.R.
12-6-63	T.P. E.R.
9-5-65	E.R.H. J.T.V.

NO. 8928-7



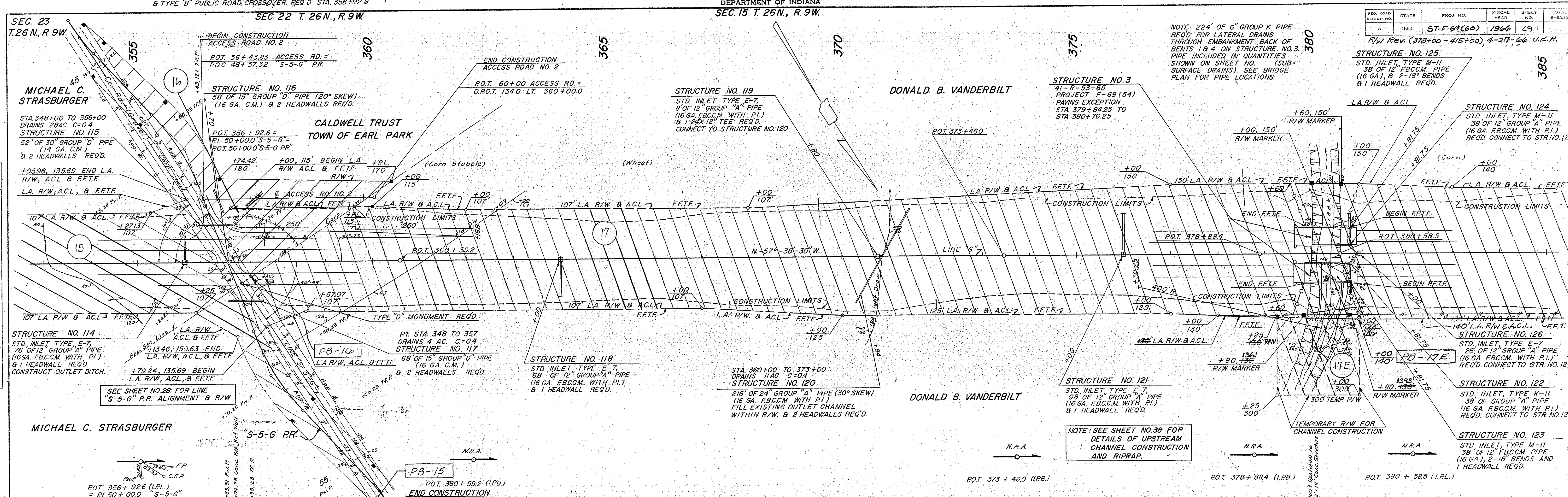
TYPE "B" ROAD APPROACH LT. & RT.
& TYPE "B" PUBLIC ROAD CROSSOVER REQ'D STA. 356+92.6

STATE HIGHWAY
DEPARTMENT OF INDIANA
SEC. 15 T. 26 N., R. 9 W.

PROJ. NO.	STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
ST-F-69(60)	IND.	1966	29	

DATE	BY	REVISION
12-28-59	L.S.M.C.	AS SHOWN
12-28-59	L.S.M.C.	ALIGNMENT CHECKED
12-28-59	L.S.M.C.	BY
12-28-59	L.S.M.C.	BY

DATE	BY	REVISION
12-28-59	L.S.M.C.	AS SHOWN
12-28-59	L.S.M.C.	ALIGNMENT CHECKED
12-28-59	L.S.M.C.	BY
12-28-59	L.S.M.C.	BY

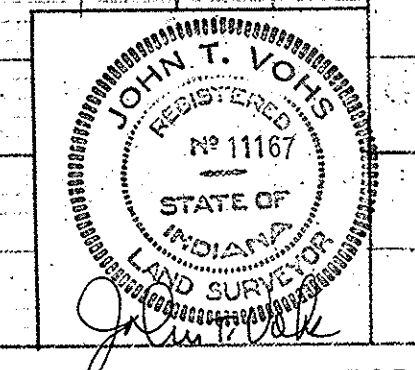
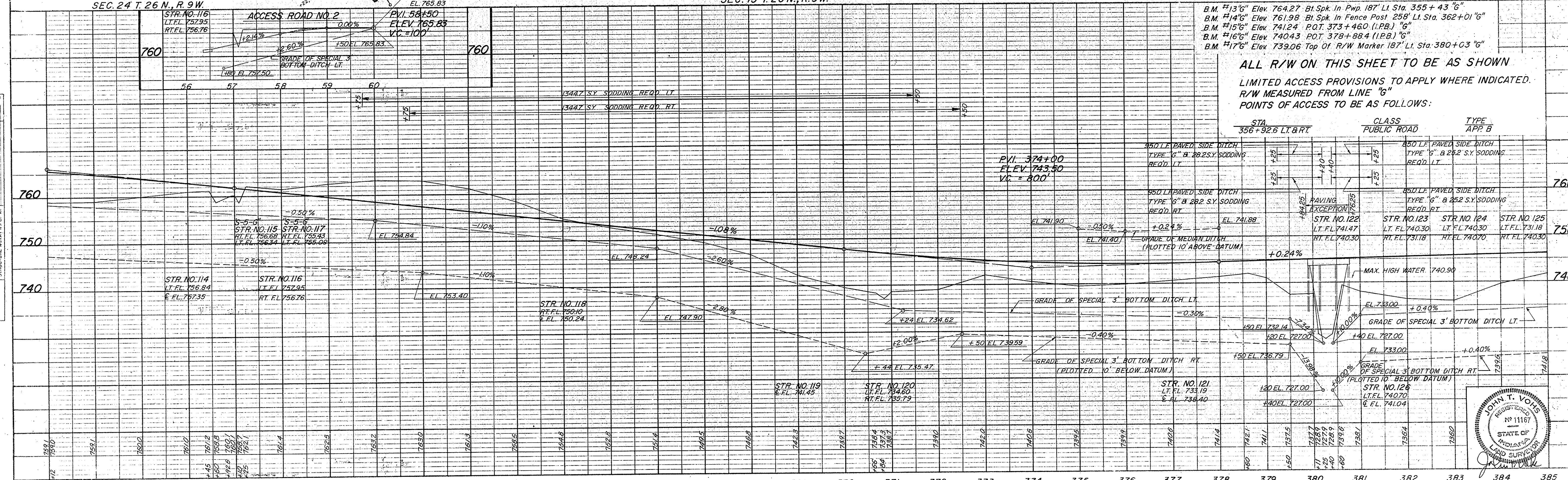


NOTE: 224' OF 6" GROUP K PIPE REQ'D FOR LATERAL DRAINS THROUGH EMBANKMENT BACK OF BENTS 1 & 4 ON STRUCTURE NO. 3. PIPE INCLUDED IN QUANTITIES SHOWN ON SHEET NO. 1 (SUB-SURFACE DRAINS). SEE BRIDGE PLAN FOR PIPE LOCATIONS.

R/W Rev. (378+00 - 415+00), 4-27-66 J.C.H.

- B.M. #13' Elev. 764.27 Bl. Spk. In Pwp. 187' Lt. Sta. 355 + 43 "6"
- B.M. #14' Elev. 761.98 Bl. Spk. In Fence Post 258' Lt. Sta. 362 + 01 "6"
- B.M. #15' Elev. 741.24 P.O.T. 373 + 460 (I.P.B.) "6"
- B.M. #16' Elev. 740.43 P.O.T. 378 + 884 (I.P.B.) "6"
- B.M. #17' Elev. 739.06 Top Of R/W Marker 187' Lt. Sta. 380 + 03 "6"

ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
R/W MEASURED FROM LINE "G"
POINTS OF ACCESS TO BE AS FOLLOWS:



STATE HIGHWAY
DEPARTMENT OF INDIANA
SEC. 15, T.26N., R.9W.
RICHLAND TWP.

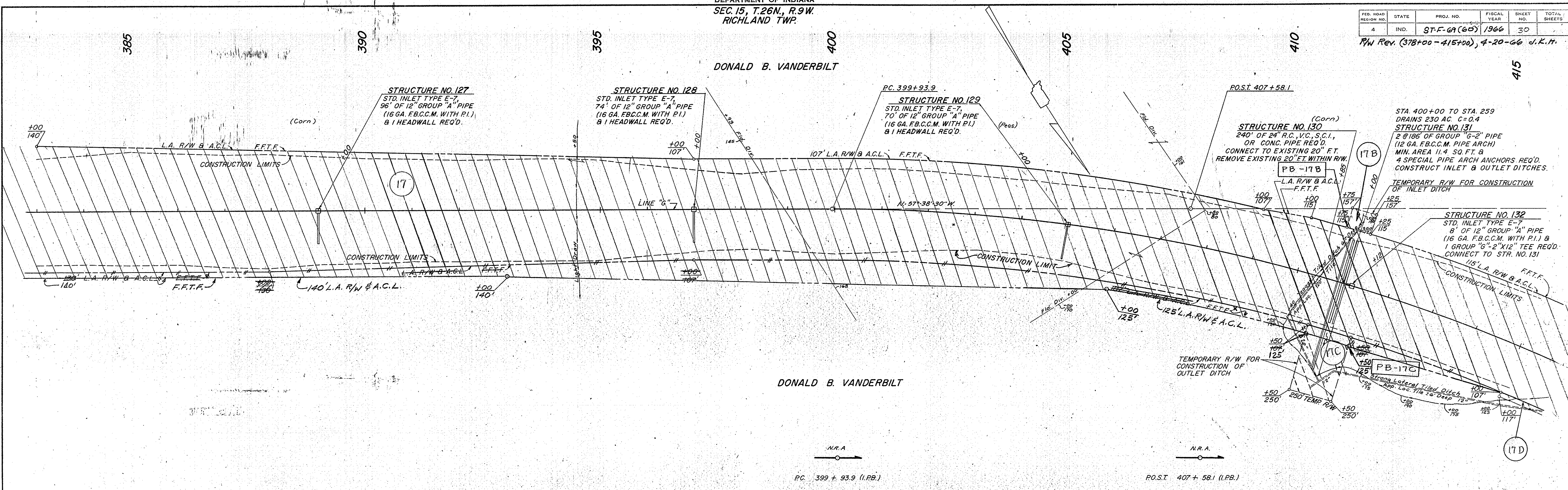
FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69 (60)	1966	30	

R/W Rev. (378+00-415+00), 4-20-66 J.K.H.

DATE	BY
5-9-63	J.B.H.C.
5-12-63	J.P.S.V.
12-6-63	J.P.S.V.

PLAN	DATE	BY
SURVEYED	5-9-63	J.B.H.C.
PLOTTED	5-12-63	J.P.S.V.
NOTES CHECKED	5-12-63	J.P.S.V.
RT. OF WAY CHECKED	12-6-63	J.P.S.V.

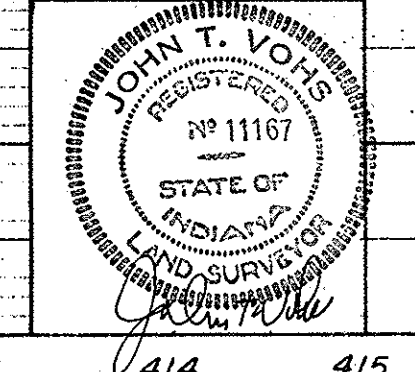
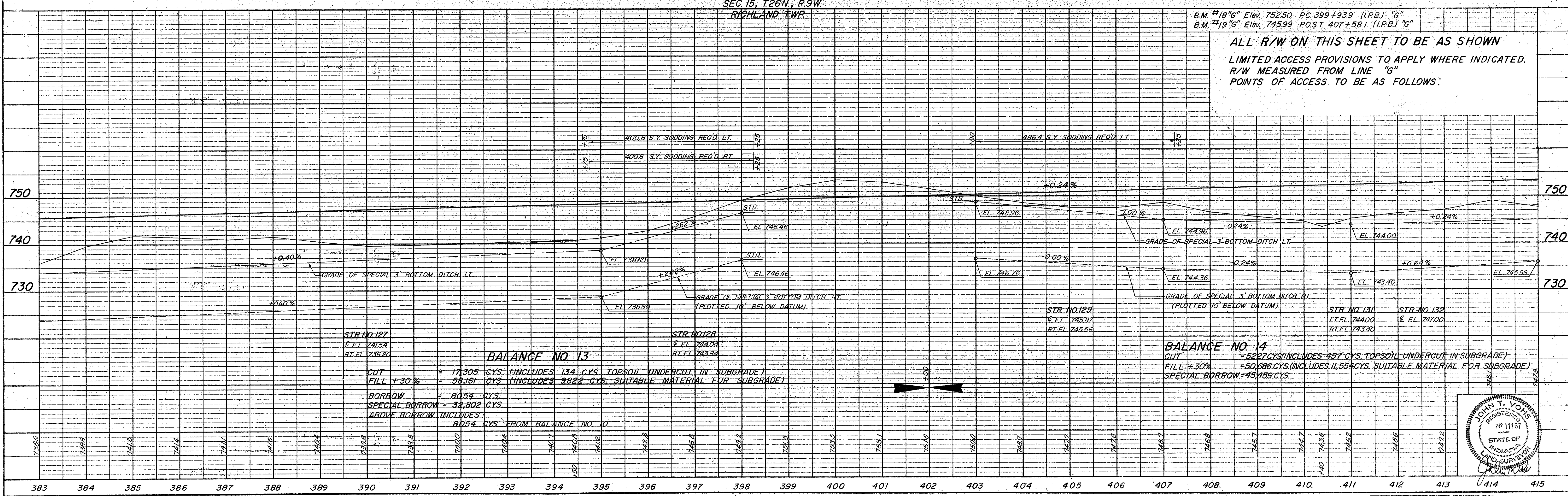
NO. 6920 ZL



DATE	BY
5-9-63	J.B.H.C.
5-12-63	J.P.S.V.
1-24-64	J.P.S.V.

PROFILE	DATE	BY
SURVEYED	5-9-63	J.B.H.C.
NOTES CHECKED	5-12-63	J.P.S.V.
B.M.'S. NOTED	5-12-63	J.P.S.V.
STRUCTURE NOTATIONS CHKD	1-24-64	J.P.S.V.

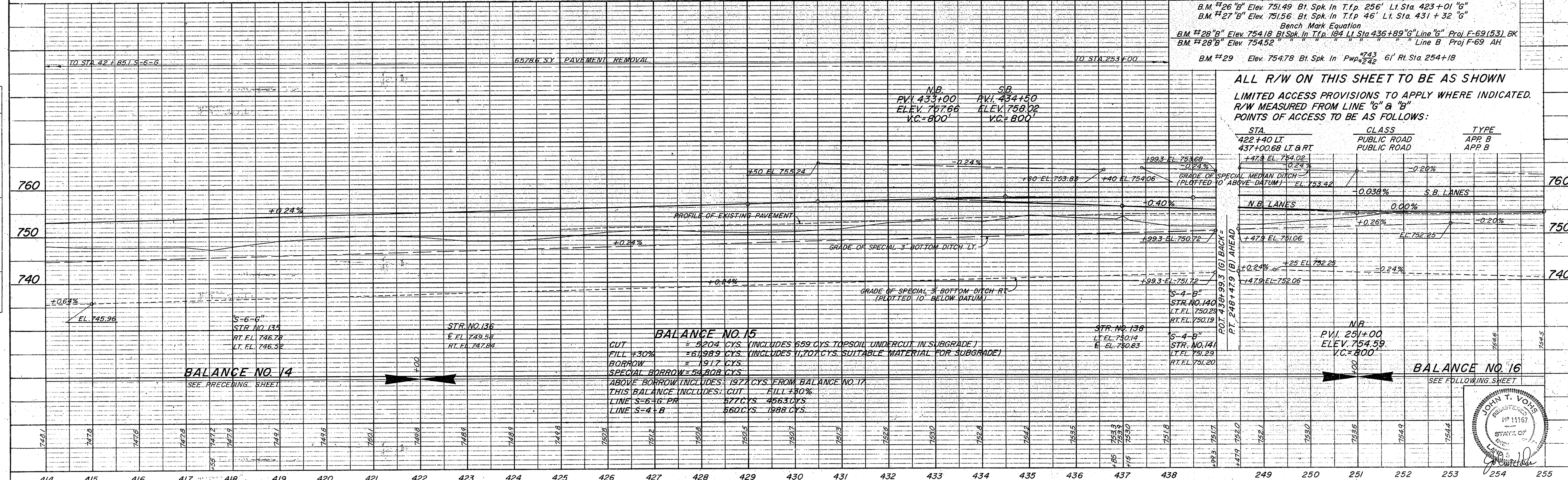
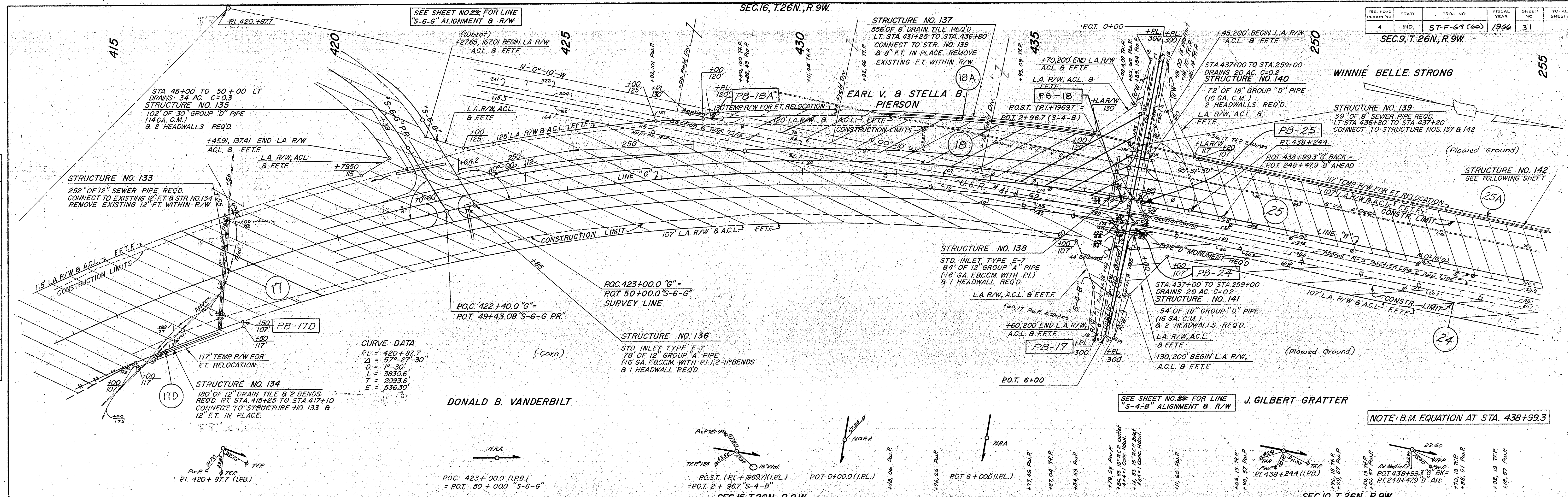
NO. 6920 ZL



FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69 (60)	1966	31	

DATE	BY
12-23	J.L.C.
12-23	J.L.C.
12-23	J.L.C.

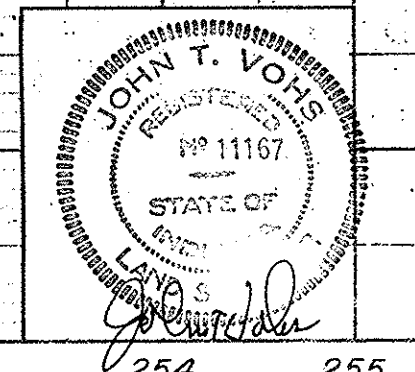
DATE	BY
12-23	J.L.C.
12-23	J.L.C.
12-23	J.L.C.



ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
R/W MEASURED FROM LINE "G" & "B"
POINTS OF ACCESS TO BE AS FOLLOWS:

STA.	CLASS	TYPE
422+40 LT	PUBLIC ROAD	APP B
437+00.68 LT & RT	PUBLIC ROAD	APP B
423+01		
431+32		
436+89		
437+00		
438+99		
447+47		
448+47		
449+25		
450+00		
451+00		
452+00		
453+00		
454+00		
455+00		

BALANCE NO. 15
CUT = 6204 CYS. (INCLUDES 659 CYS. TOPSOIL UNDERCUT IN SUBGRADE)
FILL = 61989 CYS. (INCLUDES 11,707 CYS. SUITABLE MATERIAL FOR SUBGRADE)
BORROW = 1917 CYS.
SPECIAL BORROW = 54,808 CYS.
ABOVE BORROW INCLUDES: 1977 CYS. FROM BALANCE NO. 17
THIS BALANCE INCLUDES: CUT FILL +30%
LINE S-6-G PR 577 CYS. 4563 CYS.
LINE S-4-B 560 CYS. 1988 CYS.



CLASS II DRIVE LT & PVT DRIVE
CROSSOVER REQ'D. STA. 267+14.3

STATE HIGHWAY
DEPARTMENT OF INDIANA
SEC. 9, T.26N., R.9W.

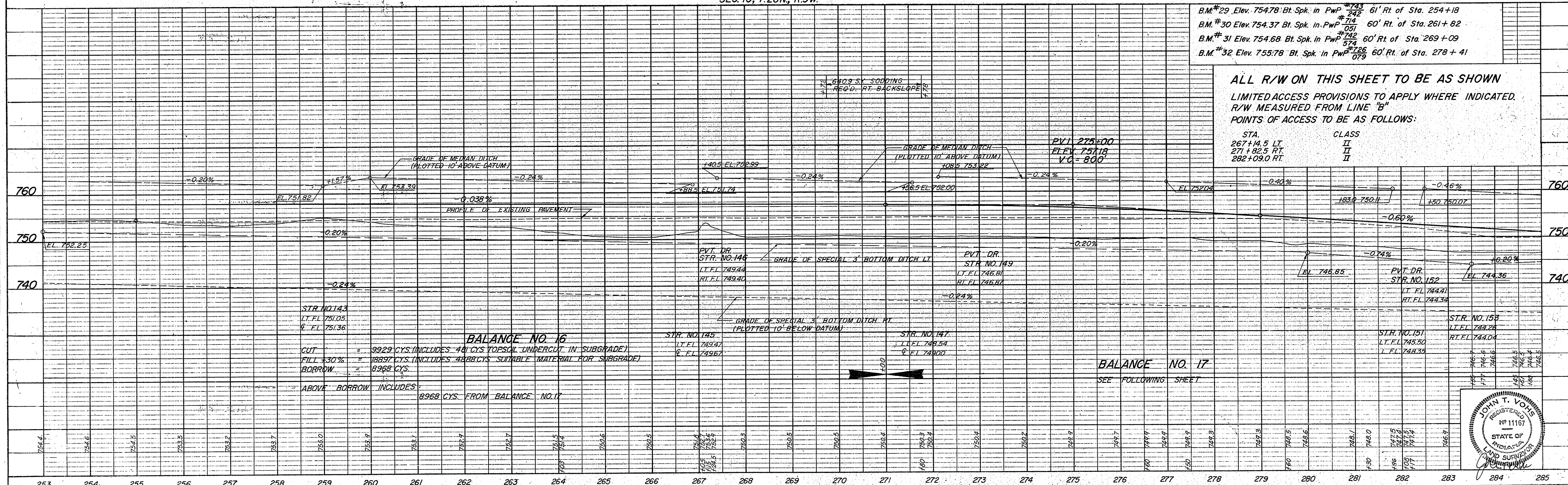
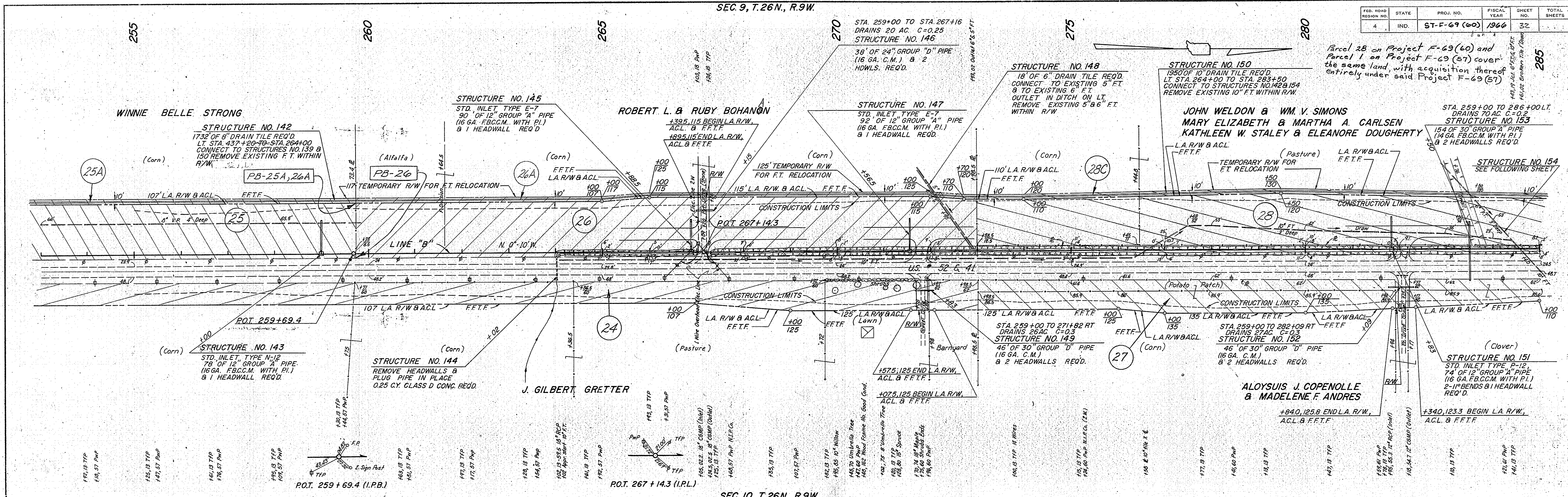
CLASS II DRIVE RT & PVT DRIVE
CROSSOVER REQ'D. STA. 271+82.5

CLASS II DRIVE RT & PVT DRIVE
CROSSOVER REQ'D. STA. 282+09.0

FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69 (60)	1966	32	

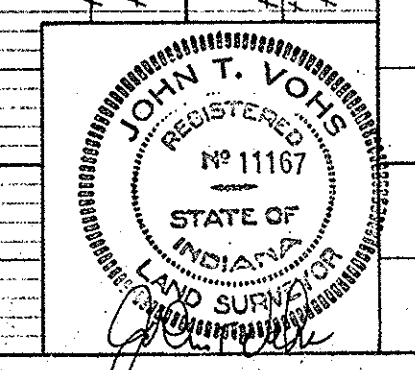
DATE	BY	REVISION
11-62	J.R.R.	1
12-62	J.R.R.	2

DATE	BY	REVISION
11-62	J.R.R.	1
12-62	J.R.R.	2



ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
R/W MEASURED FROM LINE "B"
POINTS OF ACCESS TO BE AS FOLLOWS:

STA.	CLASS
267+14.5 LT.	II
271+82.5 RT.	II
282+09.0 RT.	II



FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69(60)	1966	33	33

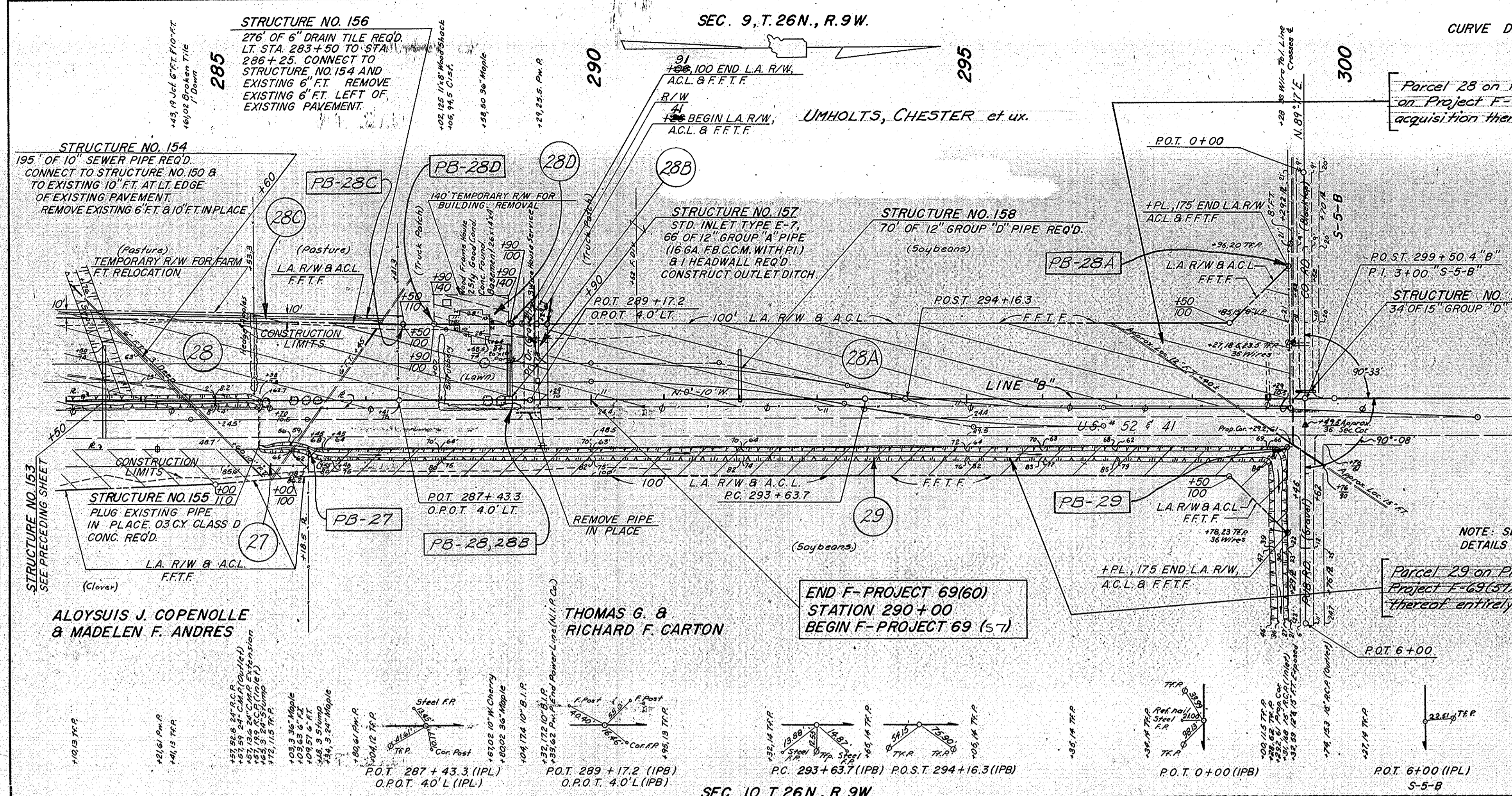
L.A. R/W Break Sta. 289+16 Lt. Rev. 7-26-65

CURVE DATA

Parcel 28 on Project F-69(60) and Parcel 1 on Project F-69(57) cover the same land, with acquisition thereof entirely under said Project F-69(57).

Parcel 29 on Project F-69(60) and Parcel 2 on Project F-69(57) cover the same land, with acquisition thereof entirely under said Project F-69(57).

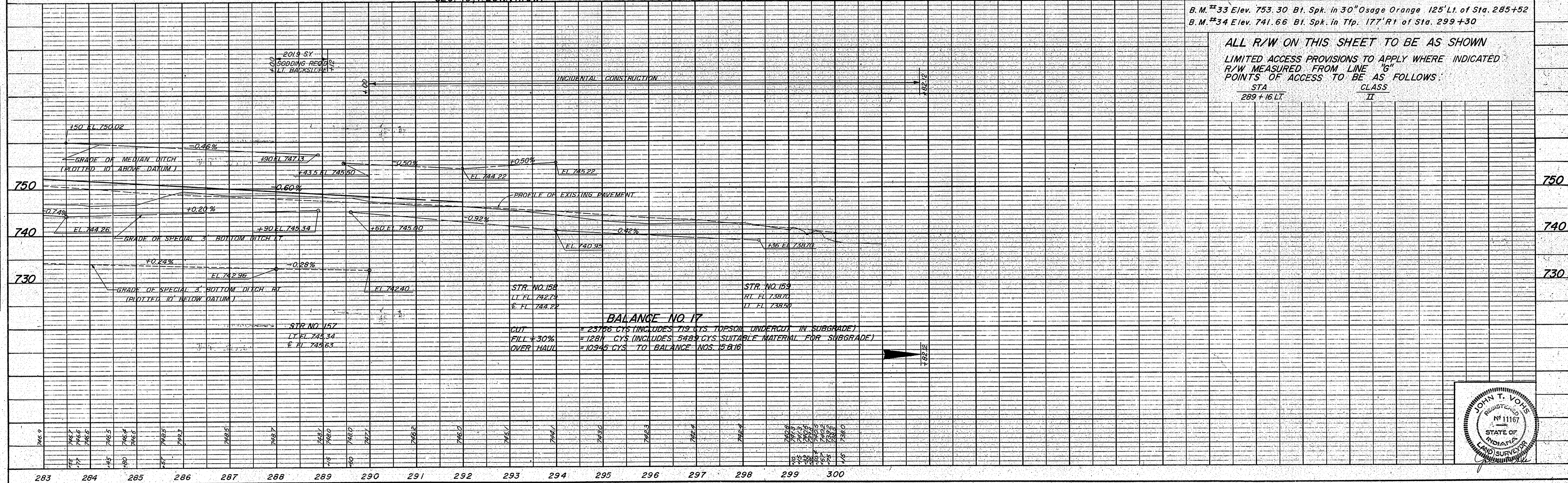
NOTE: SEE SHEET NO. 36 FOR DETAILS OF CROSS-OVER LANE



DATE	BY	REVISION
1/2-62	J.M.R.	PLANNING
1/2-62	J.M.R.	ALIGNED
1/2-62	J.M.R.	BY WAY CHECKED

PLAN	DATE	BY	REVISION
NO. 5599-4	1/2-62	J.M.R.	STRUCTURE NOTATIONS CHECKED

DATE	BY	REVISION
1/2-62	J.M.R.	PROFILES
1/2-62	J.M.R.	GRADES CHECKED
1/2-62	J.M.R.	NOTE
1/2-62	J.M.R.	STRUCTURE NOTATIONS CHECKED

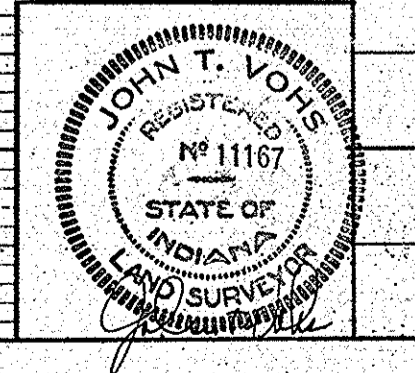


B.M. #33 Elev. 753.30 Bl. Spk. in 30" Osage Orange 125' Lt. of Sta. 285+52
B.M. #34 Elev. 741.66 Bl. Spk. in Twp. 177' Rt. of Sta. 299+30

ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED
R/W MEASURED FROM LINE 16"
POINTS OF ACCESS TO BE AS FOLLOWS:

STA	CLASS
289+16 LT	II

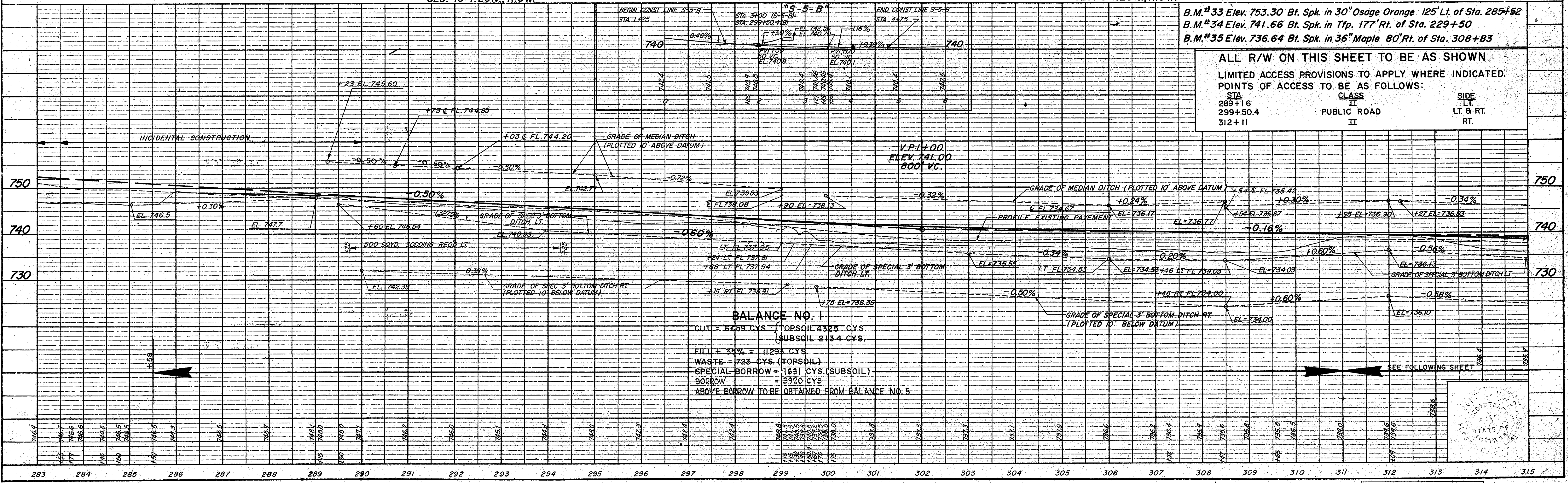
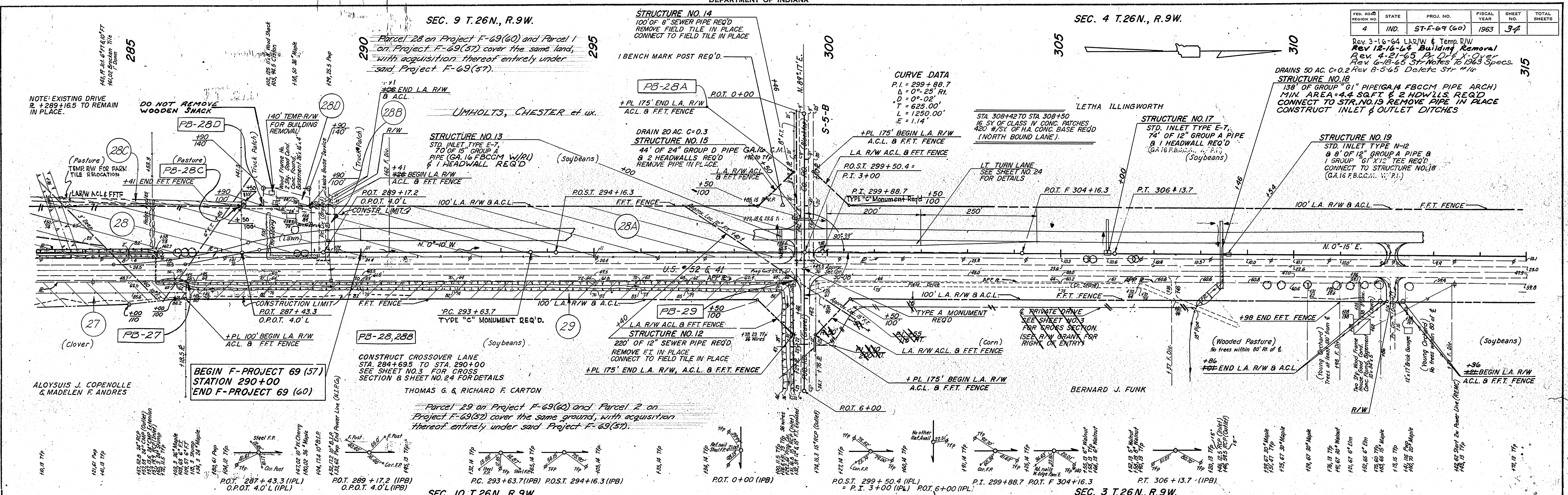
BALANCE NO. 17
CUT = 23796 CYS (INCLUDES 719 CYS TOPSOIL UNDERCUT IN SUBGRADE)
FILL = 1281 CYS (INCLUDES 5489 CYS SUITABLE MATERIAL FOR SUBGRADE)
OVER HAUL = 10963 CYS TO BALANCE NOS. 15 & 16



FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69 (60)	1963	34	

DATE	BY	REVISION
7-27-62	J.M.P.	1. PREPARED
8-1-62	J.M.P.	2. CHECKED
8-1-62	J.M.P.	3. APPROVED

DATE	BY	REVISION
7-27-62	J.M.P.	1. PREPARED
8-1-62	J.M.P.	2. CHECKED
8-1-62	J.M.P.	3. APPROVED



ALL R/W ON THIS SHEET TO BE AS SHOWN
 LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
 POINTS OF ACCESS TO BE AS FOLLOWS:

STA	CLASS	SIDE
289+16	II	LT. & RT.
299+50.4	II	RT.
312+11	II	RT.

CLASS II DRIVE LT. & PRIVATE DRIVE
CROSSOVER REQ'D. STA. 322+50

STATE HIGHWAY
DEPARTMENT OF INDIANA

CLASS II DRIVE RT. & PRIVATE DRIVE
CROSSOVER REQ'D. STA. 328+37.5

CLASS II DRIVE RT. & PRIVATE DRIVE
CROSSOVER REQ'D. STA. 336+28

CLASS V DRIVE RT. & EARTH
CROSSOVER REQ'D. STA. 342+70

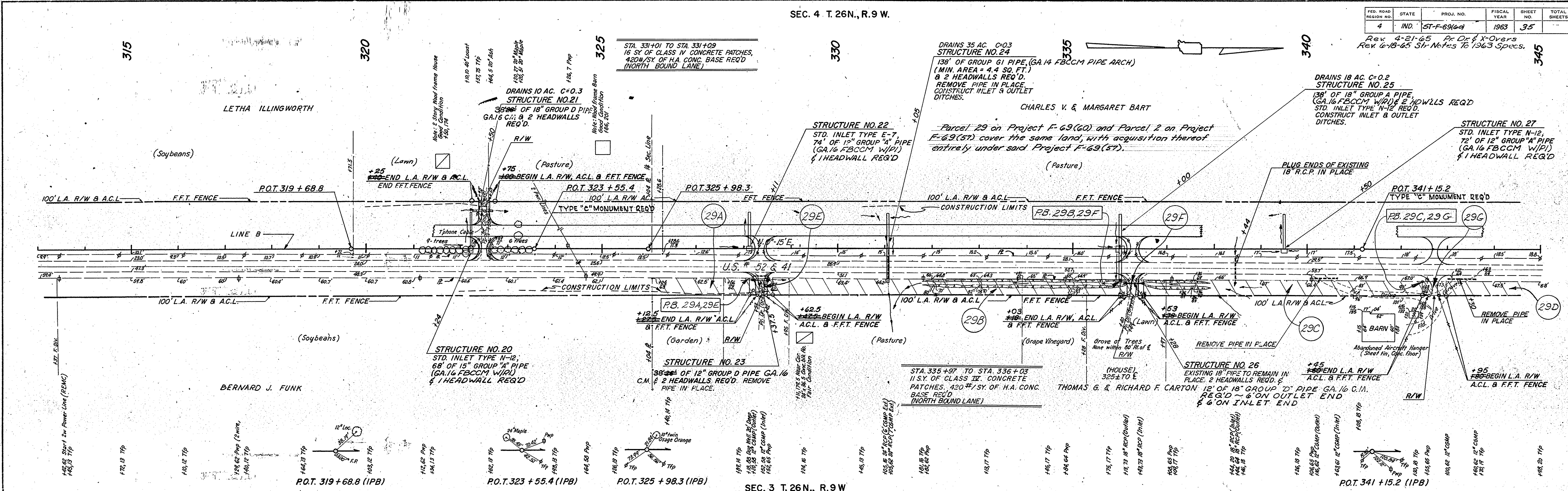
SEC. 4 T. 26N., R. 9 W.

FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	57-F-69(69)	1963	35	

Rev. 4-21-65 Pr. Dr. X-Overs
Rev. 6-18-65 Str. Notes To 1963 Specs.

DATE	BY	REVISION
11-22-62	W. J. L.	PREPARED
12-22-62	W. J. L.	ALIGNED
1-2-63	W. J. L.	RT. OF WAY CHECKED
2-2-63	W. J. L.	NO. 55967

DATE	BY	REVISION
11-22-62	W. J. L.	PREPARED
12-22-62	W. J. L.	ALIGNED
1-2-63	W. J. L.	RT. OF WAY CHECKED
2-2-63	W. J. L.	NO. 55967

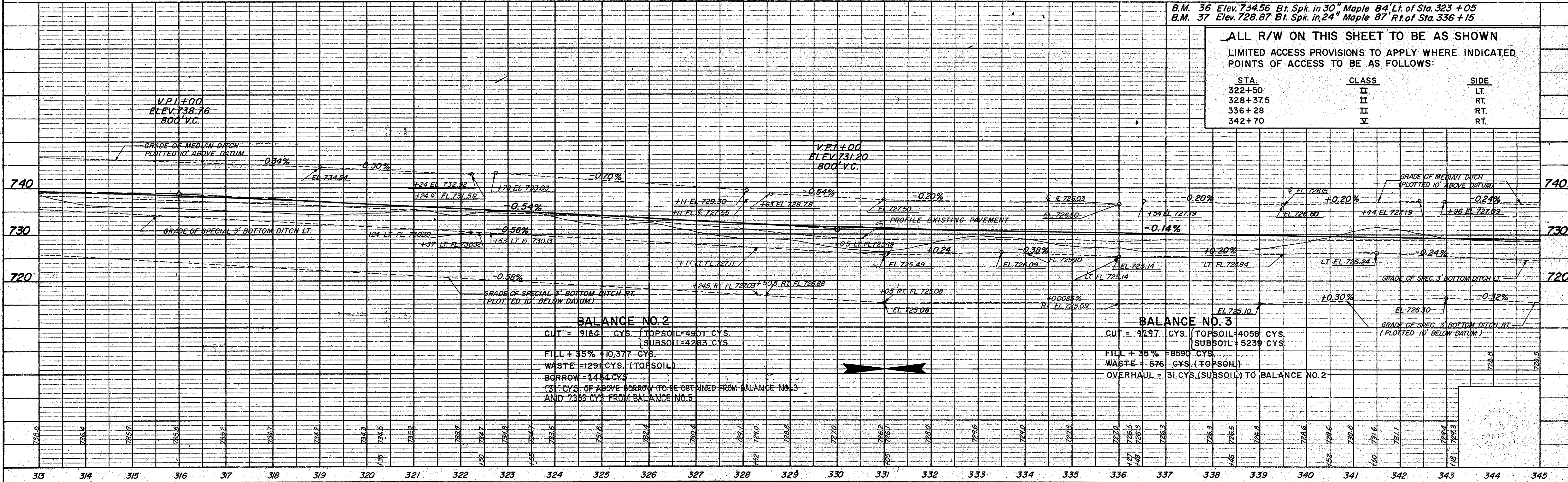


SEC. 3 T. 26N., R. 9 W.

B.M. 36 Elev. 734.56 Bt. Spk. in 30" Maple 84' Lt. of Sta. 323+05
B.M. 37 Elev. 728.87 Bt. Spk. in 24" Maple 87' Rt. of Sta. 336+15

ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED
POINTS OF ACCESS TO BE AS FOLLOWS:

STA.	CLASS	SIDE
322+50	II	LT.
328+37.5	II	RT.
336+28	II	RT.
342+70	V	RT.



TYPE "B" ROAD APPROACH LT. & RT. & CROSSOVER TYPE "B" REQ'D. STA. 350+23.15

STATE HIGHWAY DEPARTMENT OF INDIANA

CLASS II DRIVE REQ'D LT STA 361+23 & PRIVATE DRIVE CROSSOVER REQ'D STA. 361+00

CLASS II DRIVE REQ'D RT. STA. 364+80 & PRIVATE DRIVE CROSSOVER REQ'D STA. 365+00

SEC. 4 T.26N., R.9W BENTON COUNTY

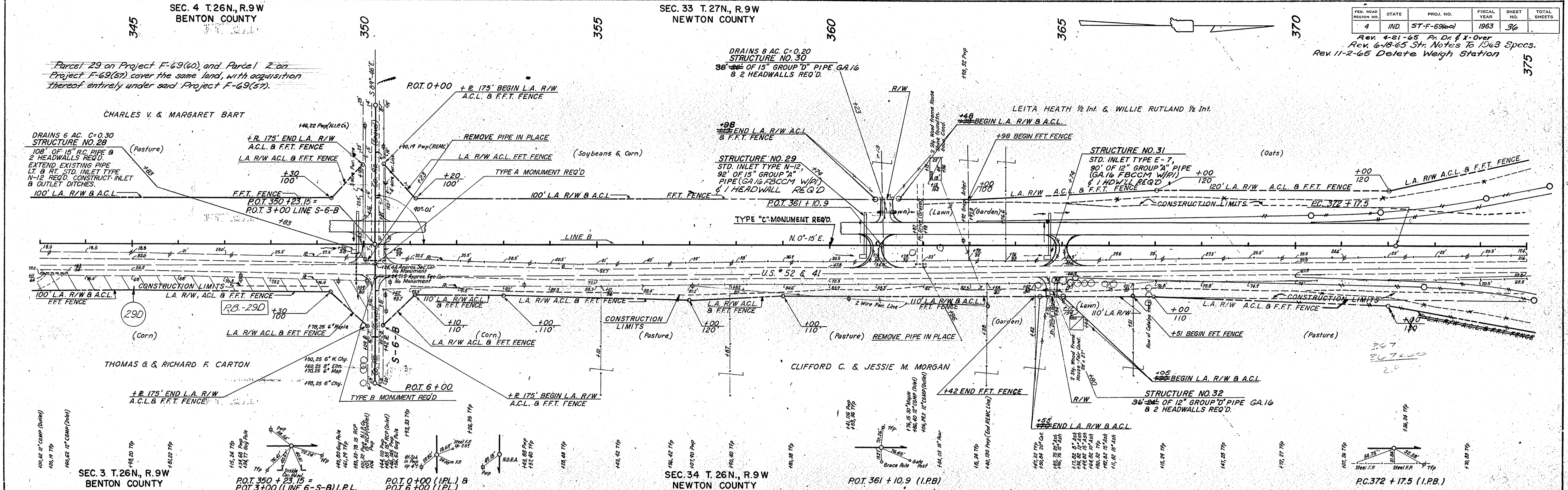
SEC. 33 T.27N., R.9W NEWTON COUNTY

FED. ROAD REGION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND	57-F-69(2)	1963	36	

Rev. 4-21-65 Pr. Dr. & X-Over
Rev. 6-18-65 Sta. Notes To 1963 Specs.
Rev. 11-2-65 Delete Weigh Station

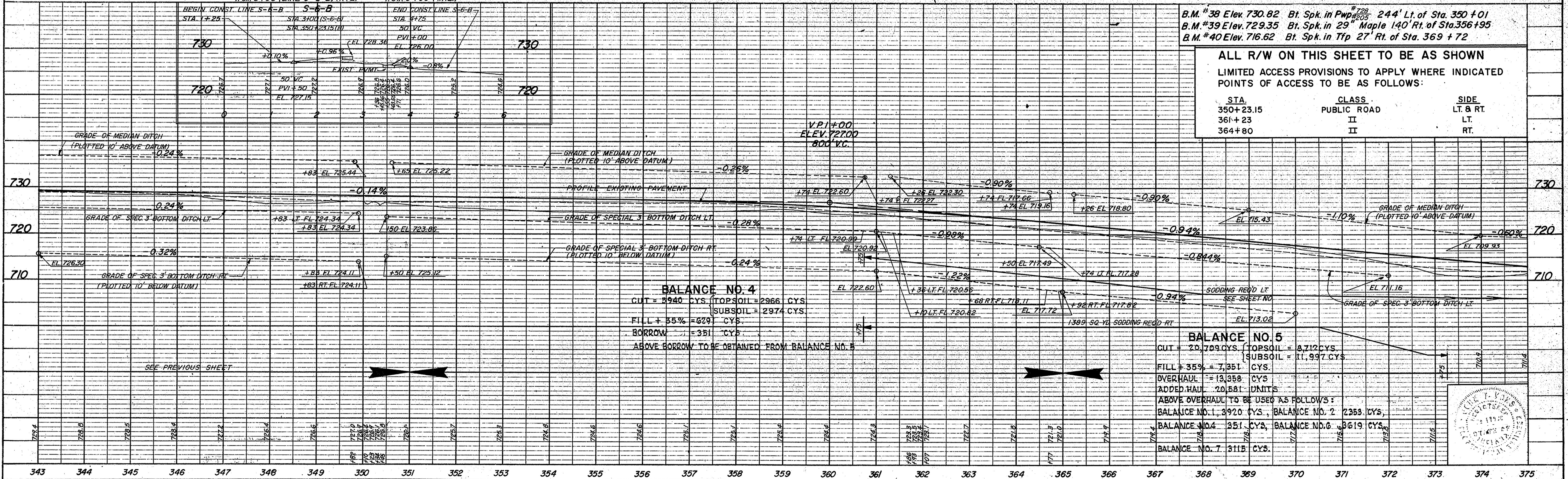
DATE	BY	REVISION
11-62	V.R.R.	1-1
11-62	V.R.R.	1-2
11-62	V.R.R.	1-3
11-62	V.R.R.	1-4
11-62	V.R.R.	1-5
11-62	V.R.R.	1-6
11-62	V.R.R.	1-7

NO. 52967 T
STRUCTURE NOTARY'S CHRD



DATE	BY	REVISION
11-62	V.R.R.	1-1
11-62	V.R.R.	1-2
11-62	V.R.R.	1-3
11-62	V.R.R.	1-4
11-62	V.R.R.	1-5
11-62	V.R.R.	1-6
11-62	V.R.R.	1-7

NO. 52967 L
STRUCTURE NOTARY'S CHRD



ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED
POINTS OF ACCESS TO BE AS FOLLOWS:

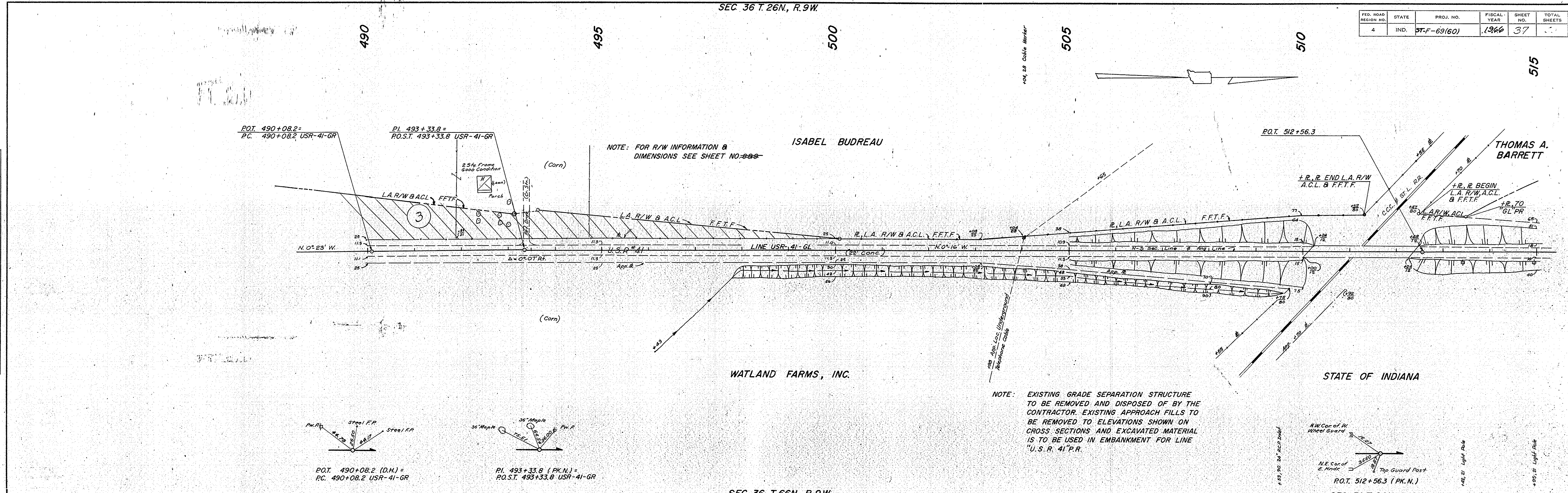
STA.	CLASS.	SIDE
350+23.15	PUBLIC ROAD	LT. & RT.
361+23	II	LT.
364+80	II	RT.

STATE HIGHWAY
DEPARTMENT OF INDIANA
SEC. 36 T. 26N., R. 9W.

FED. ROAD REGION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69(60)	1966	37	

DATE	BY	REVISION
6-24-63	J.S.H.C.	
7-2-63	T.J.P.	
12-6-63	T.M.E.	
12-6-63		

DATE	BY	REVISION
6-24-63	J.S.H.C.	
7-2-63	T.J.P.	
12-6-63	T.M.E.	
12-6-63		

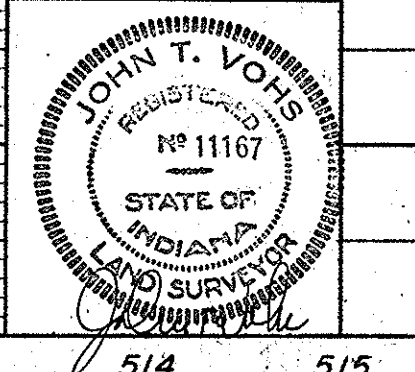
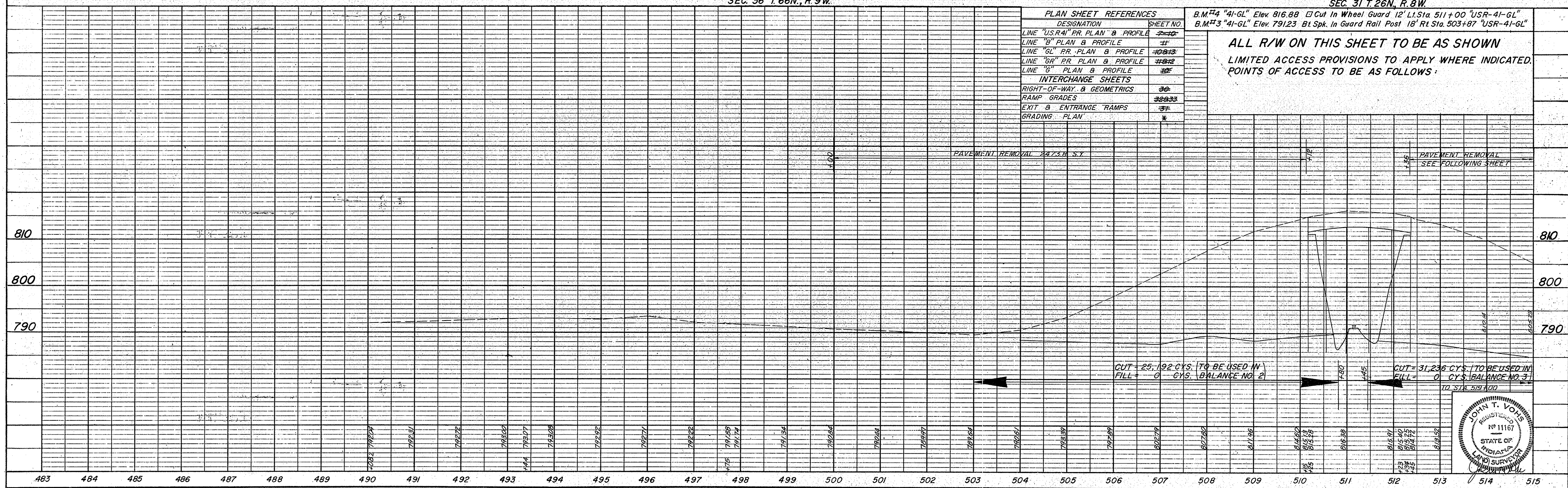


SEC. 36 T. 66N., R. 9W.

DESIGNATION	SHEET NO.
LINE "USR41" P.R. PLAN & PROFILE	26
LINE "B" PLAN & PROFILE	27
LINE "GL" P.R. PLAN & PROFILE	28
LINE "GR" P.R. PLAN & PROFILE	29
LINE "G" PLAN & PROFILE	30
INTERCHANGE SHEETS	
RIGHT-OF-WAY & GEOMETRICS	31
RAMP GRADES	32
EXIT & ENTRANCE RAMPS	33
GRADING PLAN	34

SEC. 31 T. 26N., R. 8W.
B.M. #4 "41-GL" Elev. 816.88 Cut in Wheel Guard 12' Lt. Sta. 511+00 "USR-41-GL"
B.M. #3 "41-GL" Elev. 791.23 Bl. Spk. in Guard Rail Post 18' Rt. Sta. 503+87 "USR-41-GL"

ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.
POINTS OF ACCESS TO BE AS FOLLOWS:



FED. ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	BT-F-69(60)	1966	38	

SEC. 36 T.26N., R.9W.

CURVE DATA

PI = 520+176
 $\Delta = 11^{\circ}25'17''$
 $D = 2^{\circ}00'$
 $T = 286.4'$
 $L = 570.8'$
 $E = 14.30'$

515

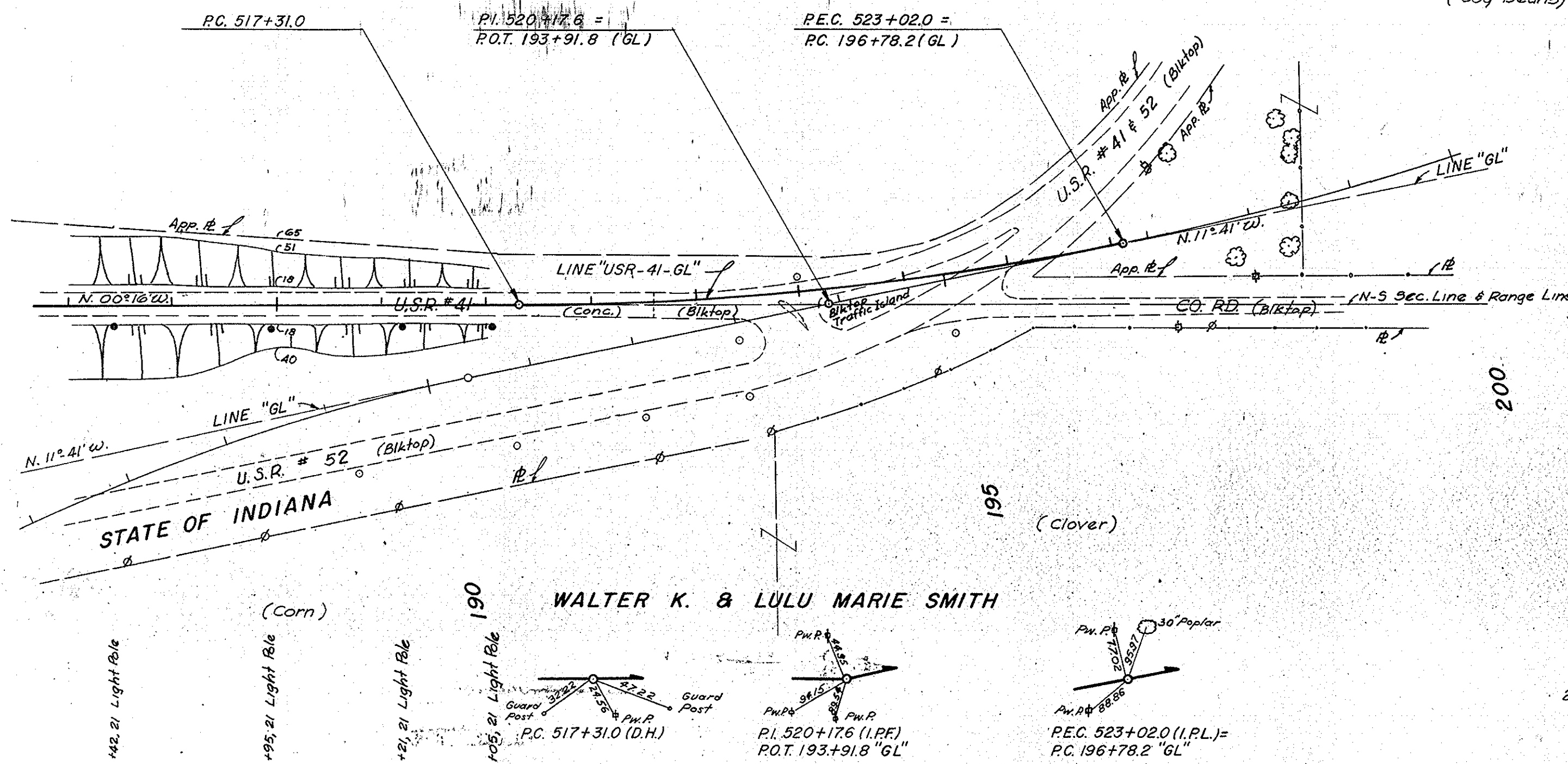
520

THOMAS A. BARRETT

THOMAS A. BARRETT
(Soy Beans)

DATE	BY
12-63	S.H.C.
12-63	T.J.P.

PLAN
 SURVEYED
 NOTE BOOK NO. 8922-7
 ALIGNMENT CHECKED
 RT. OF WAY CHECKED

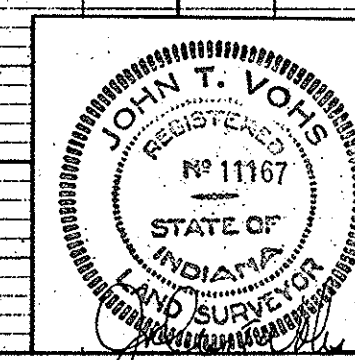
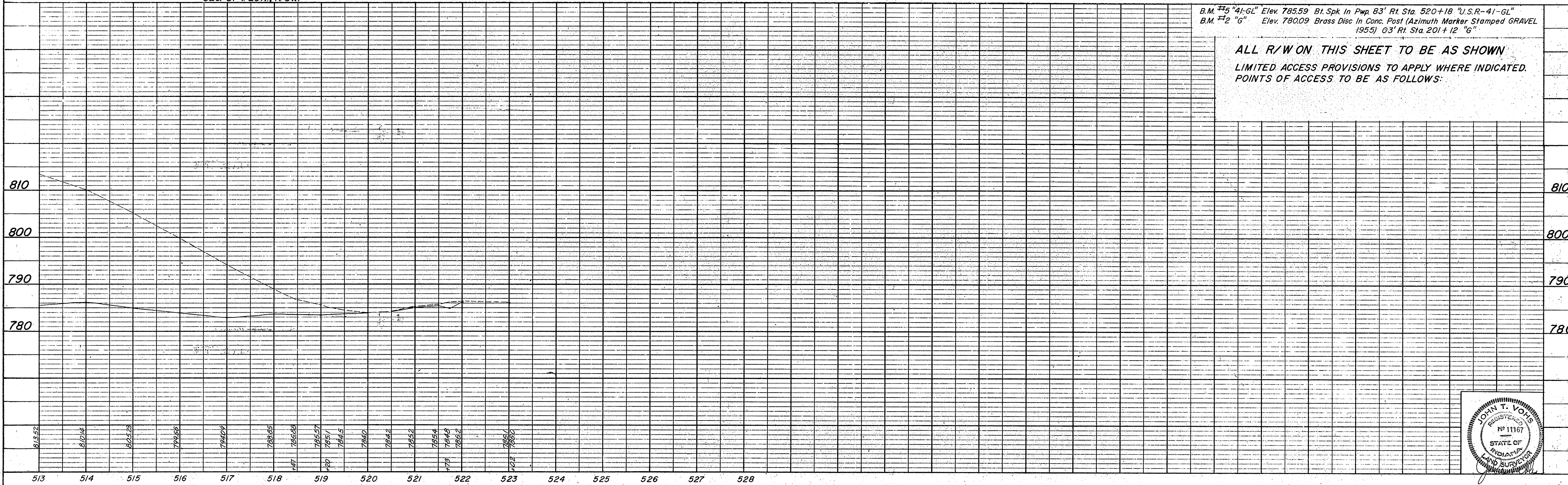


SUMMARY PAVEMENT REMOVAL
 1. USR 41 EXISTING PROJ. F-69(B) STA. 512+36 TO STA. 550+48.0 9720.8 SY
 2. USR 42 EXISTING PROJ. F-75 (D) STA. 1595+44.5 TO STA. 1619+32.6 7031.7 SY
 TOTAL 16,752.5 SY

SEC. 31 T.26N., R.8W.

DATE	BY
12-63	S.H.C.
12-63	T.J.P.

PROFILE
 SURVEYED
 NOTE BOOK NO. 8929-4
 GRADES CHECKED
 B.M. NOTED
 STRUCTURE NOTINGS CHECKED



CLASS II DRIVE REQ'D
RT STA. 46+45.0

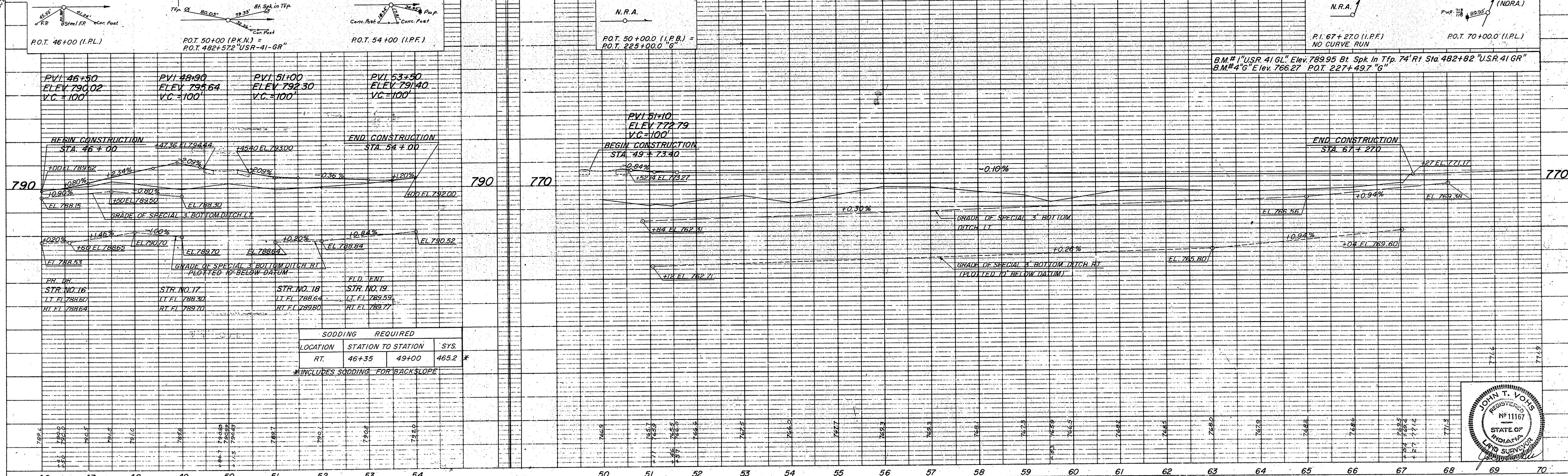
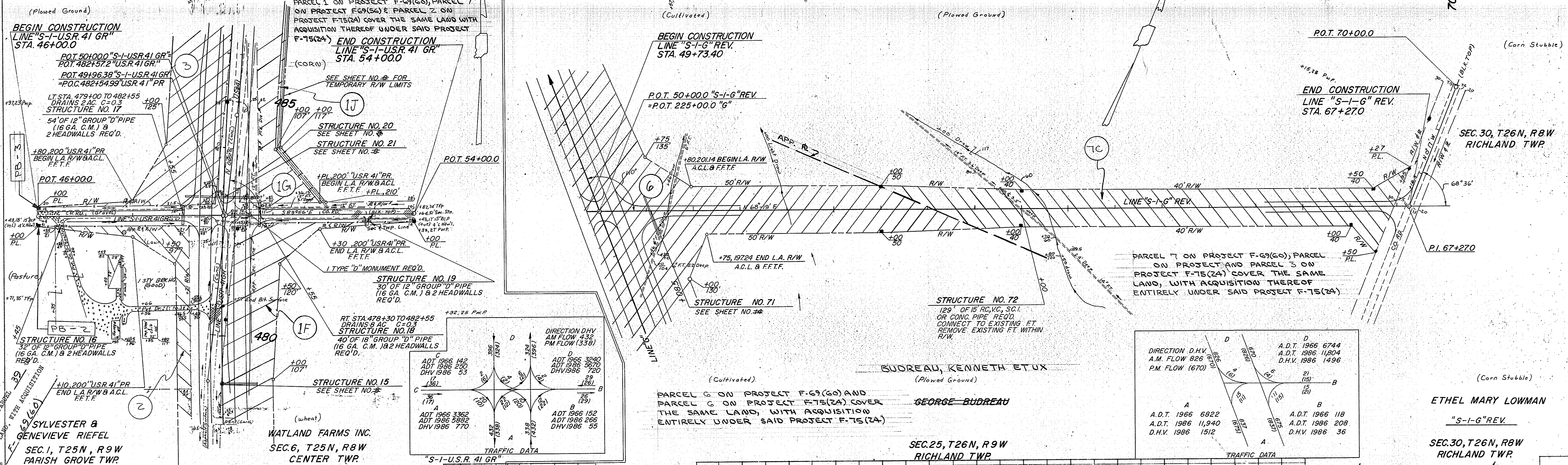
SEC. 36, T26N, R9W
RICHLAND TWP.
ISABEL BUDREAU
(Plowed Ground)

CLASS II DRIVE REQ'D
RT STA. 53+00

SEC. 31, T26N, R8W
RICHLAND TWP.
WATLAND FARMS INC.
PARCEL 1 ON PROJECT F-69(60), PARCEL 1
ON PROJECT F-69(56) & PARCEL 2 ON
PROJECT F-15(24) COVER THE SAME LAND WITH
ACQUISITION THEREOF UNDER SAID PROJECT
F-15(24) END CONSTRUCTION
LINE "S-I-GR" REV.
STA. 54+00.0

SEC. 25, T26N, R9W
RICHLAND TWP.
GEORGE BUDREAU
(Plowed Ground)

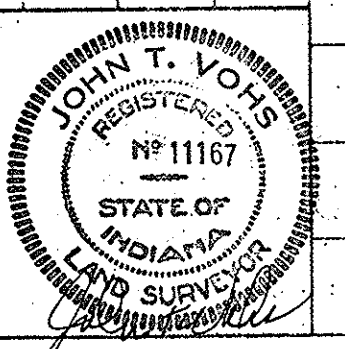
FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	F-69(60)	1966	39	



SODDING REQUIRED

LOCATION	STATION TO STATION	SYS.
RT.	46+35 49+00 465.2 *	

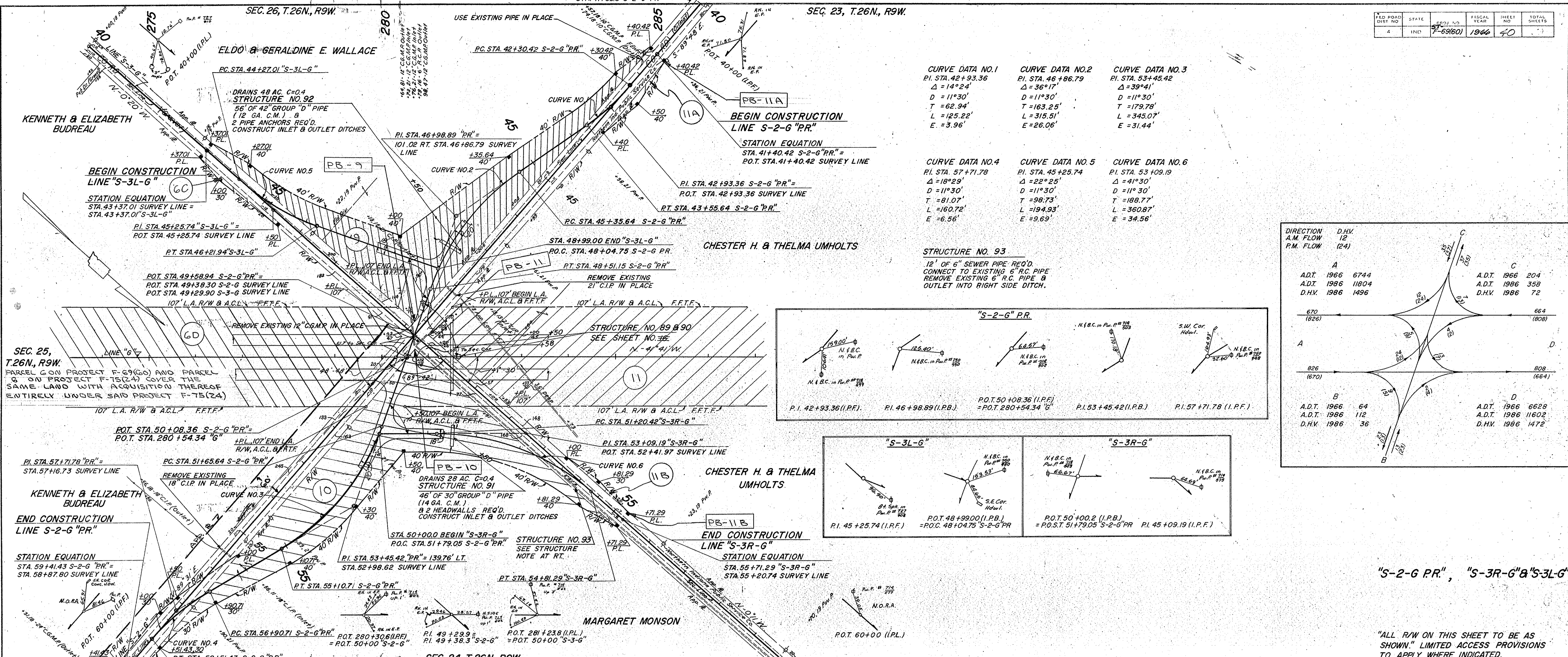
*INCLUDES SODDING FOR BACKSLOPE



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	7-6960	1966	40	7

SEC. 26, T.26N., R.9W.

SEC. 23, T.26N., R.9W.



CURVE DATA NO.1

PI. STA. 42+93.36
Δ = 14°24'
D = 11°30'
T = 62.94'
L = 125.22'
E = 3.96'

CURVE DATA NO.2

PI. STA. 46+86.79
Δ = 36°17'
D = 11°30'
T = 163.25'
L = 315.51'
E = 26.06'

CURVE DATA NO.3

PI. STA. 53+45.42
Δ = 39°41'
D = 11°30'
T = 179.78'
L = 345.07'
E = 31.44'

CURVE DATA NO.4

PI. STA. 57+71.78
Δ = 18°29'
D = 11°30'
T = 81.07'
L = 160.72'
E = 6.56'

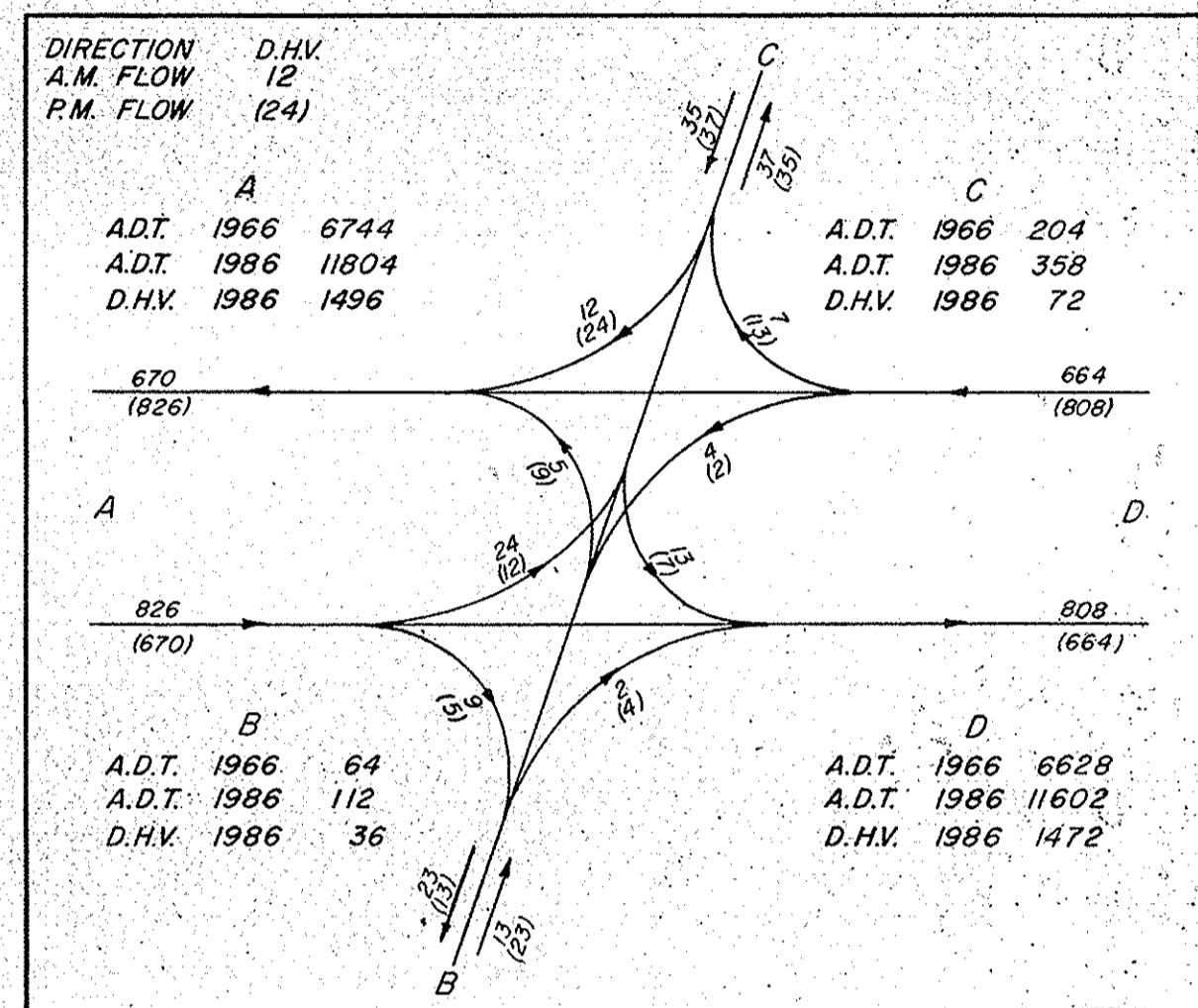
CURVE DATA NO.5

PI. STA. 45+25.74
Δ = 22°25'
D = 11°30'
T = 98.73'
L = 194.93'
E = 9.69'

CURVE DATA NO.6

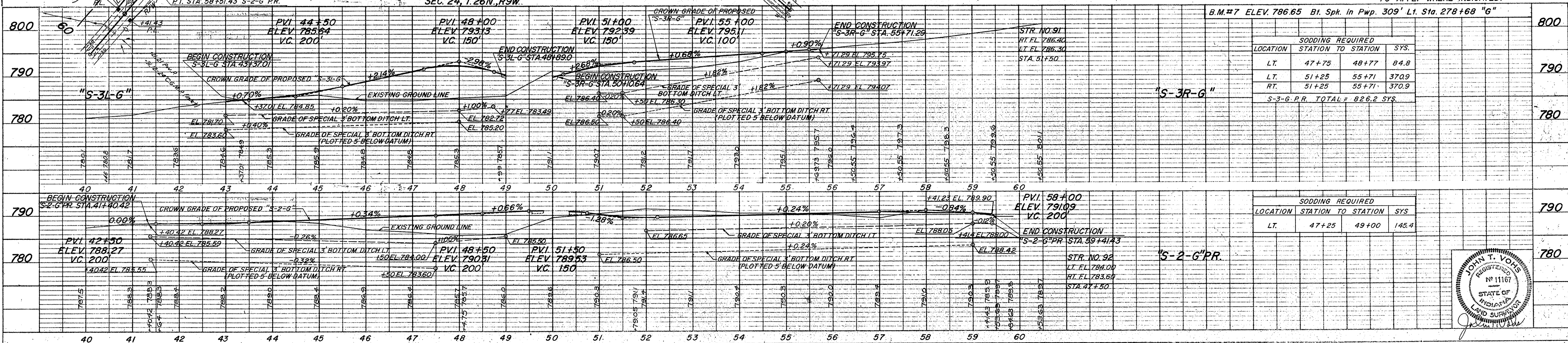
PI. STA. 53+09.19
Δ = 41°30'
D = 11°30'
T = 188.77'
L = 360.87'
E = 34.56'

STRUCTURE NO. 93
 12" OF 6" SEWER PIPE REQ'D.
 CONNECT TO EXISTING 6" R.C. PIPE
 REMOVE EXISTING 6" R.C. PIPE &
 OUTLET INTO RIGHT SIDE DITCH.



PLAN
 DATE: 12/27/65
 BY: J.S. C. / E.R.A.
 CHECKED: J.S. C. / E.R.A.
 NO. 89262 - 2 OF 40 SHEETS

PROFILE
 DATE: 12/27/65
 BY: J.S. C. / E.R.A.
 CHECKED: J.S. C. / E.R.A.
 NO. 89262 - 2 OF 40 SHEETS



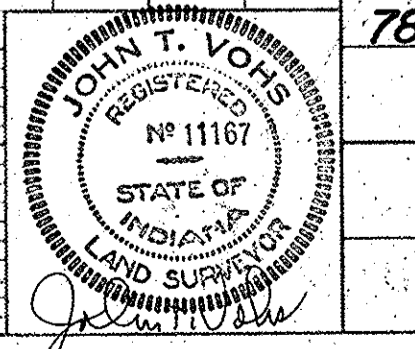
SODDING REQUIRED

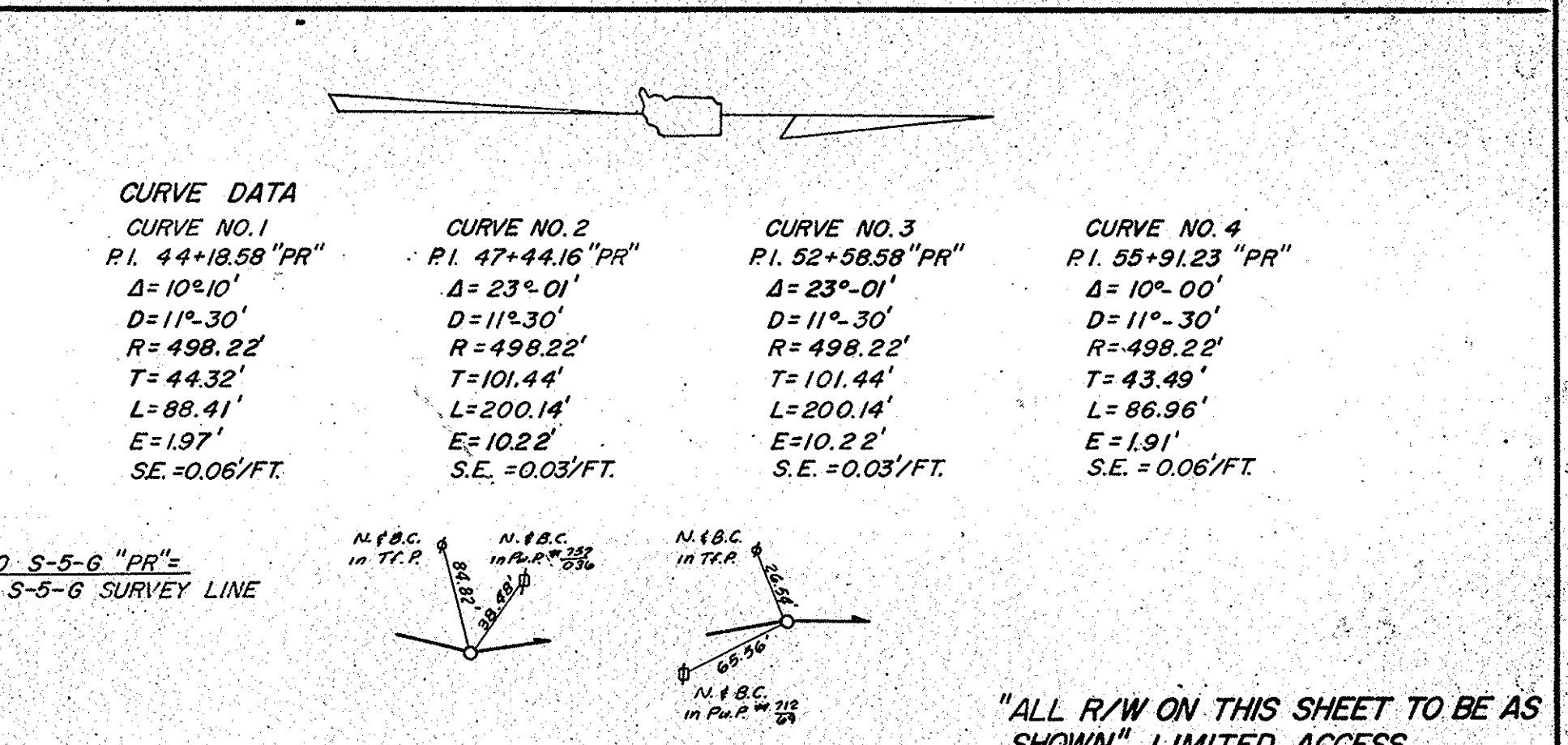
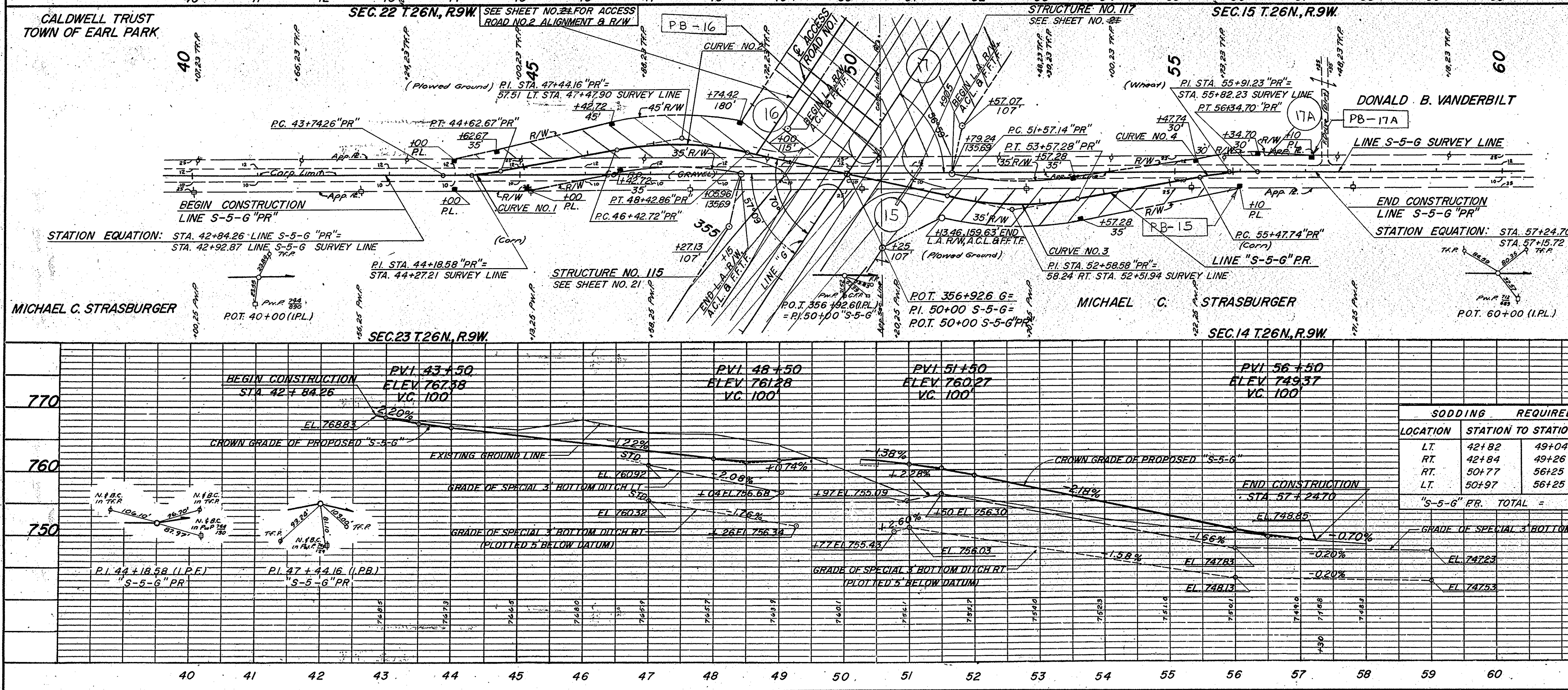
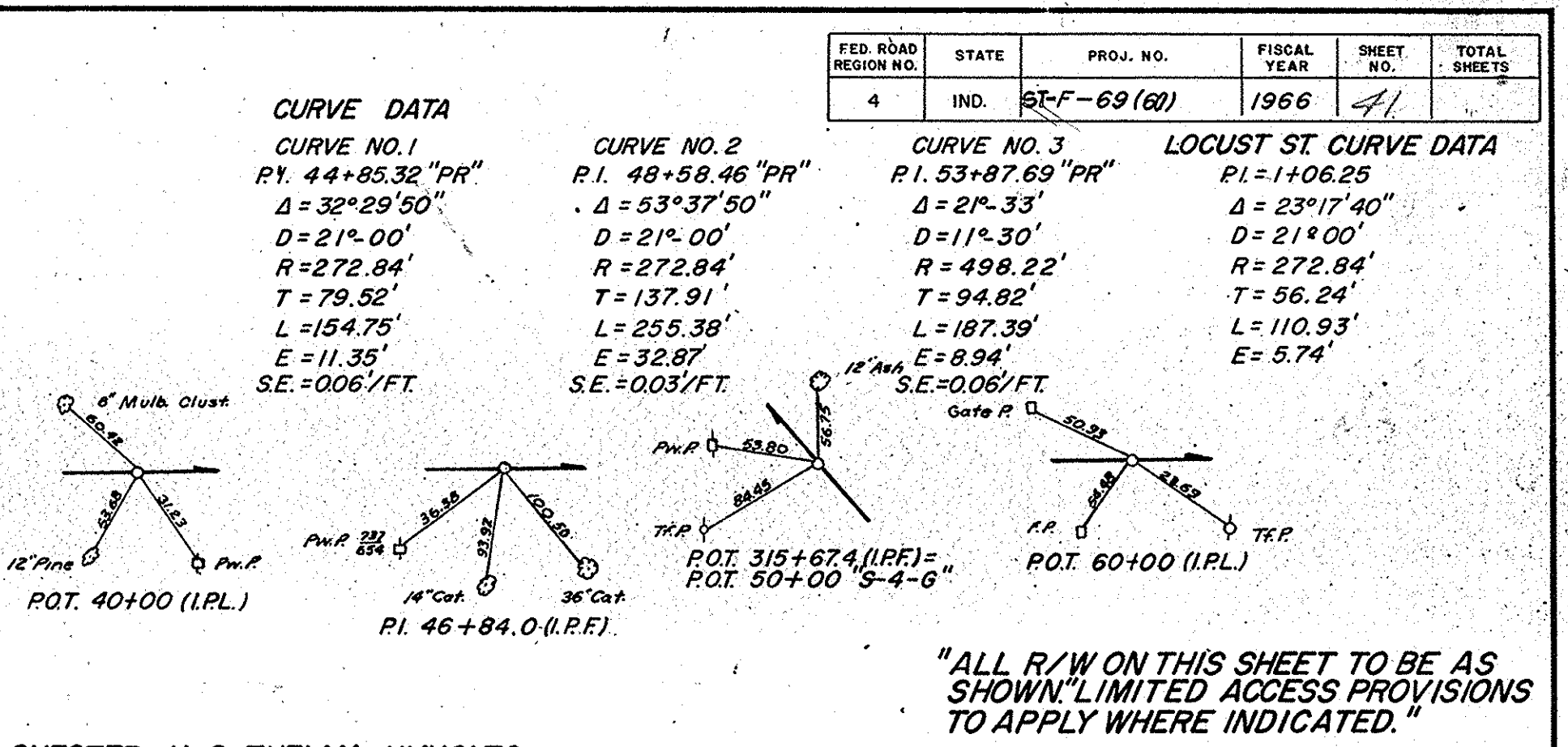
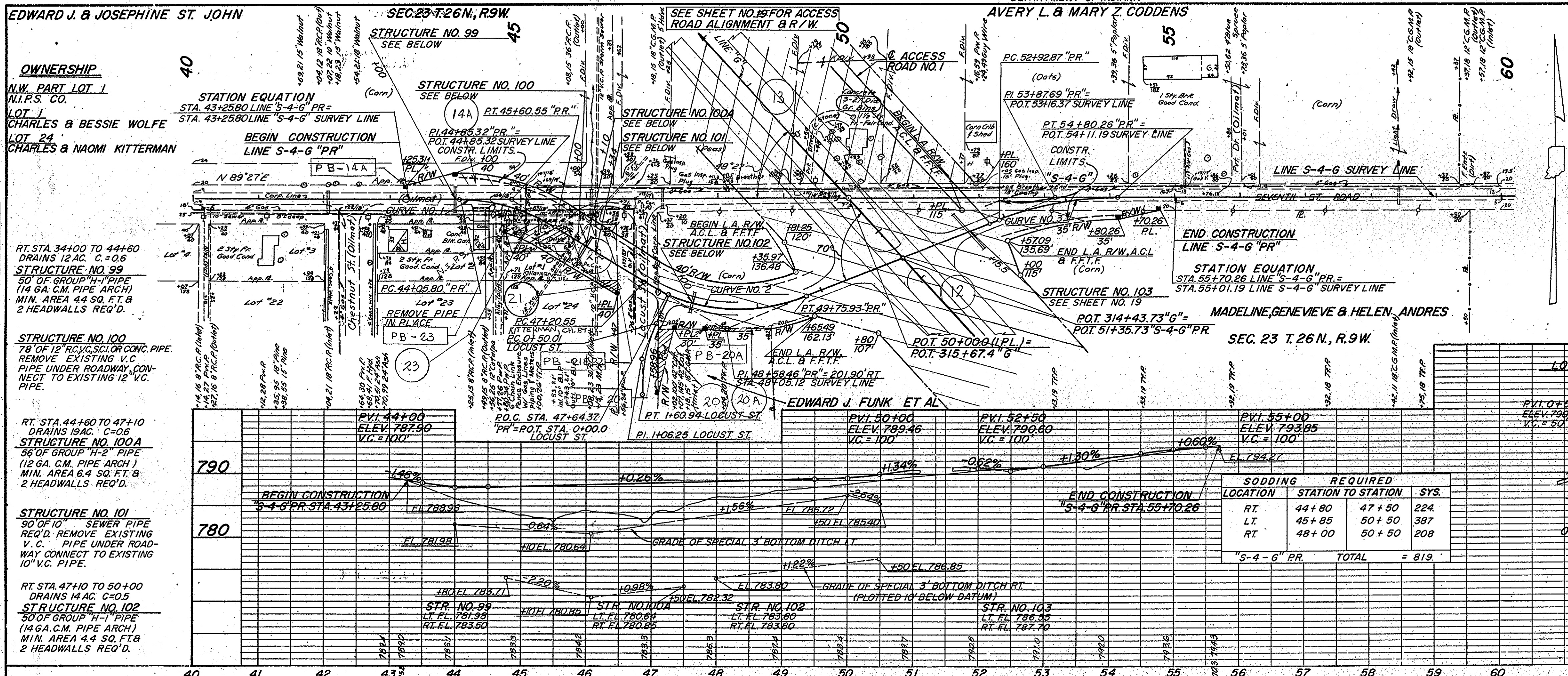
LOCATION	STATION TO STATION	SYS.
LT.	47+75 - 48+77	84.8
RT.	51+25 - 55+71	370.9
RT.	51+25 - 55+71	370.9
S-3-G P.R. TOTAL = 826.2 SYS.		

SODDING REQUIRED

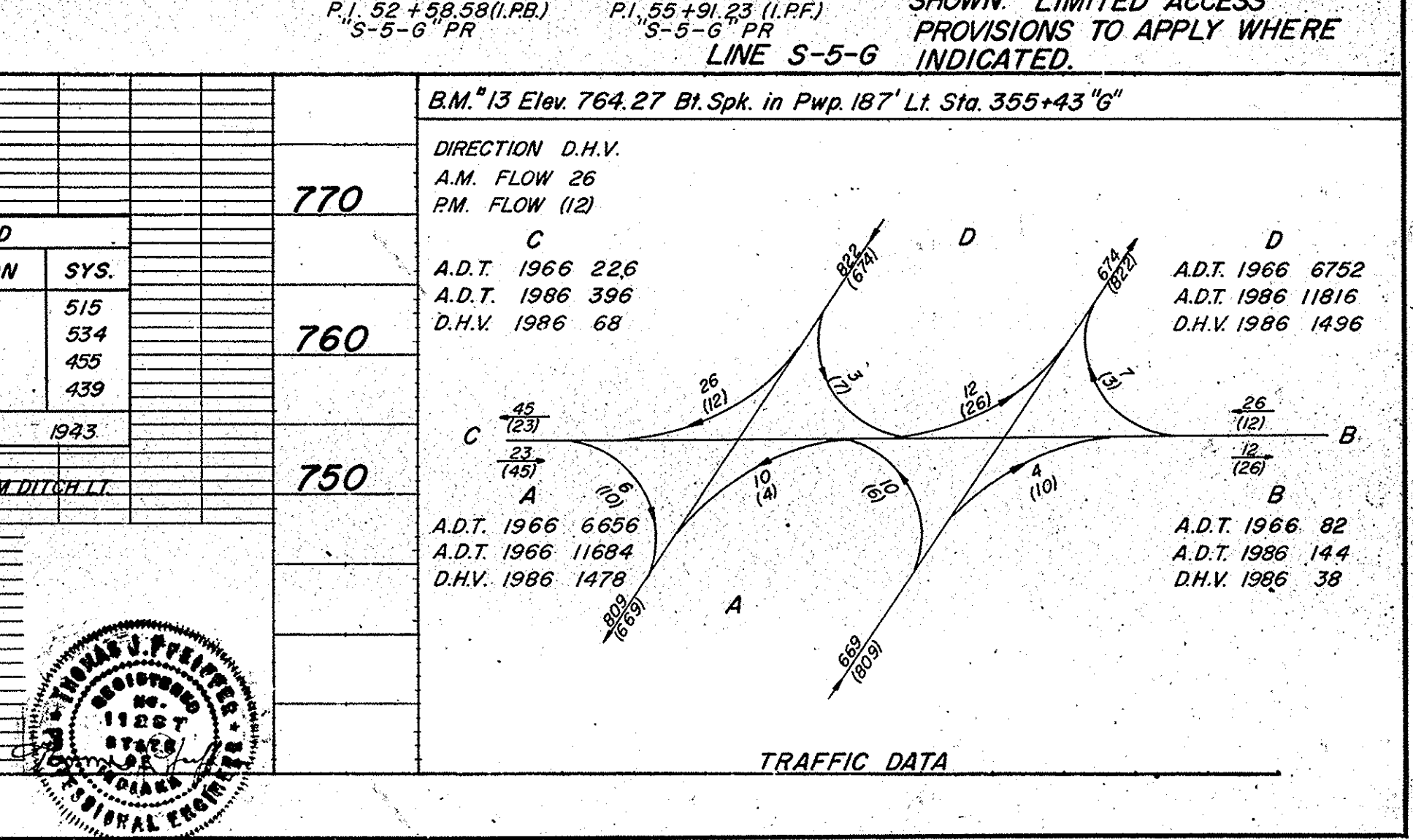
LOCATION	STATION TO STATION	SYS.
LT.	47+25 - 49+00	145.4

"ALL R/W ON THIS SHEET TO BE AS SHOWN." LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.





STATION	ELEV.	VC	VC	VC	VC
42+78.26	767.89	100'	100'	100'	100'
48+50	761.28	100'	100'	100'	100'
51+50	760.27	100'	100'	100'	100'
56+50	749.37	100'	100'	100'	100'



FED. ROAD REGION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	6T-F-69(60)	1966	42	

SEC. 16 T. 26N., R. 9W.

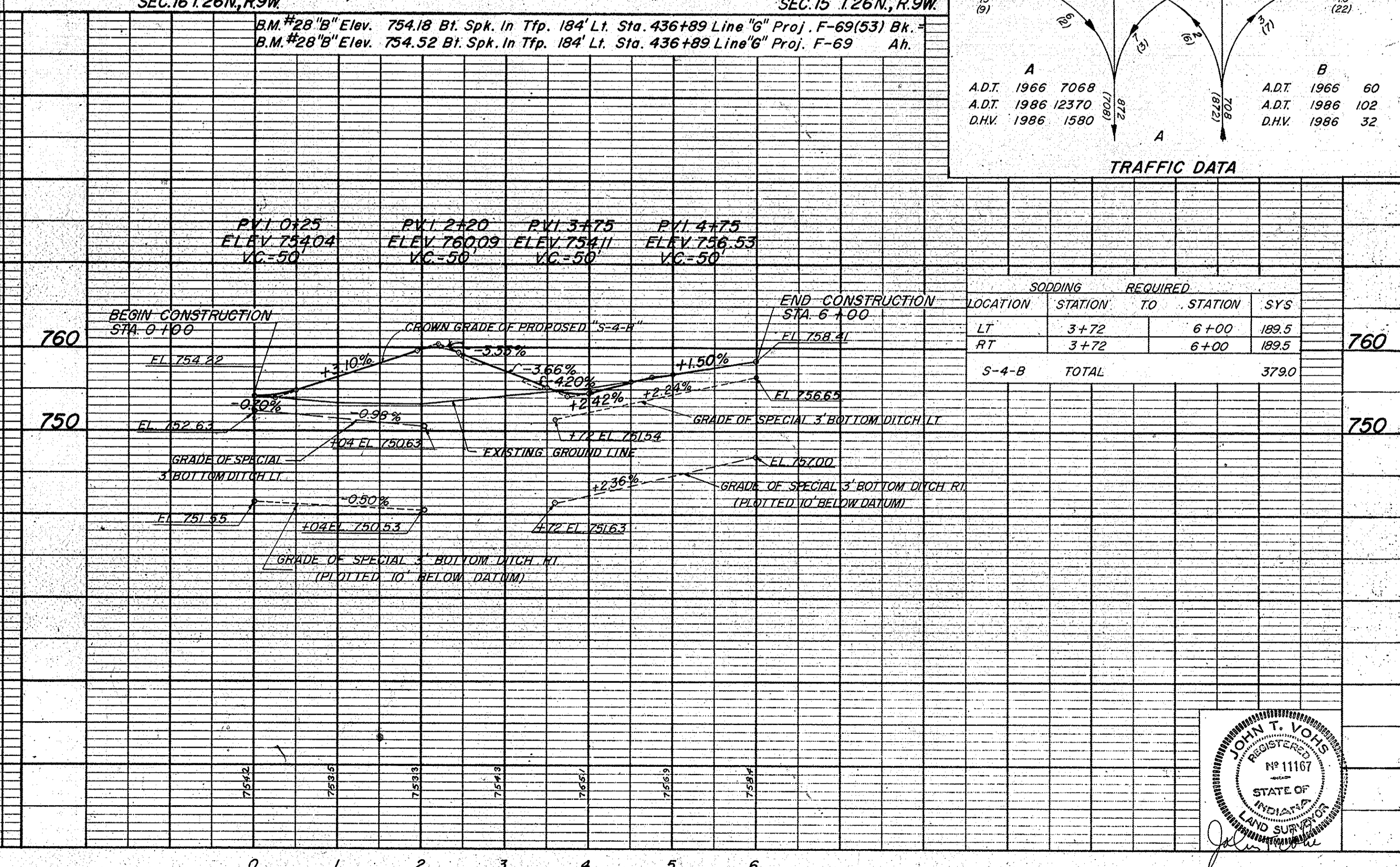
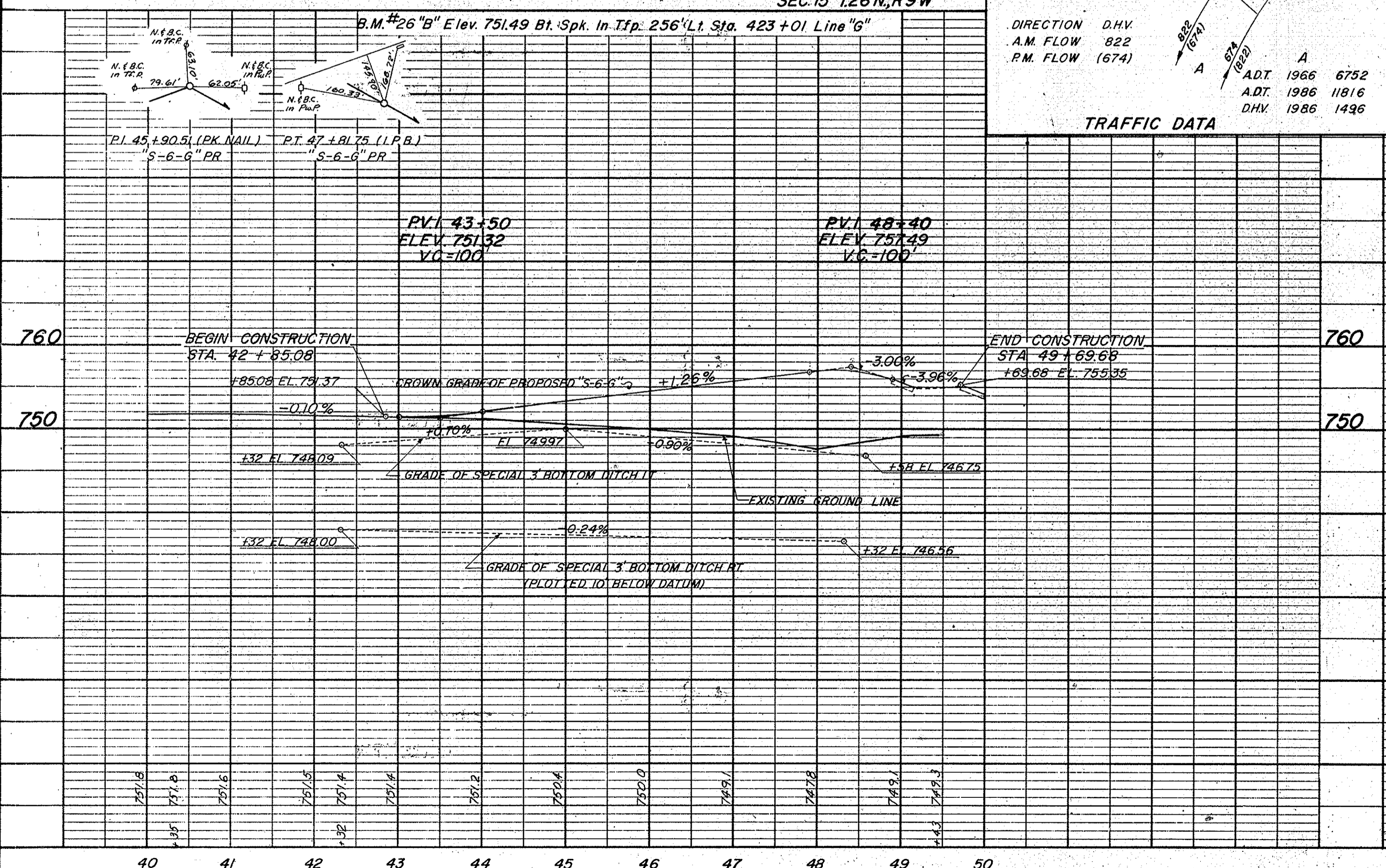
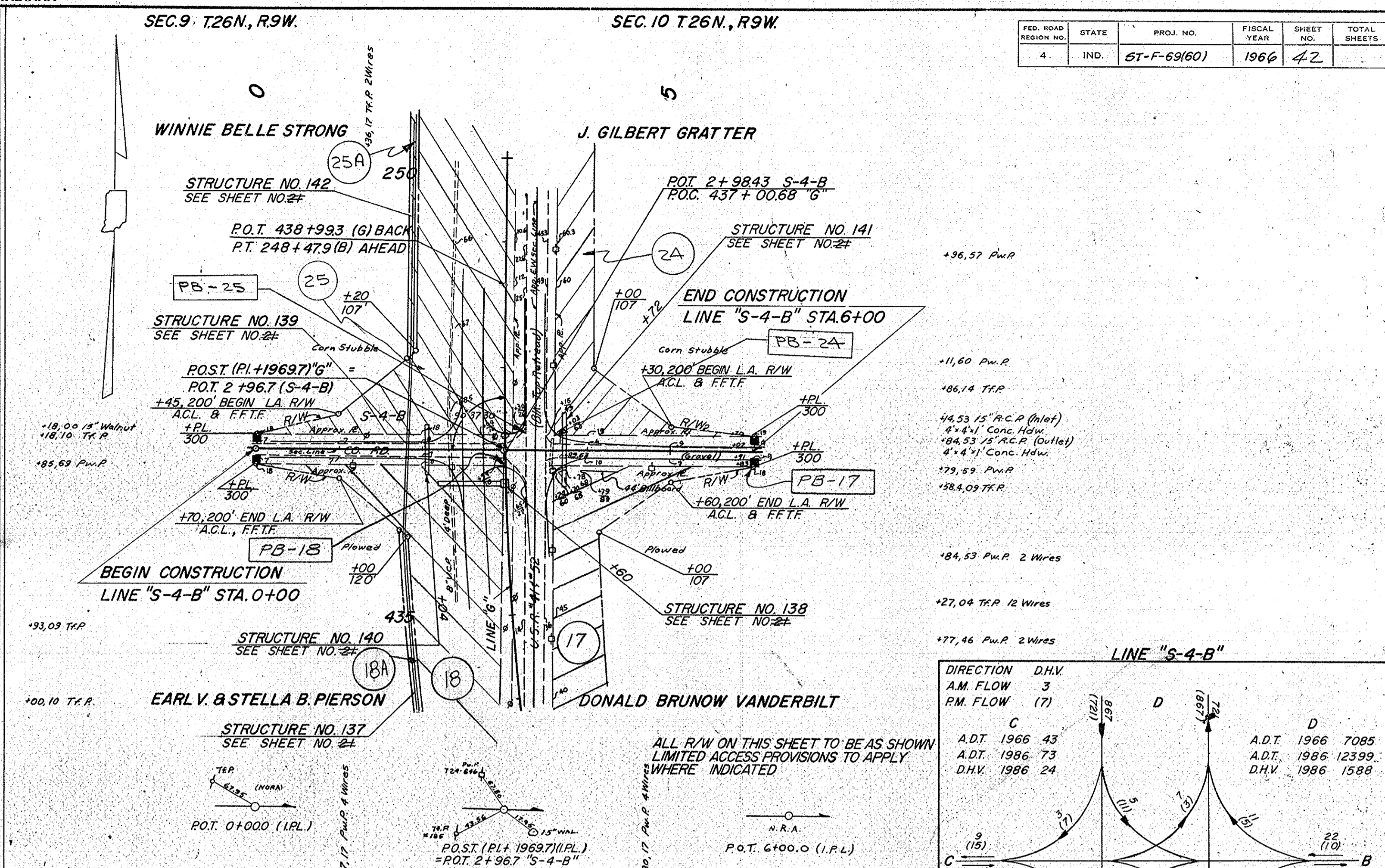
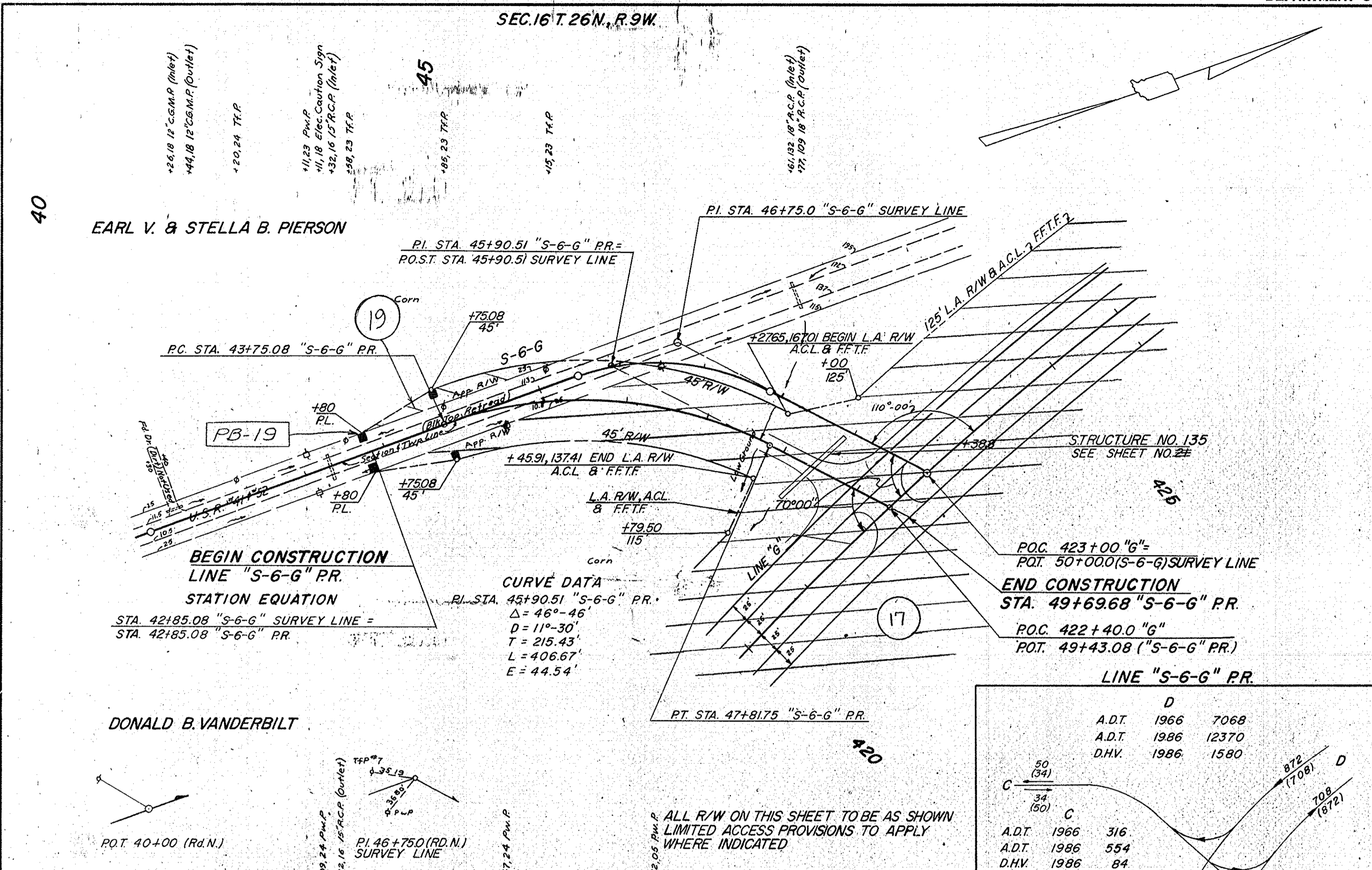
SEC. 9 T. 26N., R. 9W.

SEC. 10 T. 26N., R. 9W.

DATE	BY
5-13-64	J.S. McC.
5-14-64	J.L.M.
5-15-64	J.L.M.

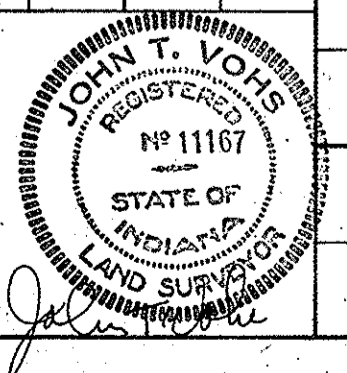
PLAN	NOTE BOOK
NO. 8282-72	NO. 8282-72

PROFILE	NOTE BOOK
NO. 8282-72	NO. 8282-72



TRAFFIC DATA

DIRECTION	D.H.V.
A.M. FLOW	3
P.M. FLOW (7)	1580



FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	57-F-69(60)	1966	3	

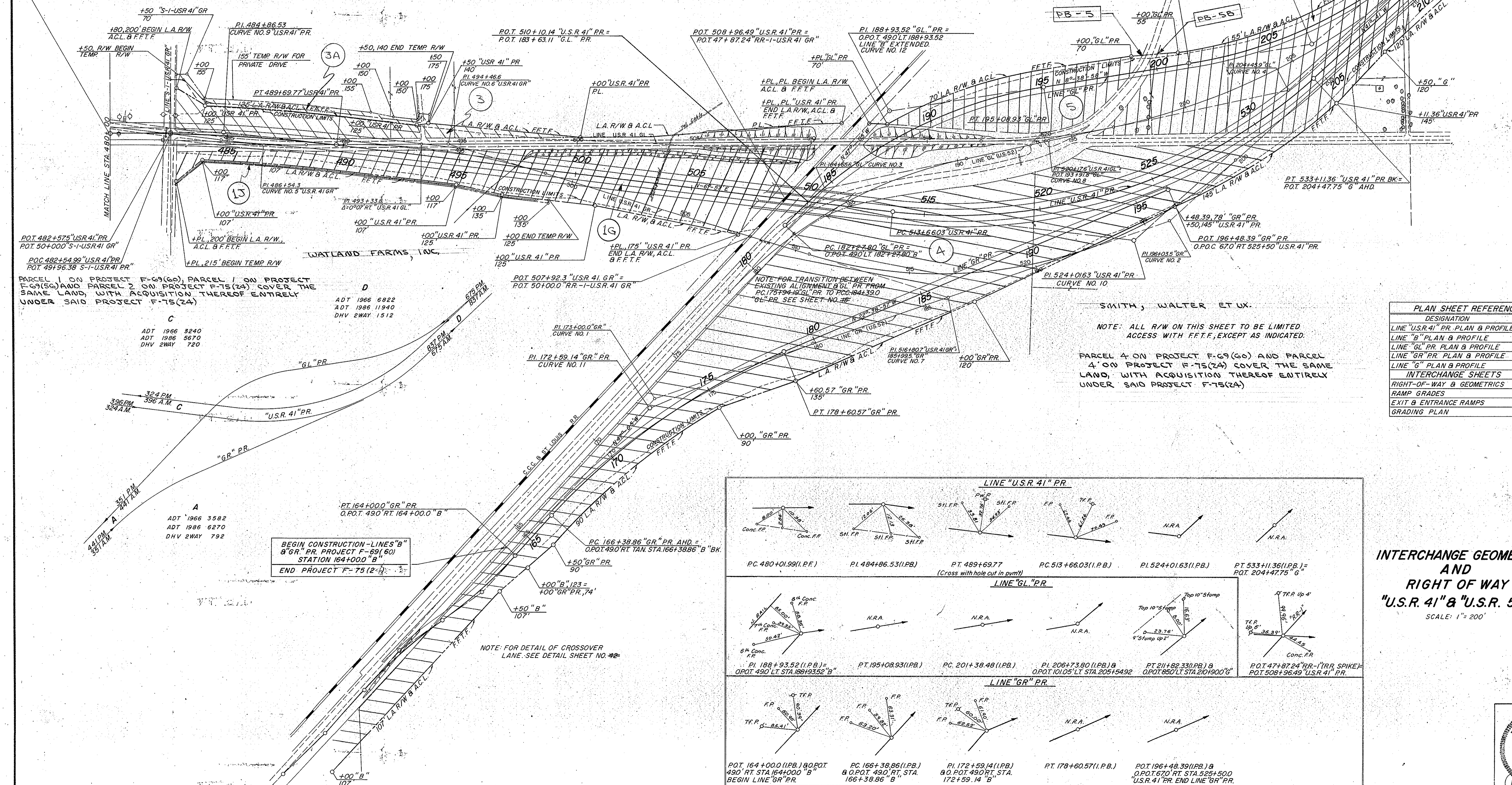
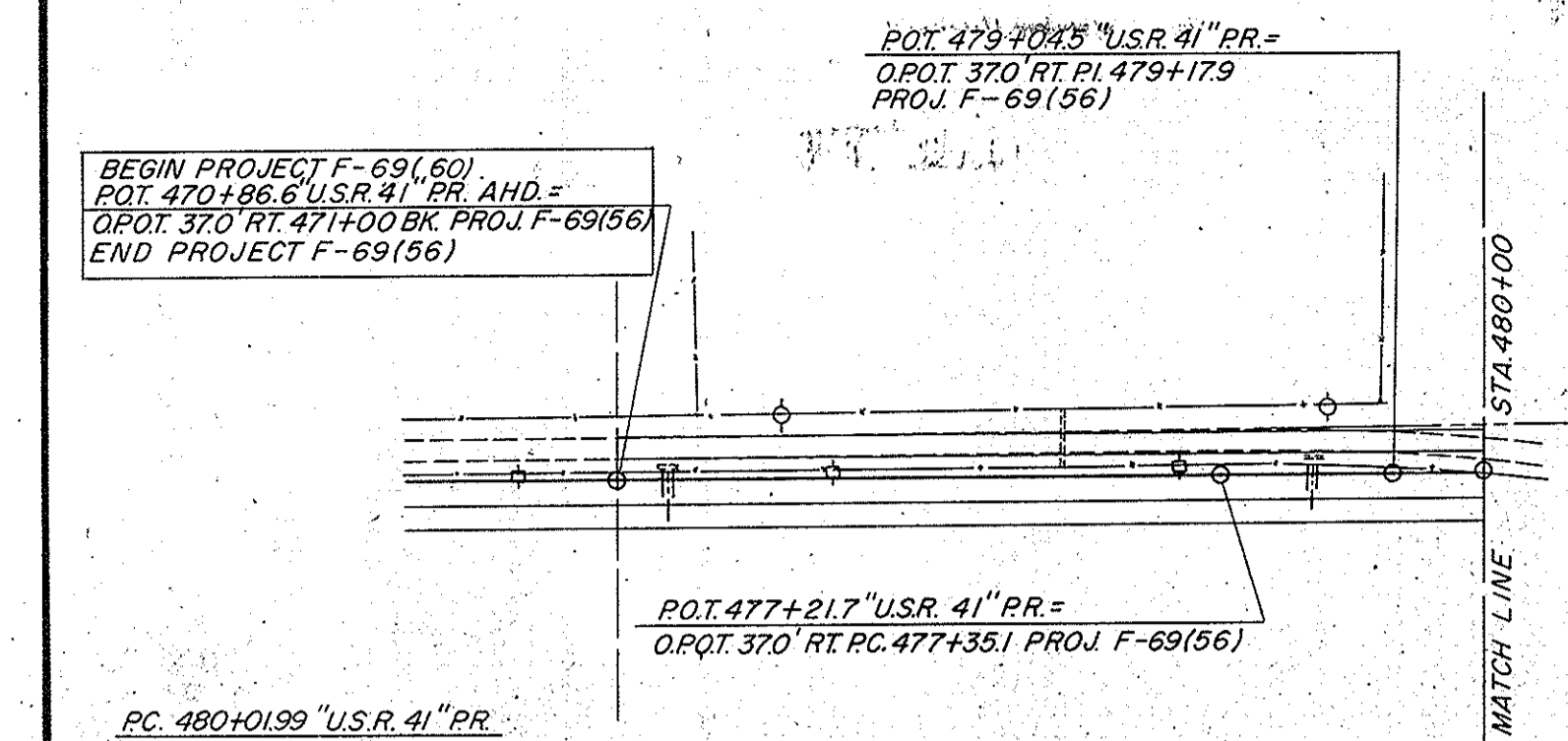
SURVEY CURVE DATA

LINE	"G R"		"G L"	
CURVE NO.	(1)	(2)	(3)	(4)
D	28°-06' RT	28°-48' LT	35°-24' RT	30°-00' LT
D	28°-00'	32°-00'	32°-00'	28°-00'
R	2864.79'	1909.86'	1909.86'	2864.79'
T	777.0'	333.4'	609.6'	267.7'
E	1505.00'	333.4'	1180.0'	1500.0'
L	183.61'	39.10'	94.0'	101.05'
PC STA.	165+83.0	192+20.1	178+56.0	195+78.2
PI STA.	173+00.0	222+80.1 USR HIGH	184+65.6	223+00.0 USR HIGH
PT STA.	179+88.0	199+76.8	190+36.0	211+78.2
		199+76.8 "G"		211+78.2 "G"

LINE	"U.S.R. 41 GR"		"U.S.R. 41 GL"	
CURVE NO.	(5)	(6)	(7)	(8)
D	72°-20' LT	172°-24' RT	36°-00' LT	110°-54' RT
D	28°-00'	28°-00'	30°-00'	28°-00'
R	2864.79'	2864.79'	1909.86'	2864.79'
T	356.1'	356.1'	320.4'	356.1'
L	356.1'	356.1'	320.4'	356.1'
PC STA.	484+70.7	490+08.2	910+60.1	517+31.2
PI STA.	486+54.3	494+46.6	416+80.7	520+17.6
PT STA.	488+37.4	498+78.2	523+02.0	523+02.0
		192+20.1 "GR"		196+78.2 "GL"

PR CURVE DATA

CURVE NO. 9	CURVE NO. 10	CURVE NO. 11
PI 484+86.53 "USR 41" PR A=745.50' T=484.54 R=7639.44' L=987.78 D=0°-45' E=15.35'	PI 524+01.63 "USR 41" PR A=494.39' T=1035.60 R=2291.83' L=1948.33 D=2°-30' E=223.12'	PI 172+59.14 "GR" PR A=2428.03' T=620.28 R=2864.79' L=1221.71 D=2°-00' E=66.38'
CURVE NO. 12	CURVE NO. 13	
PI 188+93.52 "GL" PR A=3826.02' T=665.72 R=1909.86' L=1281.13 D=3°-00' E=112.70'	PI 206+73.80 "GL" PR A=318°-56' T=535.32 R=1909.86' L=1043.85 D=3°-00' E=73.60'	



BEGIN PROJECT F-69(60)
POT 470+86.6 "USR 41" PR AHD=
O.POT 370 RT 474.00 BK PROJ F-69(56)
END PROJECT F-69(56)

PARCEL 1 ON PROJECT F-69(60) PARCEL 2 ON PROJECT F-69(60) AND PARCEL 3 ON PROJECT F-75(24) COVER THE SAME LAND, WITH ACQUISITION THEREOF ENTIRELY UNDER SAID PROJECT F-75(24)

ADT 1966 6822
ADT 1966 11940
DHV 2WAY 1512

ADT 1966 3240
ADT 1966 5670
DHV 2WAY 720

ADT 1966 3582
ADT 1966 6270
DHV 2WAY 792

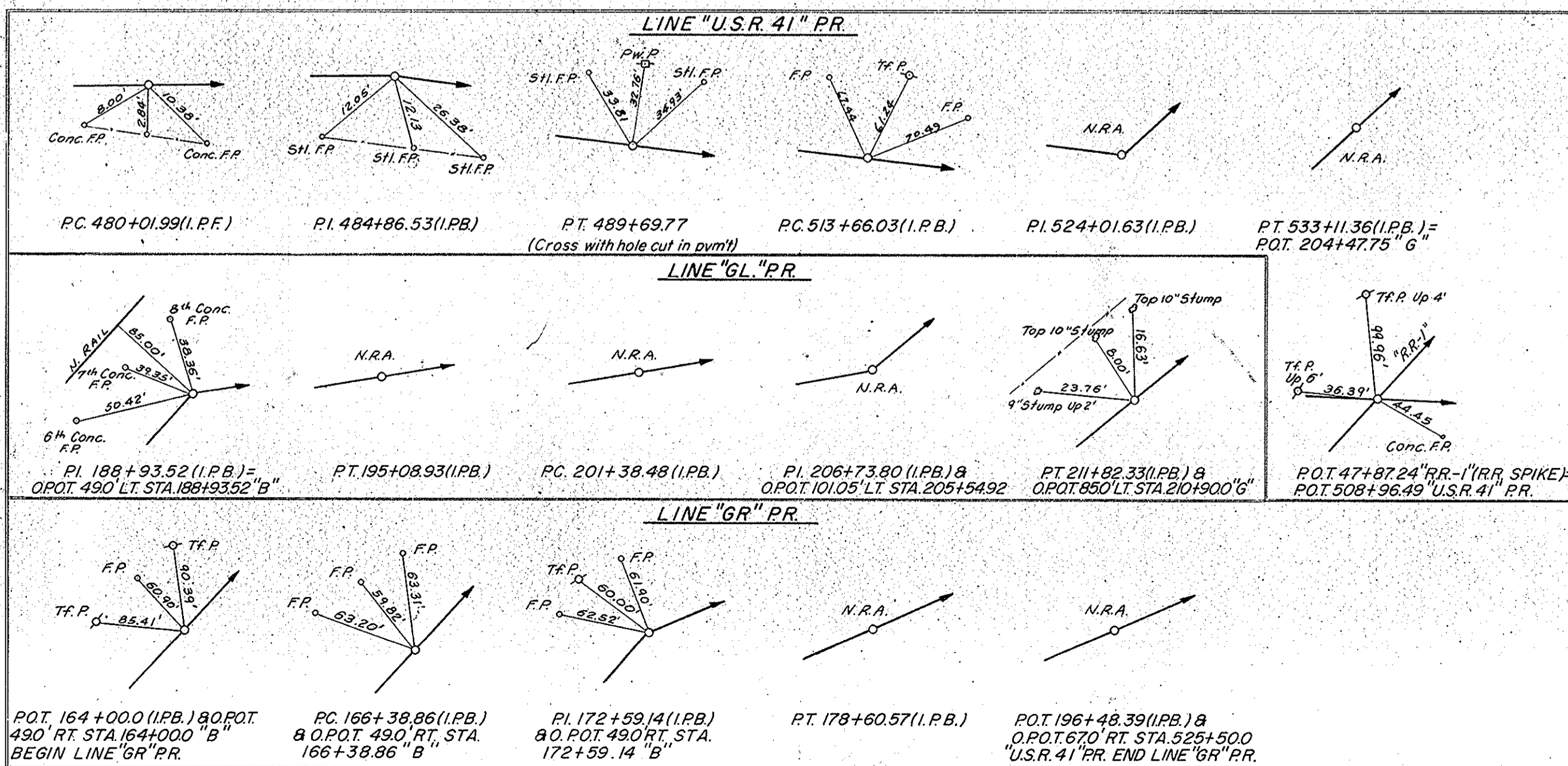
BEGIN CONSTRUCTION-LINES "B"
& "G" PR PROJECT F-69(60)
STATION 164+00.0 "B"
END PROJECT F-75(24)

PLAN SHEET REFERENCES

DESIGNATION	SHEET NO.
LINE "U.S.R. 41" PR PLAN & PROFILE	2-10
LINE "B" PLAN & PROFILE	7-1
LINE "GL" PR PLAN & PROFILE	10-8-13
LINE "GR" PR PLAN & PROFILE	11-8-12
LINE "G" PLAN & PROFILE	10-
INTERCHANGE SHEETS	
RIGHT-OF-WAY & GEOMETRICS	3-0
RAMP GRADES	3-2-33
EXIT & ENTRANCE RAMPS	3-1
GRADING PLAN	*

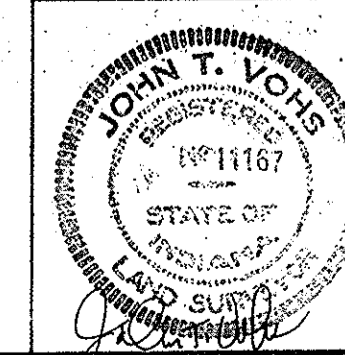
NOTE: ALL R/W ON THIS SHEET TO BE LIMITED ACCESS WITH F.F.T.F., EXCEPT AS INDICATED.

PARCEL 4 ON PROJECT F-69(60) AND PARCEL 4' ON PROJECT F-75(24) COVER THE SAME LAND, WITH ACQUISITION THEREOF ENTIRELY UNDER SAID PROJECT F-75(24)



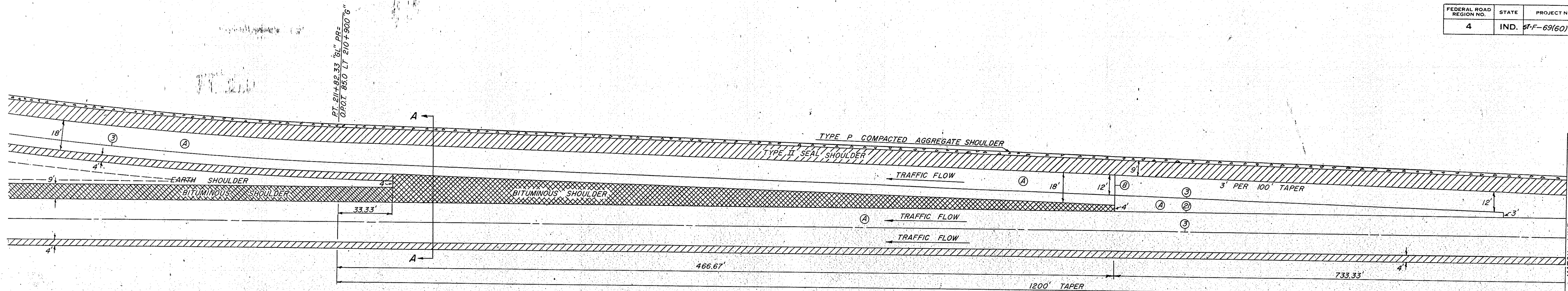
**INTERCHANGE GEOMETRICS
AND
RIGHT OF WAY
"U.S.R. 41" & "U.S.R. 52"**

SCALE: 1" = 200'

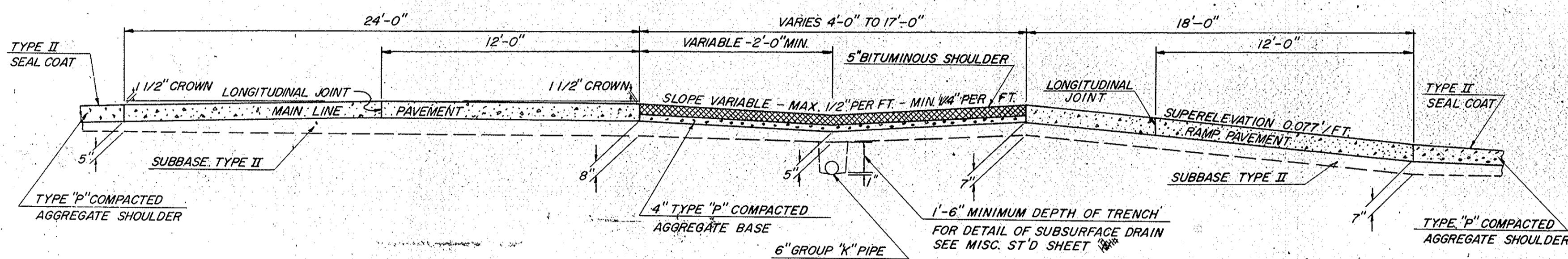


November 6, 1961

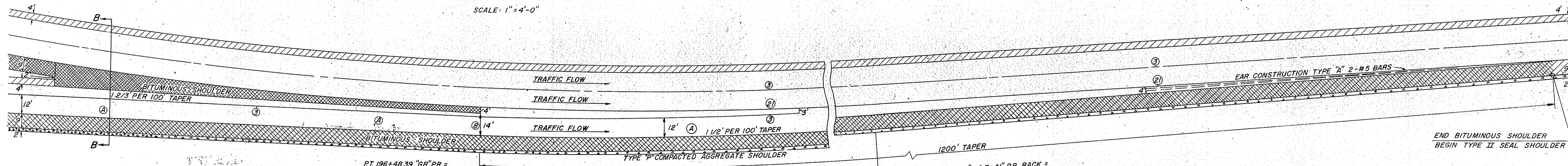
PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
F-69(60)		3		



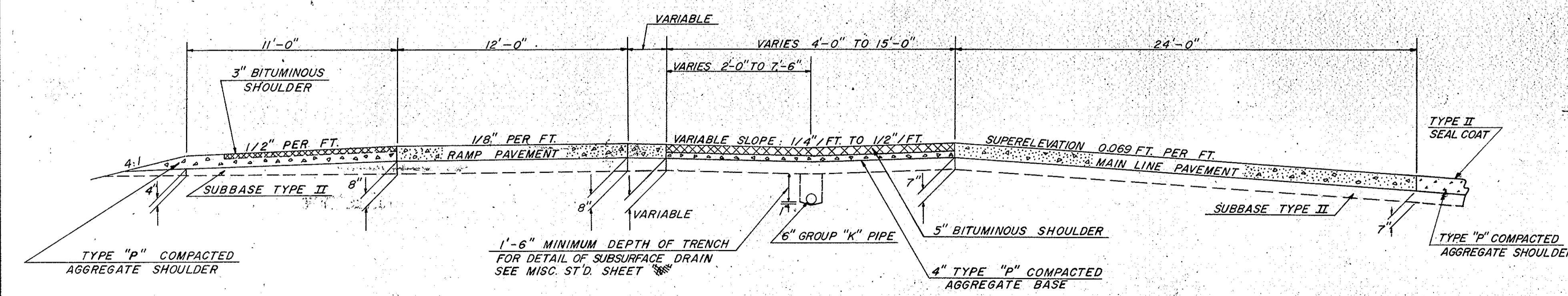
RAMP EXIT LINE "6L" PR
SCALE 1" = 30'



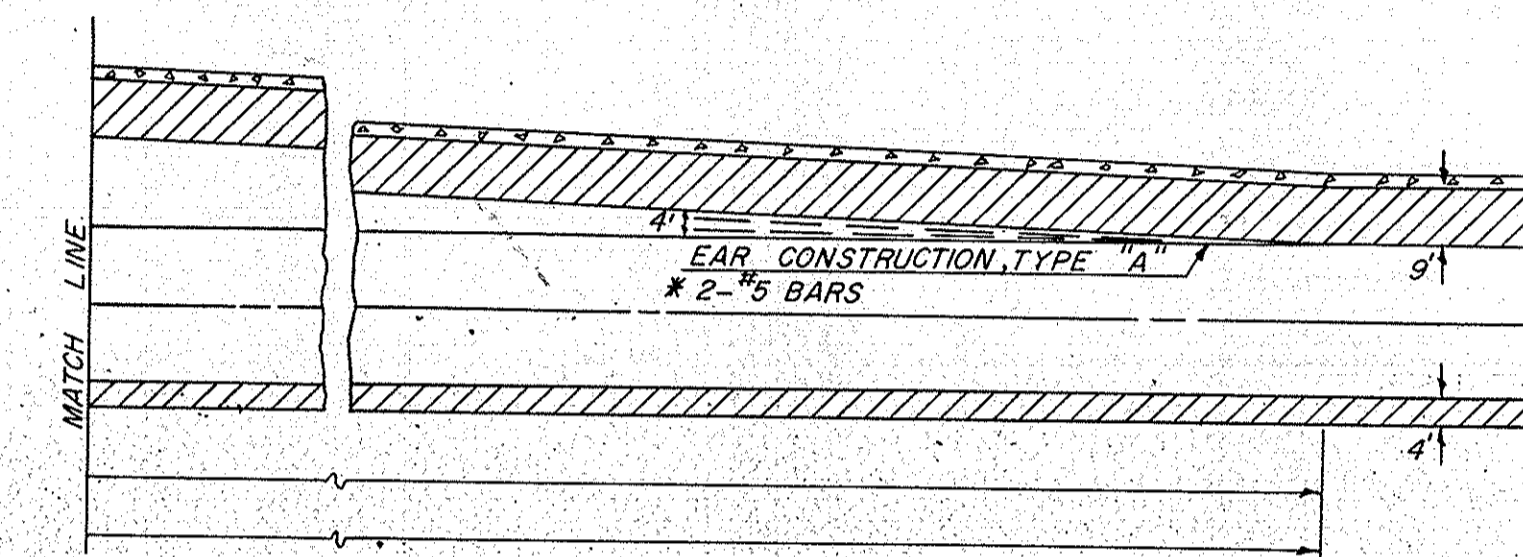
SECTION "A-A"
SCALE 1" = 4'-0"



RAMP ENTRANCE LINE "6R" PR
SCALE 1" = 30'

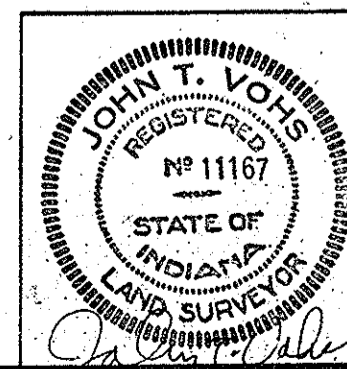


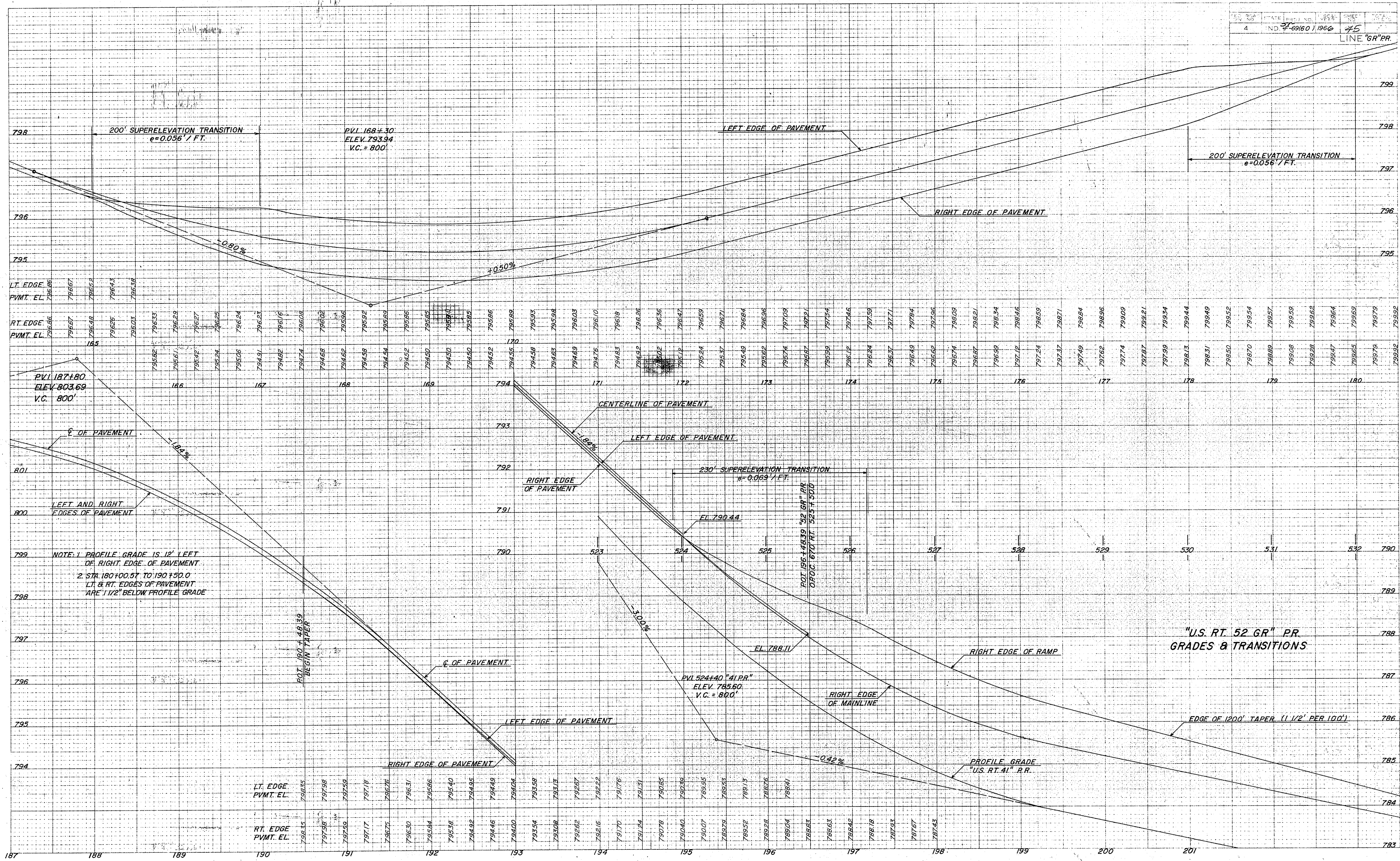
SECTION "B-B"
SCALE 1" = 4'-0"



- LEGEND**
- ④ REINFORCED CONCRETE PAVEMENT
 - ③ LONGITUDINAL JOINT
 - ② 1" PREFORMED EXPANSION JOINT WITH LOAD TRANSFER
 - ① KEYWAY CONSTRUCTION JOINT
 - ▨ TYPE II SEAL SHOULDER
 - ▩ BITUMINOUS SHOULDER

DETAILS



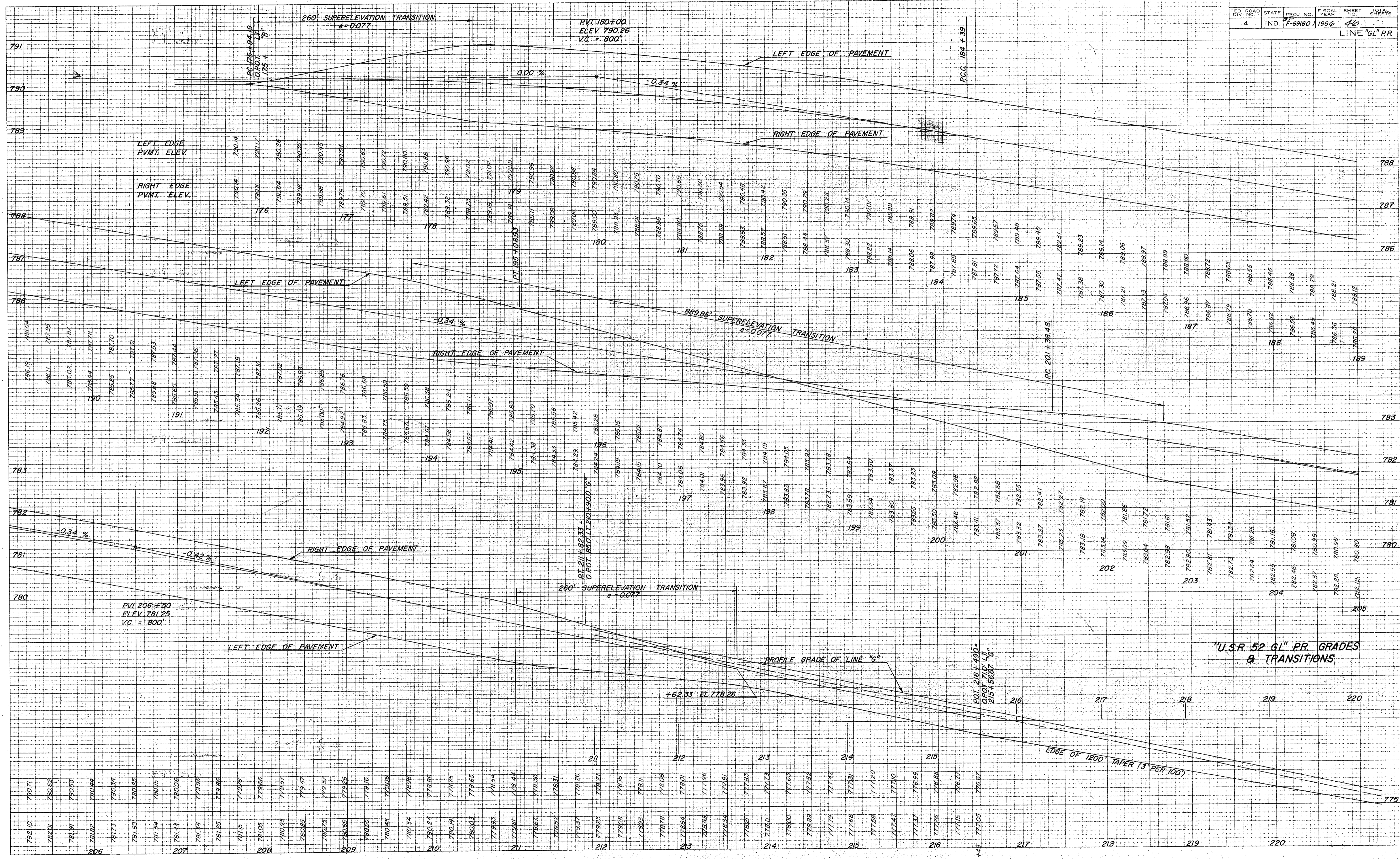


CUT

FILL

FED. ROAD DIV. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	F-69(60)	1966	40	40

LINE "GL" PR.

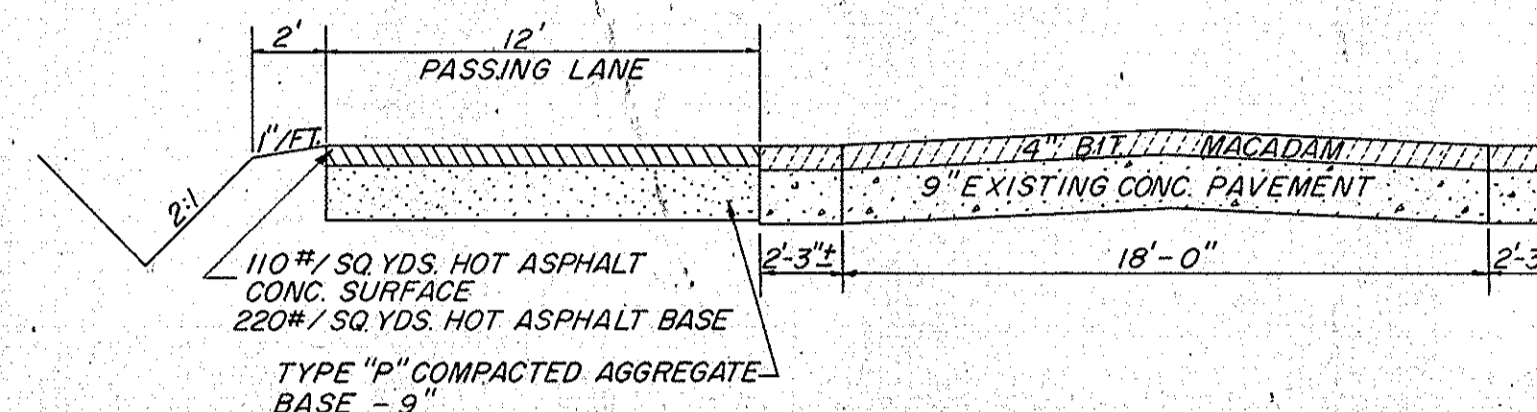


"U.S.R. 52 GL" PR. GRADES & TRANSITIONS

FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	F-69(60)	1966	47	

GENERAL NOTES

1. TYPICAL CROSS SECTIONS FOR DETOUR ROAD NO. 1 AND TEMPORARY CROSSOVER SHALL BE AS SHOWN ON SHEET NO.
2. STATIONING ALONG EXISTING U.S.R. 41 IS AS SHOWN ON THE PLANS FOR PROJECT F.A. 69 B.
3. CONSTRUCTION SEQUENCE FOR MAINTENANCE OF TRAFFIC ON U.S. ROUTE 52.
 - (a) CONSTRUCT EMBANKMENT AND 9" RC PAVEMENT FOR LINE "U.S.R. 41" PR. BETWEEN STATIONS 520+50 AND 533+00 AND LINE "GR" PR. BETWEEN STATIONS 184+00 AND 196+48.39 INCLUDING TAPER ALONG LINE "U.S.R. 41" PR. BETWEEN STATIONS 525+50 AND 533+00.
 - (b) CONSTRUCT CROSSOVER LANE ON LINE "B" BETWEEN STATIONS 190+61.0 AND 164+00. (SEE SHEET NO. 35 FOR DETAILS)
 - (c) CONSTRUCT DETOUR NO. 1, PASSING LANE LT. OF EXISTING PAVEMENT ON U.S.R. 41 BETWEEN STATIONS 526+50 AND 533+40 AND TEMPORARY CROSSOVERS AT STATIONS 523+00 AND 530+50 LINE "U.S.R. 41" PR.
 - (d) CLOSE EXISTING U.S. ROUTE 52 TO TRAFFIC THROUGH THE LIMITS OF THE INTERCHANGE AND MAINTAIN U.S. ROUTE 52 TRAFFIC ON DETOUR ROAD NO. 1 AND THE NEW PAVEMENT ON LINE "U.S.R. 41" PR. AND LINE "GR" PR.



SECTION A-A
SCALE: 3/16" = 1' HOR.
3/8" = 1' VERT.

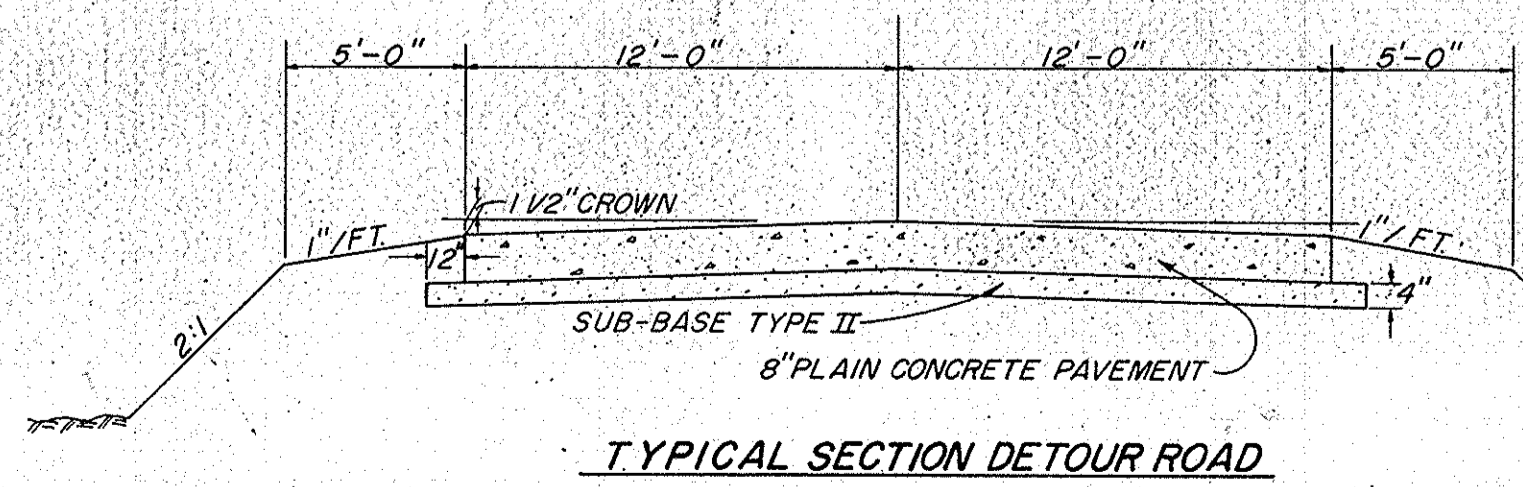
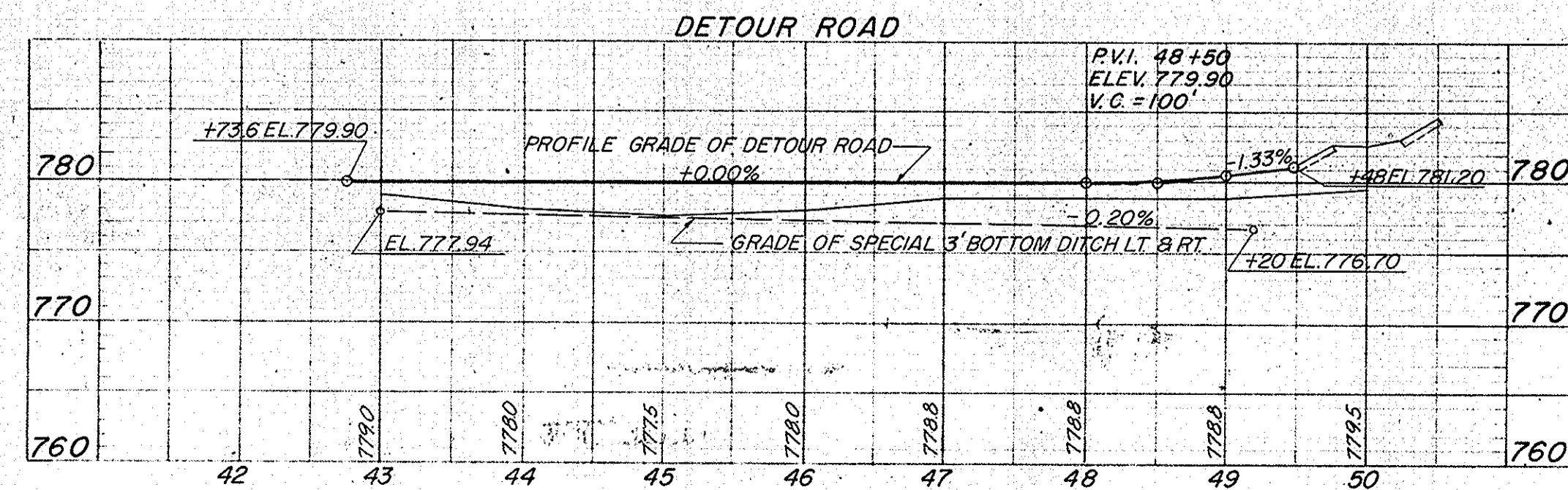
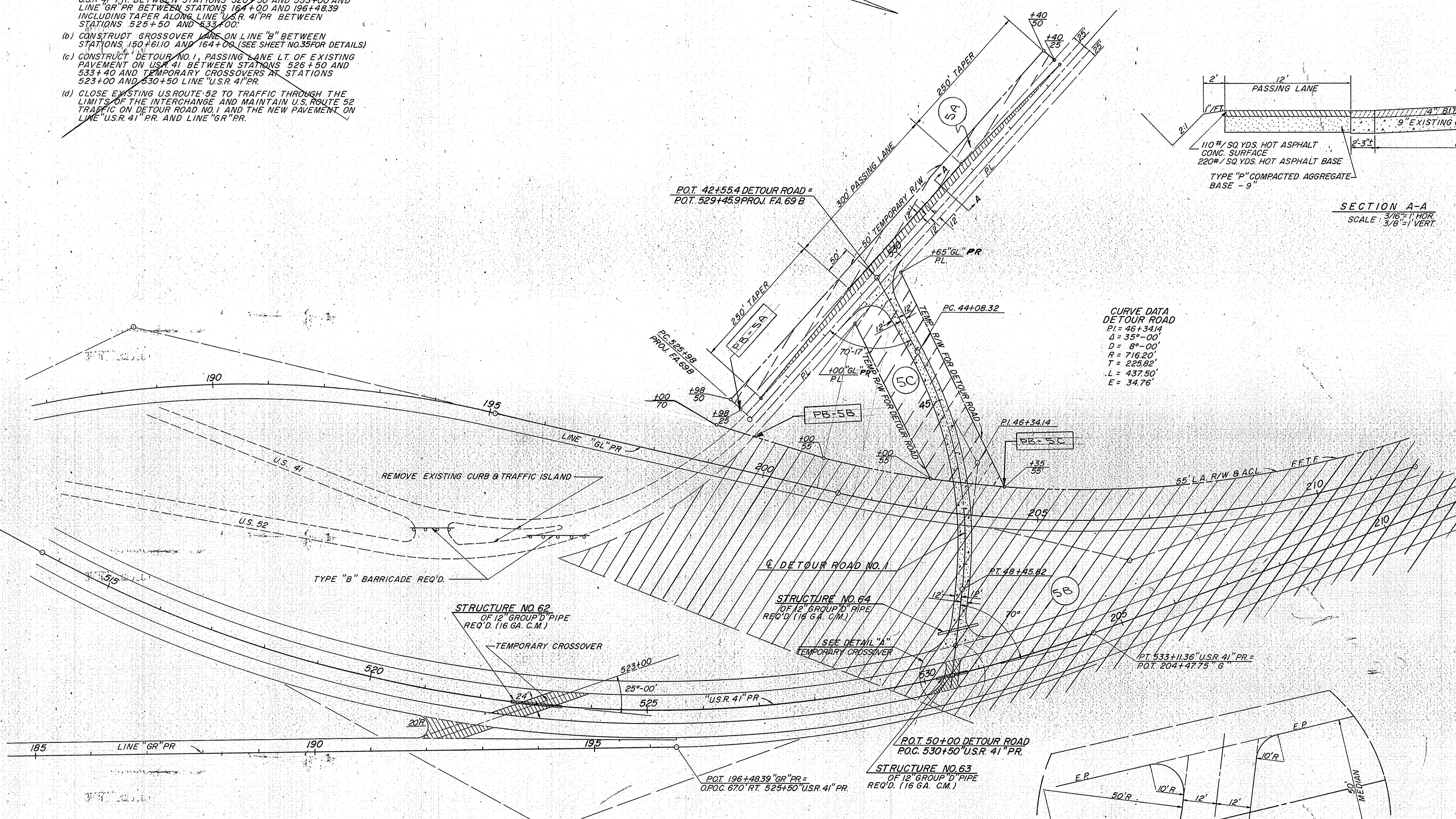
**CURVE DATA
DETOUR ROAD**
 PI = 46+34.14
 L = 35°-00'
 D = 8°-00'
 R = 716.20'
 T = 225.82'
 L = 437.50'
 E = 34.76'

LEGEND

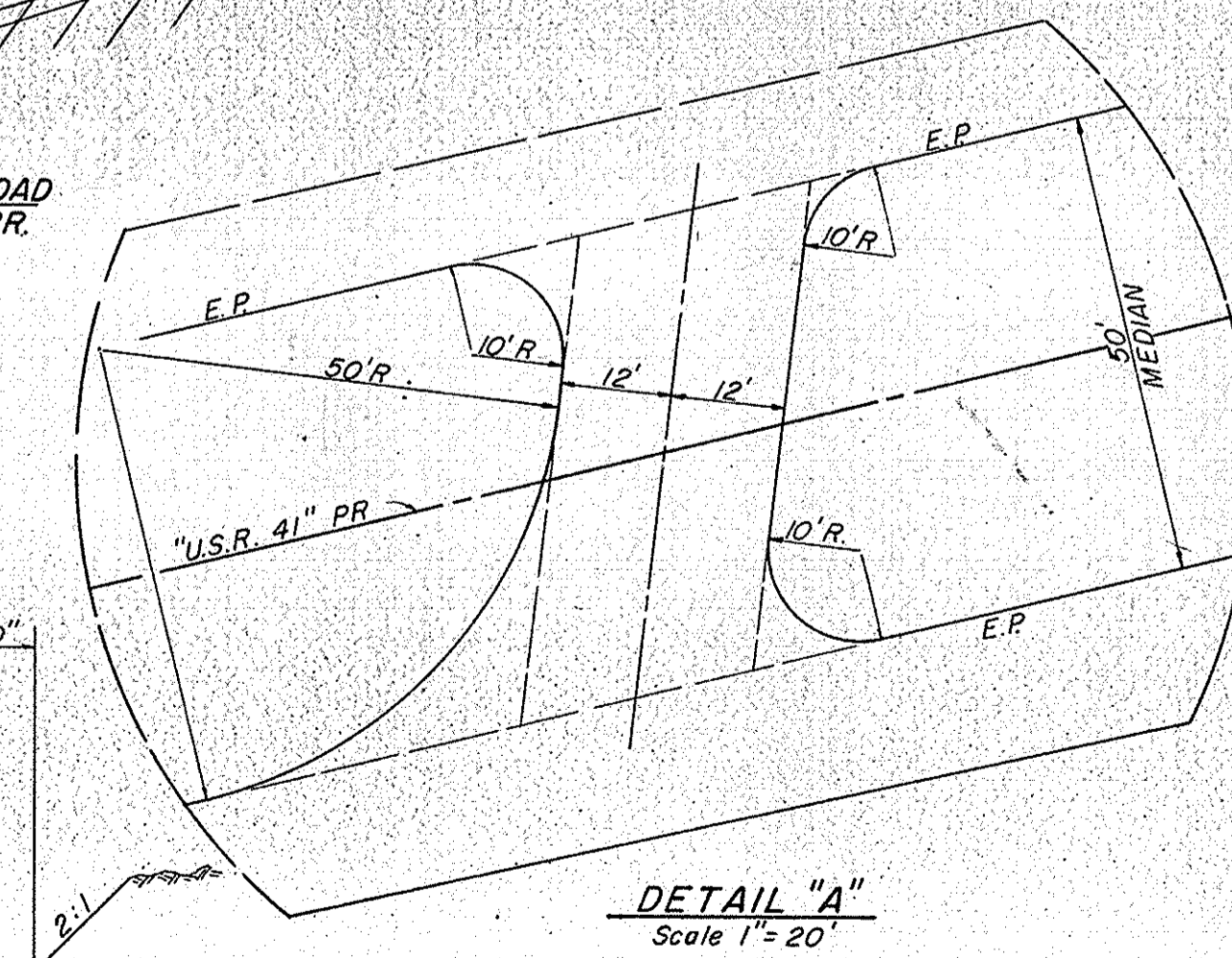
	8" PLAIN CONCRETE
	BITUMINOUS MATERIALS FOR APPROACHES

PLAN SHEET REFERENCES

DESCRIPTION	SHEET NO.
LINE U.S.R. 41 PR	2-210
LINE "GL" PR	2-2
TYPICAL CROSS SECTION DETOUR ROAD NO. 1	2-3
TYPICAL CROSS SECTION TEMPORARY CROSSOVER	2-3



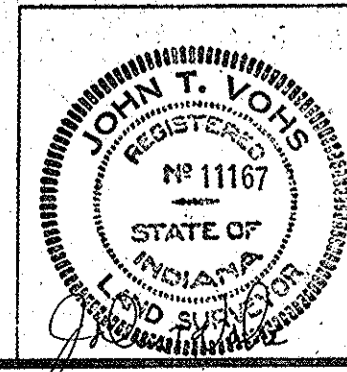
SCALE: 3/16" = 1' HOR.
3/8" = 1' VERT.



Scale 1" = 20'

**DETOUR ROAD NO. 1
DETAILS**

SCALE 1" = 100'
(Except As Noted)

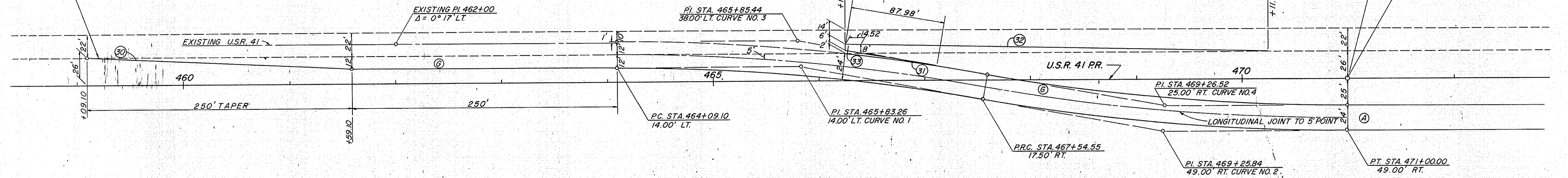


PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
F-69(60)		47		

FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	F-69(60)	1964	48	

CURVE NO. 1A2 $\Delta = 10^{\circ}25'14''$ $D = 3^{\circ}00'$ $T = 174.16'$ $L = 347.35'$ $E = 7.92'$	CURVE NO. 3 $\Delta = 10^{\circ}25'14''$ $D = 2^{\circ}57'46''$ $T = 176.34'$ $L = 351.72'$ $E = 16.00'$	CURVE NO. 4 $\Delta = 10^{\circ}25'14''$ $D = 3^{\circ}02'17''$ $T = 171.97'$ $L = 342.99'$ $E = 7.82'$
---	--	---

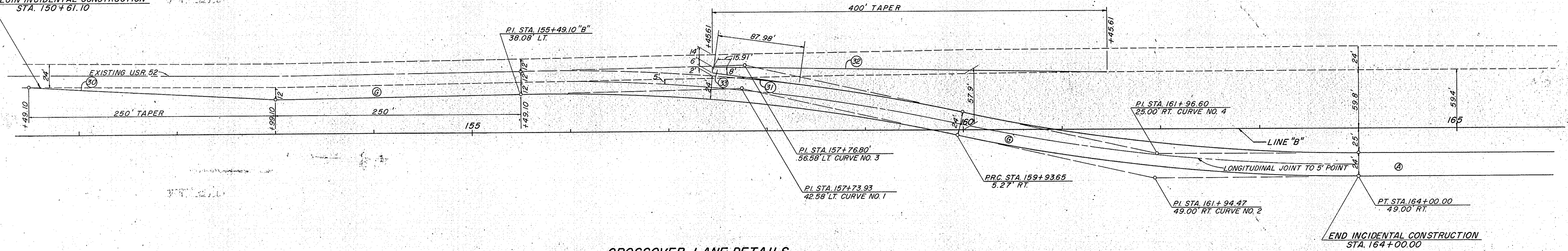
BEGIN INCIDENTAL CONSTRUCTION
STA. 459+21.10



CROSSOVER LANE DETAILS
LINE U.S.R. 41 PR
SCALE 1" = 50'

CURVE NO. 1 $\Delta = 13^{\circ}25'50''$ $D = 3^{\circ}00'$ $T = 224.87'$ $L = 447.69'$ $E = 13.19'$	CURVE NO. 2 $\Delta = 12^{\circ}17'05''$ $D = 3^{\circ}00'$ $T = 205.53'$ $L = 409.49'$ $E = 11.03'$	CURVE NO. 3 $\Delta = 13^{\circ}25'50''$ $D = 2^{\circ}57'46''$ $T = 227.70'$ $L = 453.31'$ $E = 13.36'$	CURVE NO. 4 $\Delta = 12^{\circ}17'05''$ $D = 3^{\circ}02'17''$ $T = 202.95'$ $L = 404.36'$ $E = 10.89'$
--	--	--	--

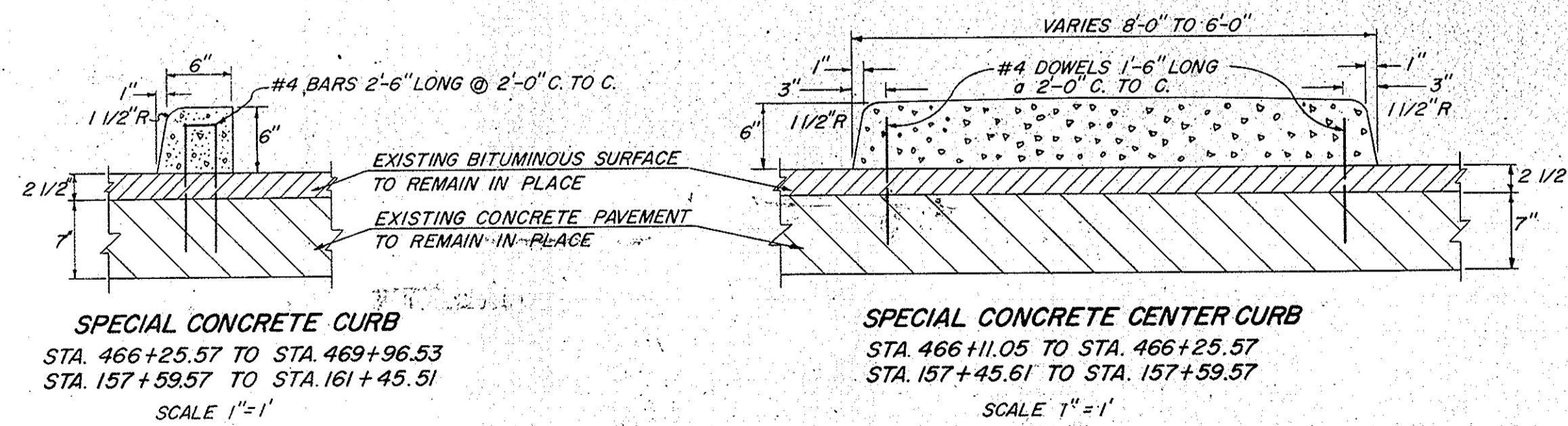
BEGIN INCIDENTAL CONSTRUCTION
STA. 150+61.10



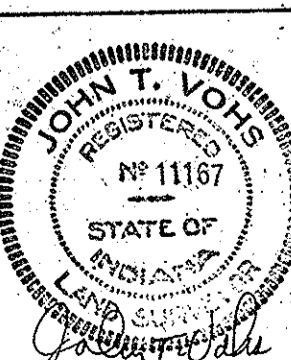
CROSSOVER LANE DETAILS
LINE "B"
SCALE 1" = 50'

LEGEND

- (A) REINFORCED CONCRETE PAVEMENT
- (B) PLAIN CONCRETE PAVEMENT
- (C) KEYWAY CONSTRUCTION JOINT
- (D) EAR CONSTRUCTION TYPE "A"
- (E) INTEGRAL CONCRETE CURB
- (F) SPECIAL CONCRETE CURB
- (G) SPECIAL CONCRETE CENTER CURB



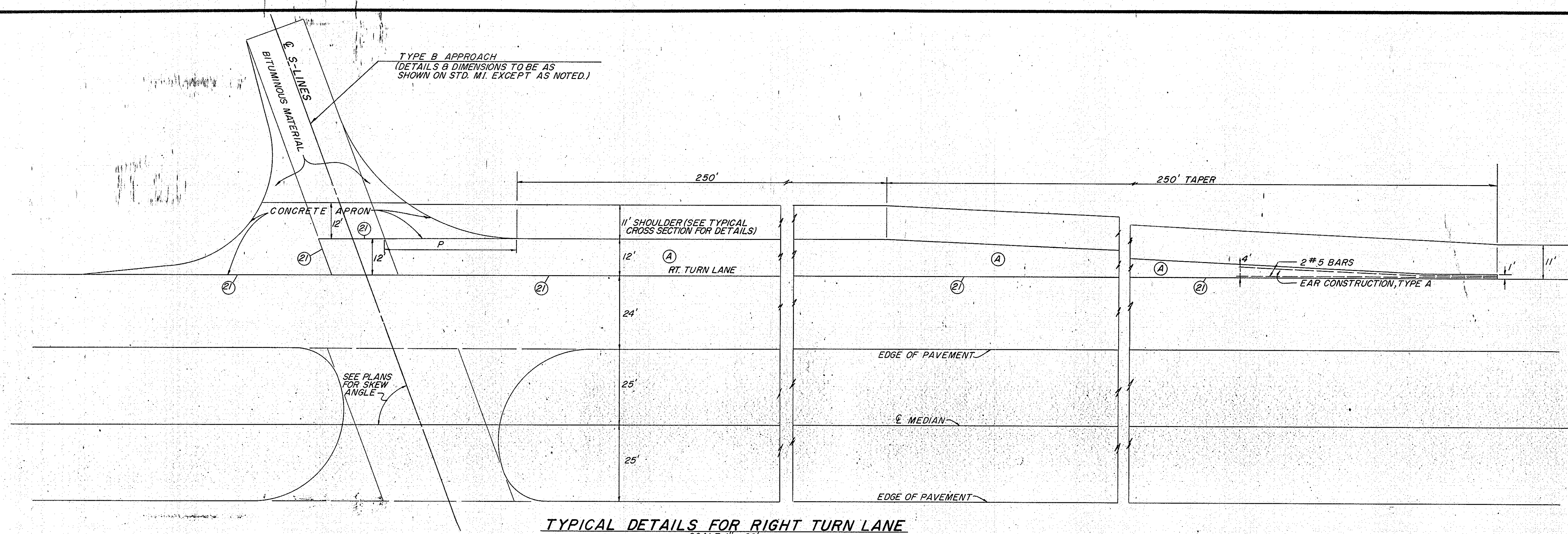
DETAILS



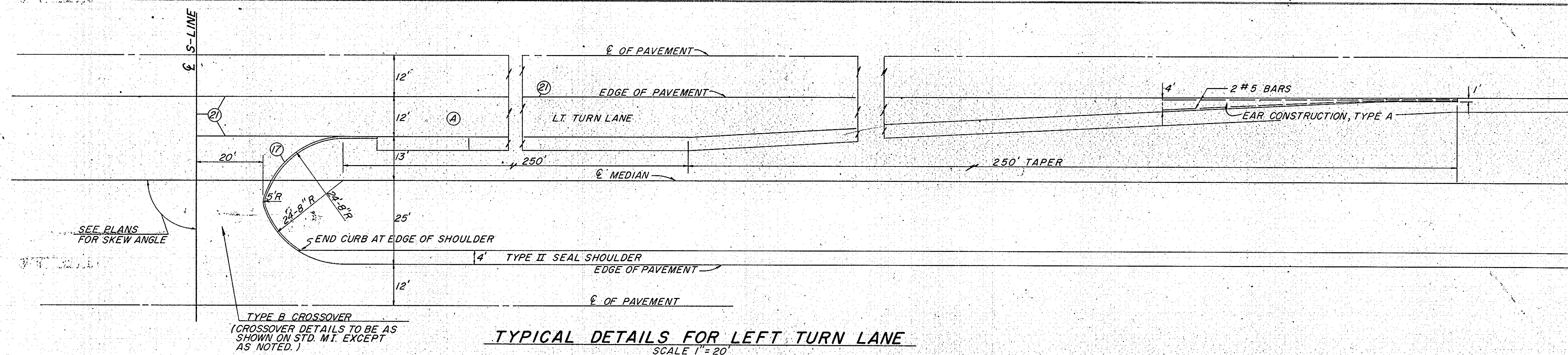
November 6, 1961

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
F-69(60)		48		

FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	ST-F-69(60)	1964	49	



TYPICAL DETAILS FOR RIGHT TURN LANE
SCALE 1" = 20'



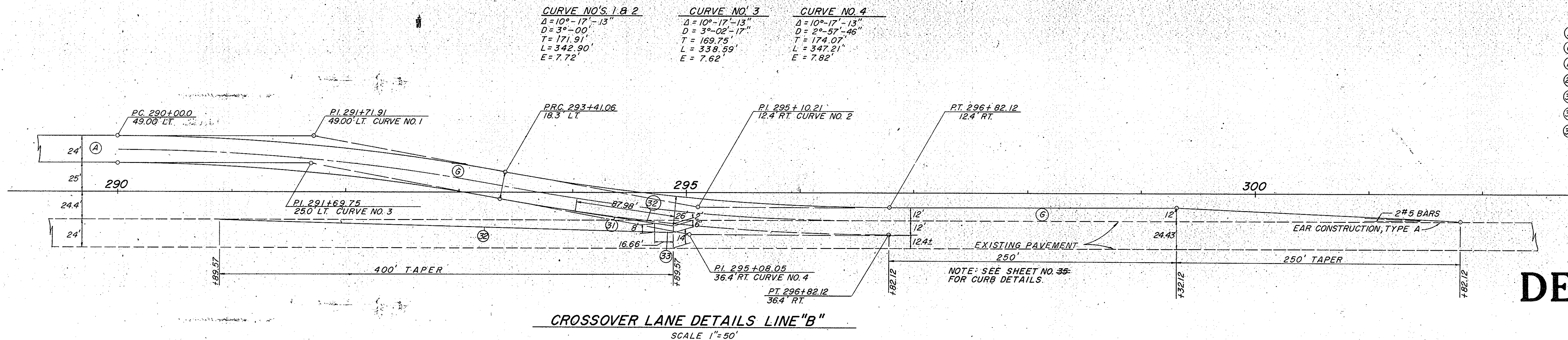
TYPICAL DETAILS FOR LEFT TURN LANE
SCALE 1" = 20'

TURN LANES REQUIRED AS FOLLOWS

INTERSECTION	MAINLINE STA.	TRAFFIC DIRECTION	LANE REQ'D.
S-1-USR 41 GR.	482 + 54.99	SOUTH BOUND	RIGHT TURN
		SOUTH BOUND	LEFT TURN
		NORTH BOUND	RIGHT TURN
		NORTH BOUND	LEFT TURN
S-1-G REV.	225 + 00.0	NORTH BOUND	RIGHT TURN
S-4-G PR.		SOUTH BOUND	RIGHT TURN
S-5-G PR.	356 + 92.6	NORTH BOUND	LEFT TURN
S-6-G PR.	422 + 40.0	SOUTH BOUND	RIGHT TURN
		SOUTH BOUND	RIGHT TURN

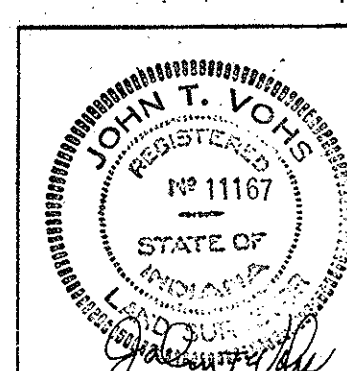
CURVE NO'S 1 & 2	CURVE NO. 3	CURVE NO. 4
$\Delta = 10^\circ - 17' - 13''$	$\Delta = 10^\circ - 17' - 13''$	$\Delta = 10^\circ - 17' - 13''$
$D = 3^\circ - 00'$	$D = 3^\circ - 02' - 17''$	$D = 2^\circ - 57' - 46''$
$T = 171.91'$	$T = 169.75'$	$T = 174.07'$
$L = 342.90'$	$L = 339.59'$	$L = 347.21'$
$E = 7.72'$	$E = 7.62'$	$E = 7.82'$

- LEGEND
- (A) REINFORCED CONCRETE PAVEMENT
 - (B) PLAIN CONCRETE PAVEMENT
 - (C) INTEGRAL CONCRETE CURB TYPE C
 - (D) KEYWAY CONSTRUCTION JOINT
 - (E) INTEGRAL CONCRETE CURB
 - (F) SPECIAL CONCRETE CURB
 - (G) SPECIAL CONCRETE CENTER CURB



CROSSOVER LANE DETAILS LINE "B"
SCALE 1" = 90'

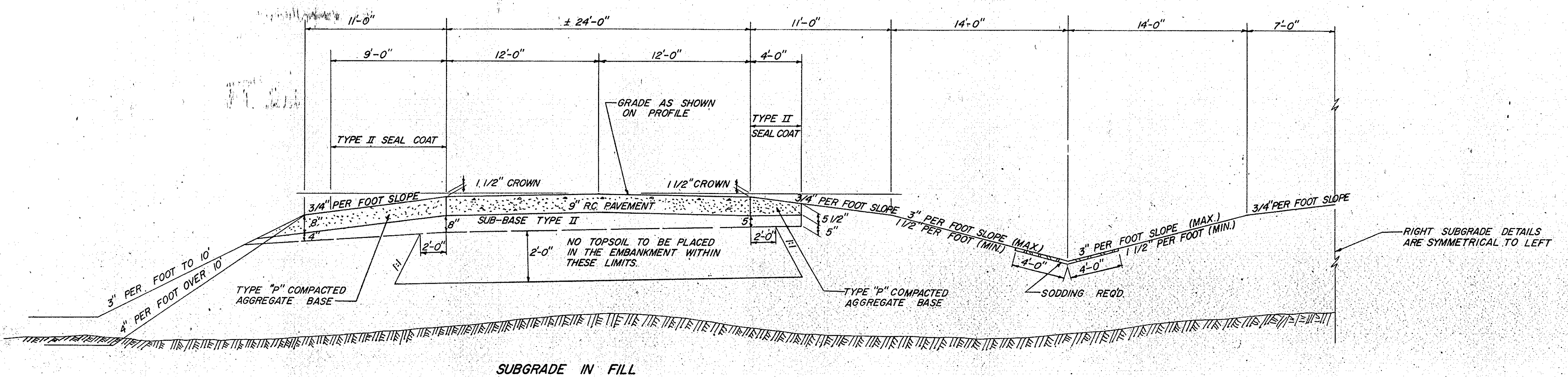
DETAILS



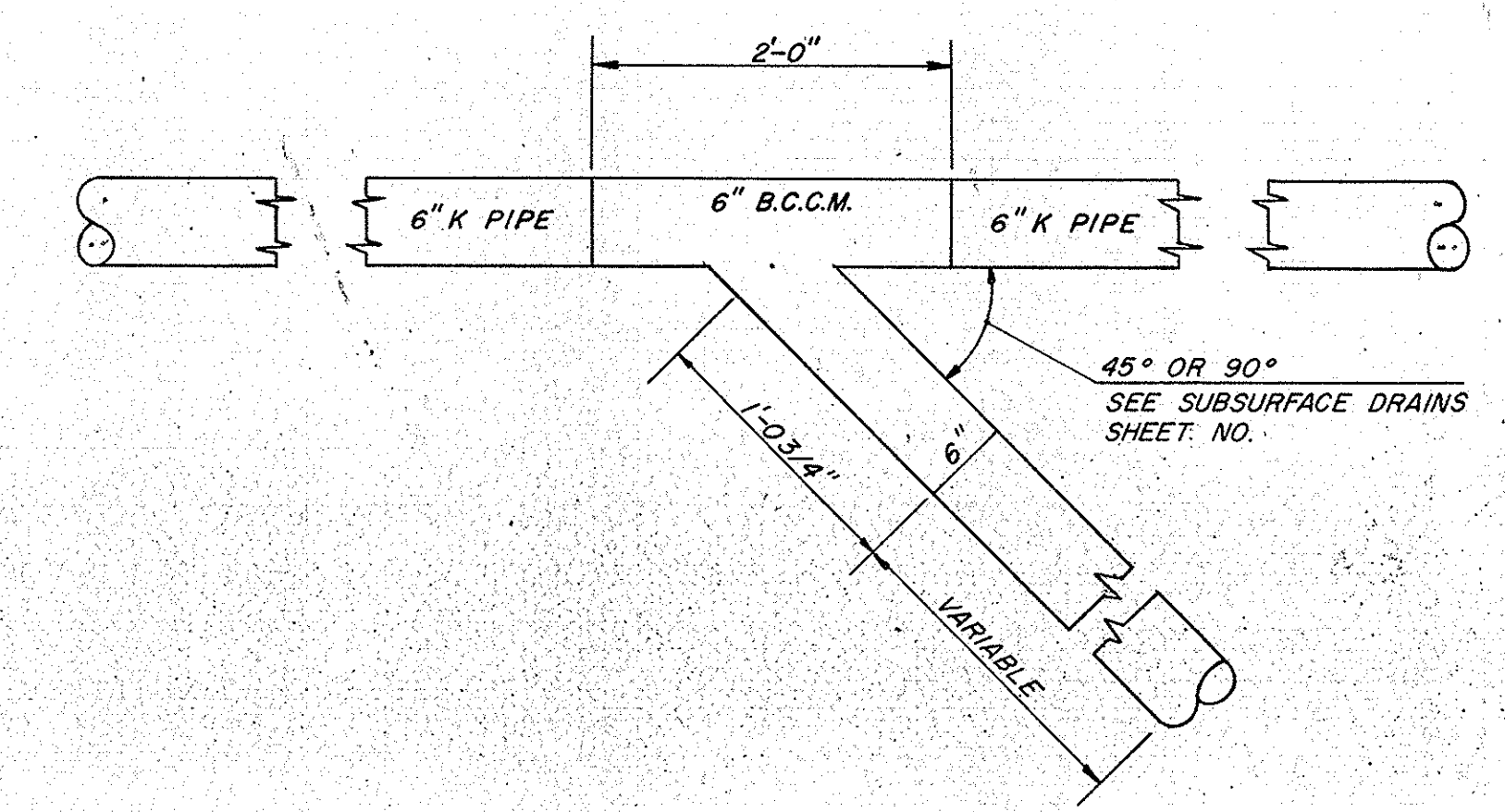
November 6, 1961

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE
F-69(60)		49		

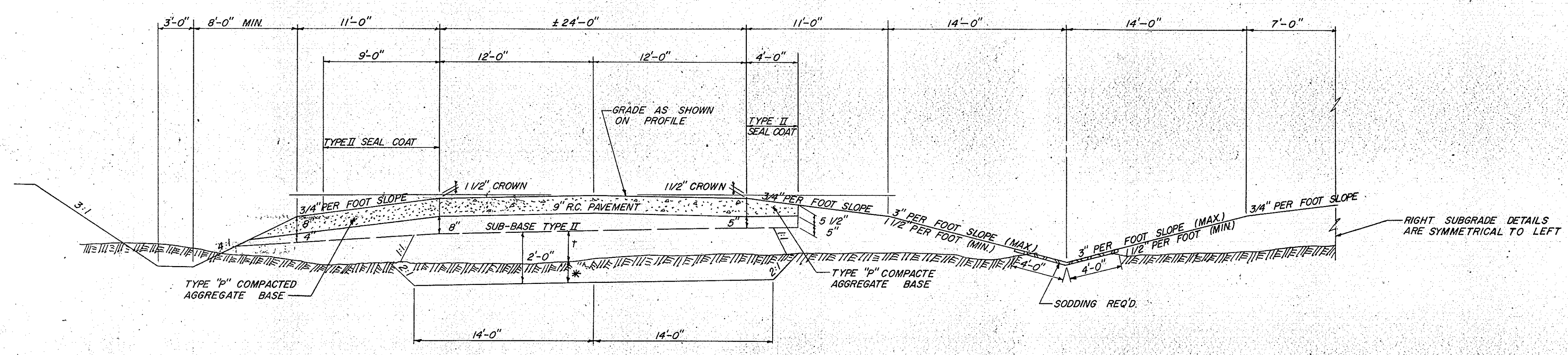
FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	F-69(60)	1966	50	



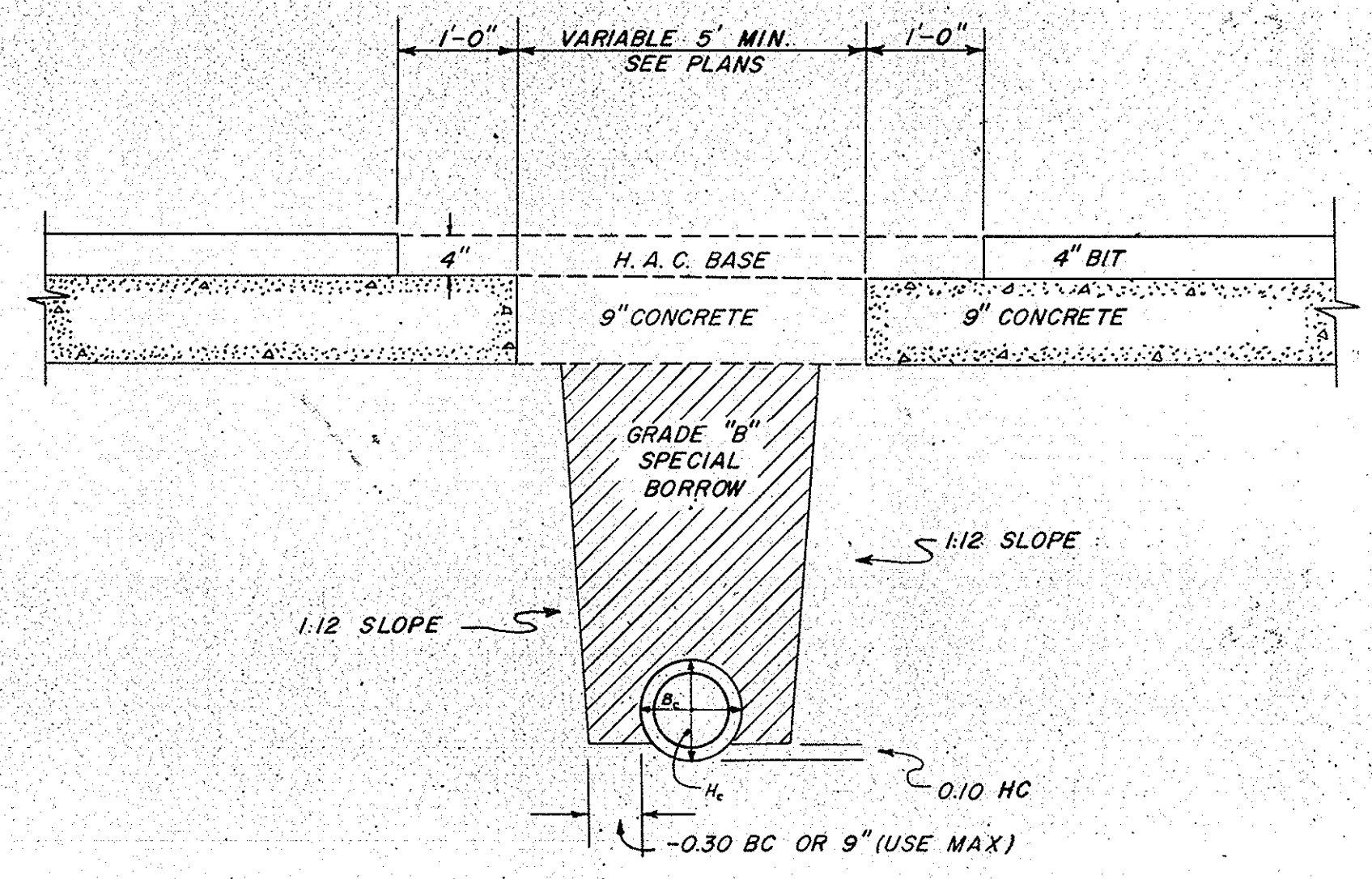
SUBGRADE IN FILL



SUBDRAIN CONNECTION



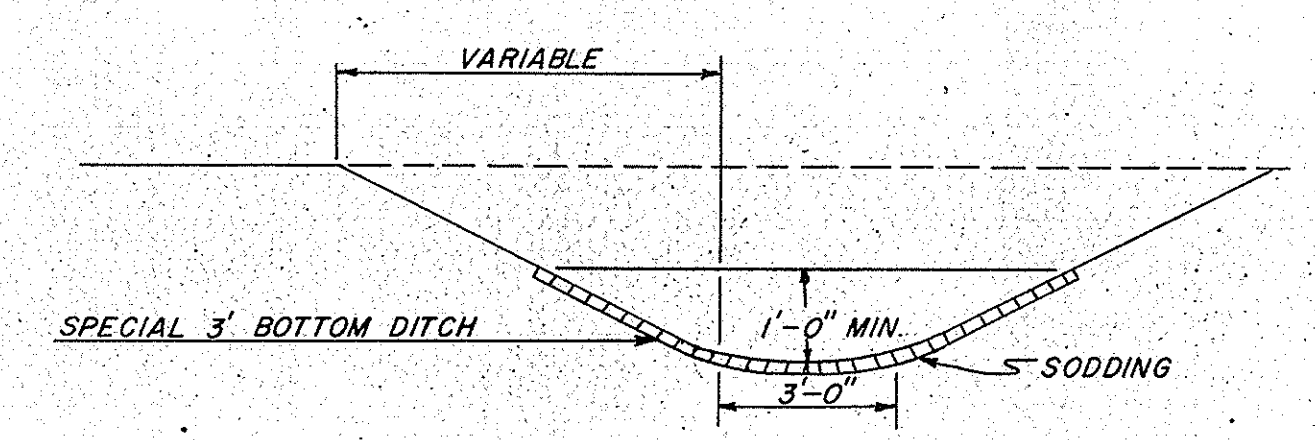
SUBGRADE IN CUT



DETAIL OF PAVEMENT REMOVAL & PATCHING FOR PLACING PIPE UNDER EXISTING PAVEMENT

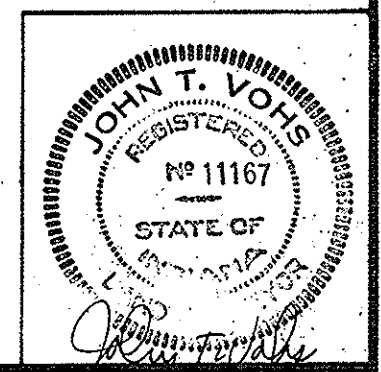
* TOPSOIL ENCOUNTERED WITHIN THESE LIMITS SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL.
 † SUITABLE MATERIAL ONLY.
 ± PAVEMENT WIDTH VARIABLE ON LINES "GR" PR AND "GL" PR

TYPICAL DETAILS FOR SUBGRADE TREATMENT
 SCALE: 3/16" HOR = 1'
 3/8" VERT = 1'



SODDED DITCH DETAIL

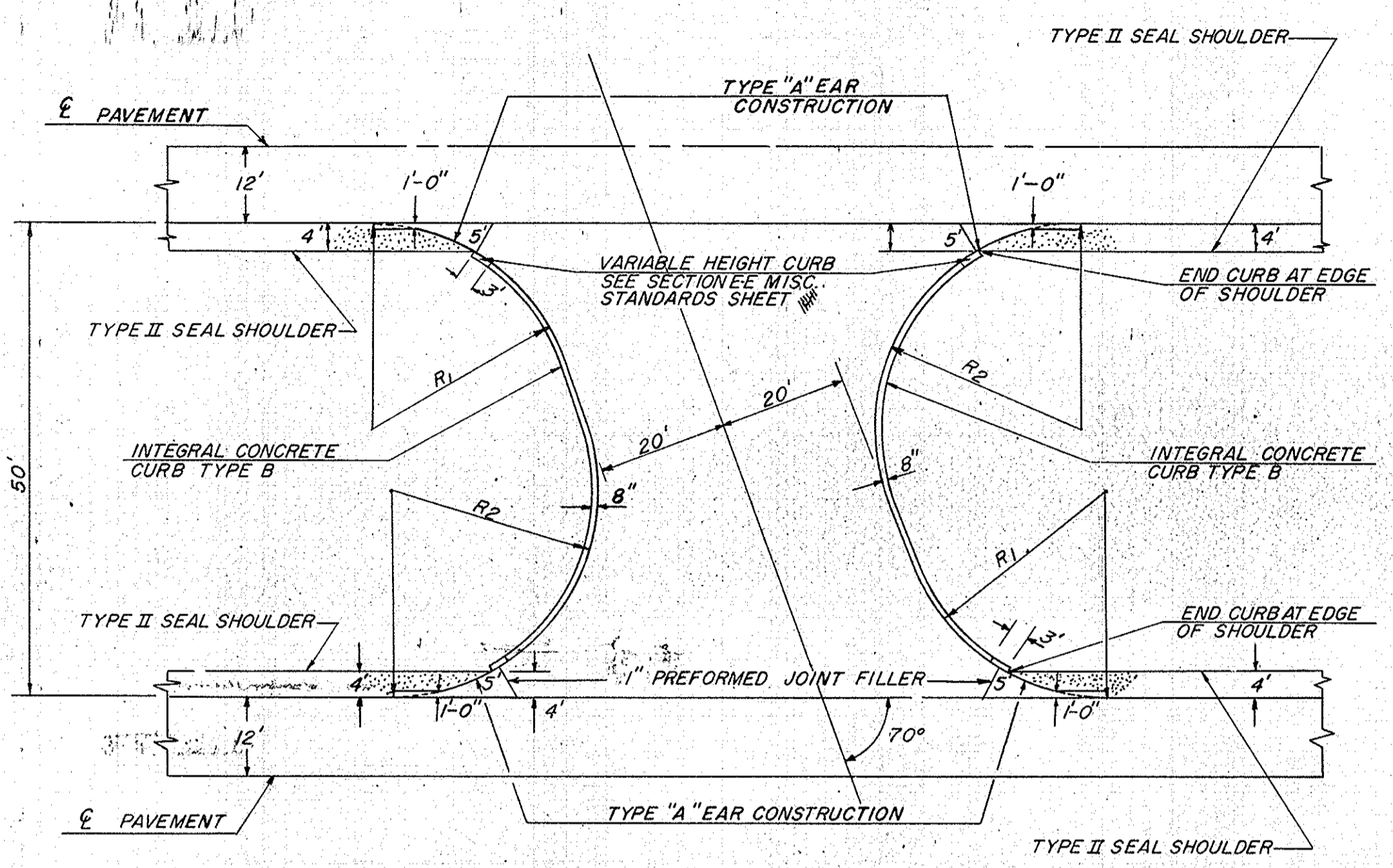
DETAILS



PROJECT NO.	LINE NO.	SHEET NO.	TOTAL SHEETS	FILE
F-69(60)	B	50		

STRUCTURE NO.
2 STR. PLATE PIPE ARCHES
MIN. AREA 31.0 SQ. FT.
SPAN 7'-3" RISE 5'-3"

FEDERAL ROAD REGION NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	STF-69 (60)	1966	51	

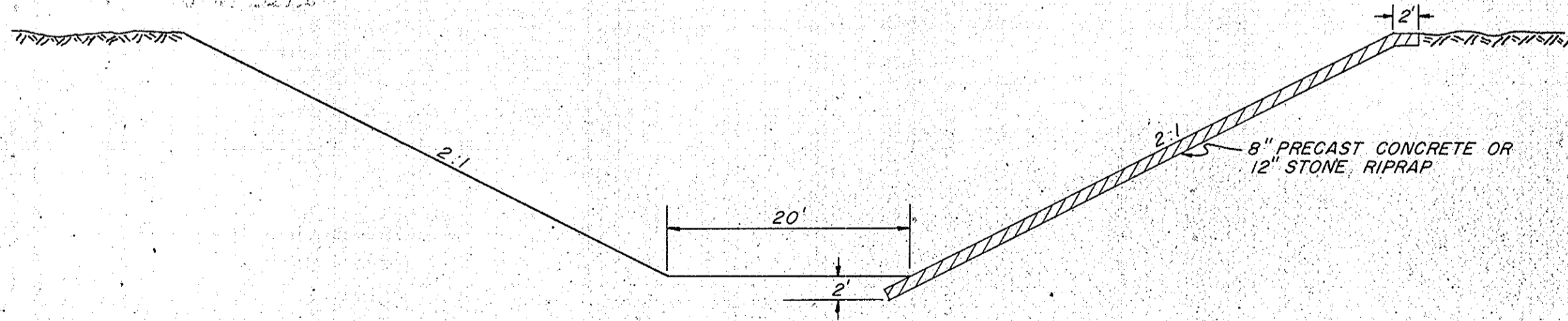


NOTE: R₁ & R₂ TO BE AS SHOWN ON APPROACH AND CROSSOVER TABLE

NOTE: ALL PUBLIC ROAD CROSSOVERS TO BE CONSTRUCTED OF STANDARD CONCRETE PAVEMENT. SEE MISCELLANEOUS STANDARD SHEET FOR PAVEMENT AND CONSTRUCTION DETAILS.

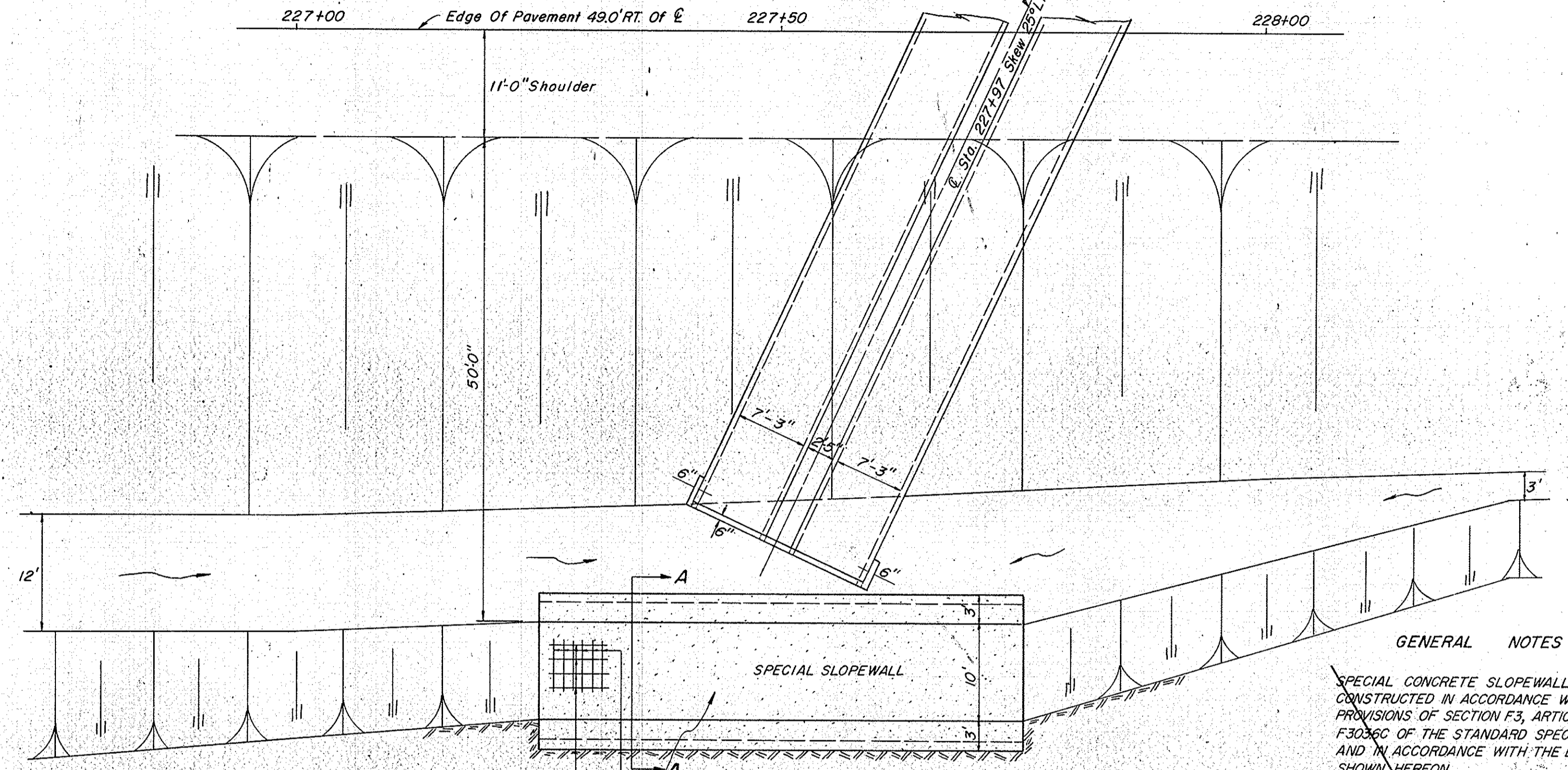
TYPICAL DETAIL OF CURB CONSTRUCTION ON PUBLIC ROAD CROSSOVER TYPE "B"

NOT TO SCALE

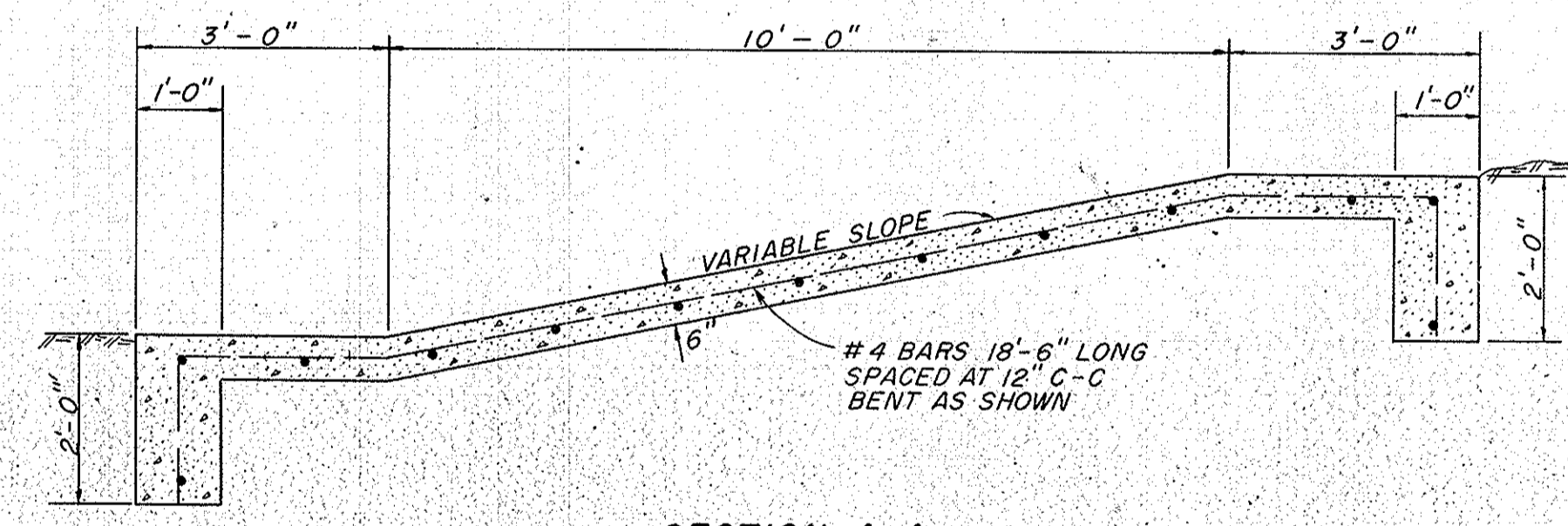


DETAILS OF SUGAR CREEK CHANNEL CHANGE
RT. STA. 380 + 30.25

Scale: 1"=10'



PLAN VIEW
Scale 1"=10'



SECTION A-A
Scale 1"=2'

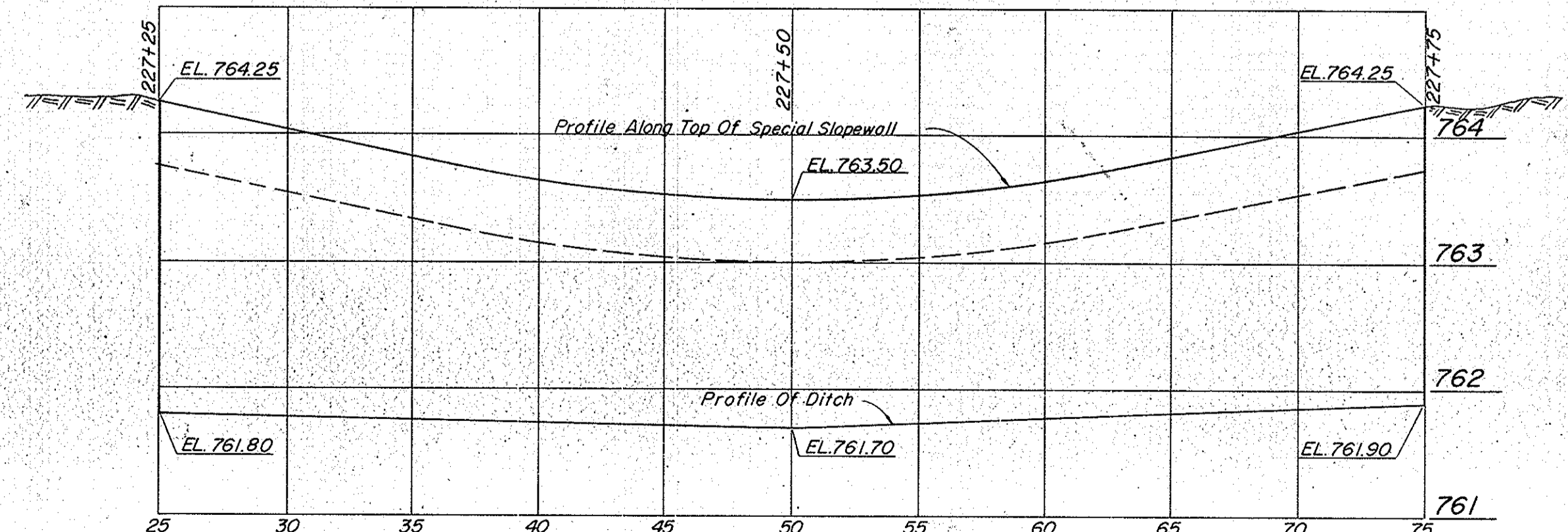
GENERAL NOTES

SPECIAL CONCRETE SLOPEWALL SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PROVISIONS OF SECTION F3, ARTICLE F303.6C OF THE STANDARD SPECIFICATIONS AND IN ACCORDANCE WITH THE DETAILS SHOWN HEREON.

SPECIAL CONCRETE SLOPEWALL SHALL BE MEASURED IN SQUARE YARDS. TOEWALLS SHALL BE CONVERTED INTO EQUIVALENT SQUARE YARDS OF SLOPEWALL AND PAID FOR AS SUCH.

"SPECIAL CONCRETE SLOPEWALL" WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD, WHICH PAYMENT SHALL INCLUDE AND BE FULL COMPENSATION FOR FURNISHING HAULING AND PLACING ALL MATERIALS EXCEPT REINFORCING STEEL, EXCAVATION, EDGING, CONSTRUCTING JOINTS, TOEWALLS, FORMS, FINISHING, CURING, BACKFILLING, AND FOR ALL LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SPECIFIED.

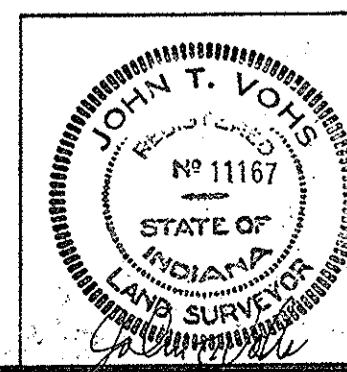
"REINFORCING STEEL", EXCEPT MESH, TIE AND DOWEL BARS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER POUND COMPLETE IN PLACE AND ACCEPTED.



PROFILE
Scale: 1"=1' Vert.
1"=5' Hor.

DETAILS OF SPECIAL SLOPEWALL
STA. 227+25 TO 227+75

DETAILS

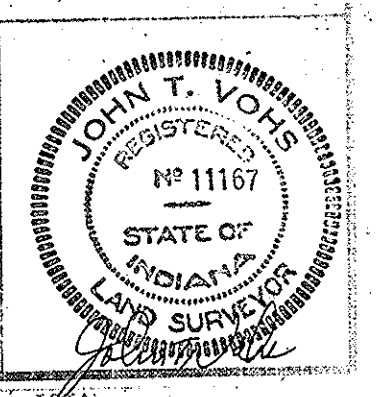


APPROACH & CROSSOVER TABLE

DETAIL	DESCRIPTION	LINE	STATION	EXCAVATION		LENGTH FT.	WIDTH FT.	RADI FT.	SURFACING REQ'D SQ. YDS.	BITUMINOUS MIXTURE FOR APPROACHES			TYPE "P" COMPACTED AGGREGATE					BITUMINOUS MAT'L PRIME COAT	BITUMINOUS MAT'L TACK COAT	BITUMINOUS MAT'L SEAL COAT	COVERING AGGREGATE	6" PLAIN CONC. PAVEMENT	8" PLAIN CONC. PAVEMENT	9" REINF. CONC. PAVEMENT	REINFORCING STEEL	INTEGRAL CONC. CURB TYPE "B"	INTEGRAL CONC. CURB	SPECIAL CONC. CURB	CONC. CENTER CURB	1" PREFORMED JOINT FILLER	1" PREFORMED EXPANSION JOINT W/LOAD TRANSFER					
				CUT CYS.	FILL CYS.					LBS. PER SQ. YD.			3" SYS.	5" SYS.	8" SYS.	9" SYS.	SURF. 8" SYS.																			
										220 SYS.	110 SYS.	135 SYS.																								
	EARTH CROSSOVER	USR 41 PR.	471+62	3	70	24	19'-31'																													
	CLASS II DRIVE	USR 41 PR.	LT. 471+62	0	75	48	15'-25'																													
TYPE "B"	PUBLIC ROAD APPROACH	S-1-USR 41 GR	LT. 482+54.99	475	877	347.4	20	38'-38"	853.3			741.3			741.3		741.3	741.3				112.0	85.7							10						
TYPE "B"	PUBLIC ROAD CROSSOVER	S-1-USR 41 GR	℄ 482+54.99	5	124	40	24'-8"	24'-8"	214.4													214.4	140.2	83.4						20	41					
TYPE "B"	PUBLIC ROAD APPROACH	S-1-USR 41 GR	RT. 482+54.99	34	675	354.6	20	38'-38"	869.0			757.0			757.0		757.0	757.0				112.0	85.7							10						
	CLASS II DRIVE	S-1-USR 41 GR	RT. 46+45.0	0	40	16.4	24	15'-25'	65.3	65.3	65.3			65.3		65.3	65.3	65.3																		
	CLASS II DRIVE	S-1-USR 41 GR	RT. 53+00.0	0	30	13	24	15'-25'																												
	CLASS II DRIVE	USR 41 PR	LT. 493+25	2	184	899	24	15'-25'	260.0	260.0	260.0			260.0		260.0	260.0	260.0													20	25.5				
	PR. DR. CROSSOVER	USR 41 PR	℄ 493+25	3	26	24	19'-31'	196.4														196.4														
TYPE "B"	PUBLIC ROAD CROSSOVER	S-1-G REV.	℄ 225+00	9	88	40	31'-8"	288.9														288.9	139.1	100.4						20	43					
TYPE "B"	PUBLIC ROAD APPROACH	S-1-G REV.	RT. 225+00	121	925	85.6	20	36'-0"	335.8			213.0			213.0		213.0	213.0				122.8	100.5							10						
TYPE "B"	PUBLIC ROAD	S-1-G REV.	51+50.49 TO 66+33.45	3292	7930	1483.0	20	36'-0"	3295.6			3295.6			3295.6		3295.6	3295.6																		
TYPE "B"	PUBLIC ROAD APPROACH	S-1-G REV.	66+33.45 TO 67+19	56	130	85.6	20	36'-0"	335.8			335.8			335.8		335.8	335.8																		
TYPE "B"	PUBLIC ROAD APPROACH	S-2-G PR.	LT. 280+54.34	1498	1749	815.8	20	36'-0"	1958.5			1844.6			1844.6		1844.6	1844.6				113.9	100.5							10						
TYPE "B"	PUBLIC ROAD CROSSOVER	S-2-G PR.	℄ 280+54.34	8	66	40	31'-8"	288.9														288.9	139.1	100.4						20	43					
TYPE "B"	PUBLIC ROAD APPROACH	S-2-G PR.	RT. 280+54.34	2558	235	880.9	20	36'-0"	2103.3			1989.4			1989.4		1989.4	1989.4				113.9	100.5							10						
	CLASS II DRIVE	S-2-G PR.	RT. 41+65.5	0	17	15.6	24	15'-25'																												
TYPE "B"	PUBLIC ROAD APPROACH	S-3L-G PR.	RT. 48+04.75 S-2-G PR.	405	2633	552.0	18	38'	1229.4			245.4			1229.4	245.4	245.4	245.4																		
TYPE "B"	PUBLIC ROAD APPROACH	S-3R-G PR.	LT. 51+79.05 S-2-G PR.	1100	680	560.6	18	36'-0"	1284.6			335.8			1284.6	335.8	335.8	335.8																		
TYPE "B"	PUBLIC ROAD APPROACH	S-4-G PR.	LT. 314+43.73	766	2899	745.0	20	36'-0"	1800.1			1660.5			1660.5		1660.5	1660.5				139.6	100.5							10						
TYPE "B"	PUBLIC ROAD CROSSOVER	S-4-G PR.	℄ 314+43.73	6	88	40	31'-8"	288.9														288.9	139.1	85.9						20	43					
TYPE "B"	PUBLIC ROAD APPROACH	S-4-G PR.	RT. 314+43.73	562	196	382.4	20	36'-0"	994.3			880.4			880.4		880.4	880.4				113.9	100.5							10						
	CLASS II DRIVE	S-4-G PR.	RT. 44+48 S-4-G PR.	4	31	29.0	40	20'	148.0	148.0	148.0			148.0		148.0	148.0	148.0																		
TYPE "C"	PUBLIC ROAD APPROACH	LOCUST ST.	RT. 47+62.5 S-4-G PR.	13	450	149.0	18'	25'	314.4			314.4			314.4		314.4	314.4																		
	ACCESS RD. NO. 1	S-4-G PR.	LT. 52+78.33 S-4-G PR.	0	2726	994.4	18'	15'-25'																												
TYPE "B"	PUBLIC ROAD APPROACH	S-5-G PR.	LT. 356+92.6	4449	102	650.8	20	36'-0"	1591.9			1452.3			1452.3		1452.3	1452.3				139.6	100.5							10						
TYPE "B"	PUBLIC ROAD CROSSOVER	S-5-G PR.	℄ 356+92.6	8	66	40	30'-0"	334.5														334.5	133.0	71.4						20	53					
TYPE "B"	PUBLIC ROAD APPROACH	S-5-G PR.	RT. 356+92.6	714	1592	672.6	20	36'-0"	1640.2			1526.3			1526.3		1526.3	1526.3				113.9	100.5							10						
*	ACCESS RD. NO. 2	S-5-G PR.	LT. 48+57.32	1234	72	346.2	18	15'-25'																												
TYPE "B"	PUBLIC ROAD APPROACH	S-6-G PR.	LT. 422+40	569	3444	593.1	22	30'-0"	1594.6			1426.6			1426.6		1426.6	1426.6				168.0	100.5							10						
TYPE "B"	PUBLIC ROAD CROSSOVER	S-6-G PR.	℄ 422+40	8	66	40	30'-0"	334.5														334.5	133.0	71.4						20	53					
TYPE "B"	PUBLIC ROAD APPROACH	S-4-B	LT. 437+00.68	101	1393	249.4	20	38'	620.9			522.2			522.2		522.2	522.2				98.7	85.7							10						
TYPE "B"	PUBLIC ROAD CROSSOVER	S-4-B	℄ 437+00.68	8	66	40	31'-8"	283.1														283.1	140.2	106.5						20	41					
TYPE "B"	PUBLIC ROAD APPROACH	S-4-B	RT. 437+00.68	451	70	249.4	20	38'	627.2			528.5			528.5		528.5	528.5				98.7	85.7							10						
	CLASS II DRIVE	B	LT. 267+14.5	5	87	66	24	15'-25'	196.3	196.3	196.3			196.3		196.3	196.3	196.3																		
	PR. DR. CROSSOVER	B	℄ 267+14.5	5	42	24	30'-5"	190.9														190.9									20	25.5				

NO RIGHT OF ENTRY REQUIRED FOR CONSTRUCTION OF DRIVES.
 † APPROACH APRON SHALL BE CONSTRUCTED OF BITUMINOUS MATERIAL AND TYPE "P" COMPACTED AGGREGATE BASE.
 * INCLUDES QUANTITY FOR CLASS II DRIVE.

DETAILS

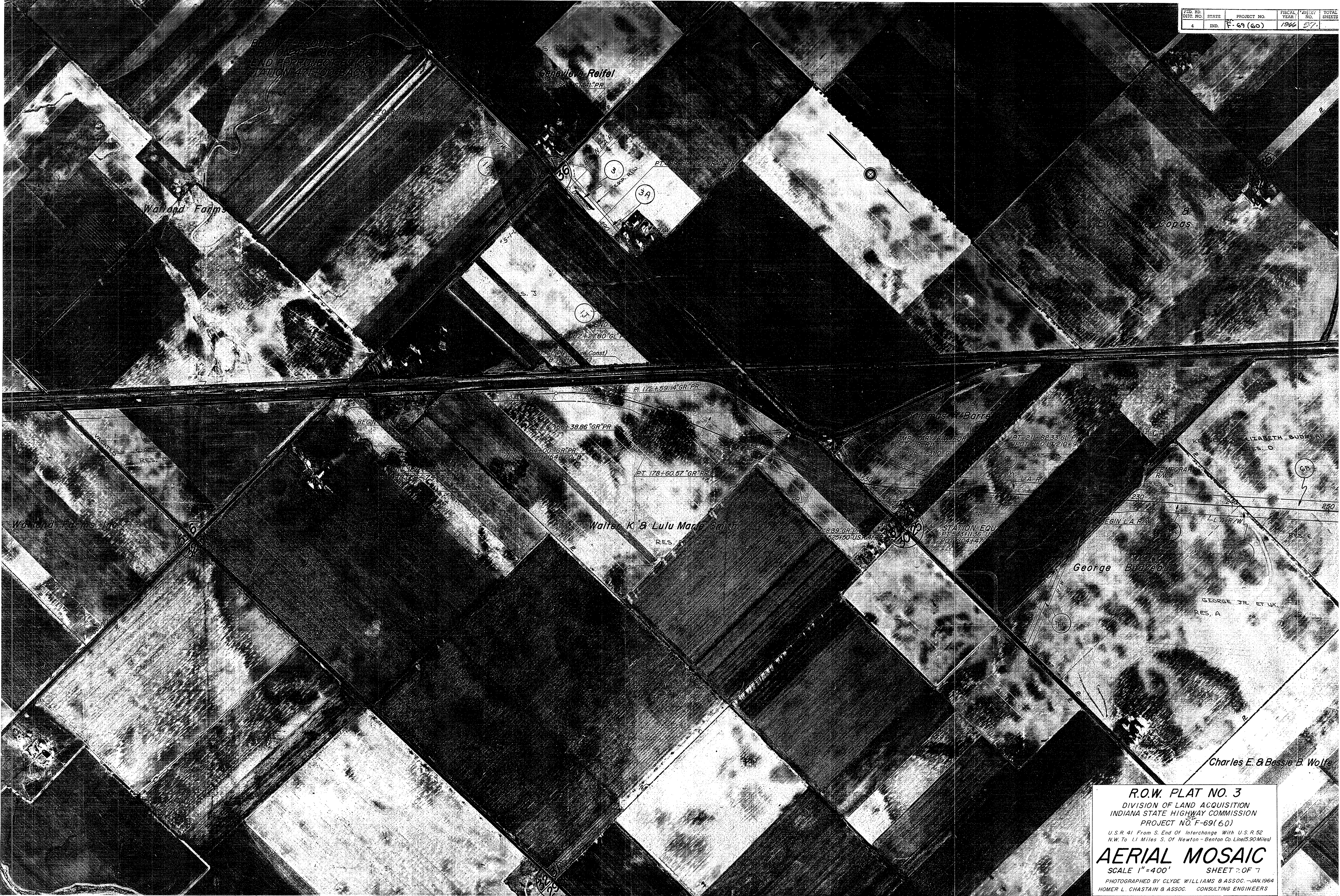


APPROACH TABLE

LOCATION	DESCRIPTION	CUT	FILL	WIDTH	RADIUS	LENGTH	GRADE LESS THAN 10% NOT SHOWN	DISTANCE FROM CURB LINE	BITUMINOUS MIXTURE FOR APPROACHES SQ.YDS.	COMPACTED AGGREGATE BASE SQ.YDS.			GUIDE POST TYPE "A"
										3"	6"	8"	
		CYS.	CYS.	FT.	FT.	FT.			3"	6"	8"	EACH	
24+00 Lt.	CLASS IV	20		30	20'-20'	48			178.9		178.9		6
31+95 Rt.	CLASS V	24		12	15'-25'	132							
31+95 Lt.	CLASS II	5		12	15'-25'	48			84.2		84.2		
41+75 Rt.	CLASS II	35		12	15'-25'	132			196.2		196.2		
49+75 Lt.	CLASS II	5		12	15'-25'	48							
60+40 Rt.	CLASS V	41		12	15'-25'	132							
60+40 Lt.	CLASS II	3		12	15'-25'	48			84.2		84.2		
73+50 Rt.	CLASS II	24		12	15'-25'	132			196.2		196.2		
73+50 Lt.	CLASS II	5		12	15'-25'	48			84.2		84.2		
83+85 Rt.	CLASS V	23		12	15'-25'	132							
83+85 Lt.	CLASS II	5		12	15'-25'	48			84.2		84.2		
92+90 Rt.	CLASS II	21		12	15'-25'	132			196.2		196.2		
101+75 Lt.	CLASS II	5		12	15'-25'	48			84.2		84.2		
105+80 Rt.	CLASS II	23		12	15'-25'	132			196.2		196.2		
116+40 Rt.	TYPE "B"	54		20	38.0'	60			245.4			245.4	
116+40 Lt.	TYPE "B"	54		20	38.0'	60			245.4			245.4	
116+60, 210' Rt.	CLASS V	2		12	15'-25'	8							
168+45, 210' Rt.	CLASS V	1		12	15'-25'	10							
168+60 Rt.	TYPE "B"	54		20	38.0'	60			245.4			245.4	
168+60 Lt.	TYPE "B"	54		20	38.0'	60			245.4			245.4	
168+75, 210' Rt.	CLASS V	1		12	15'-25'	14							
168+75, 210' Lt.	CLASS V	5		12	15'-25'	48							
208+80 Rt.	CLASS II	24		12	15'-25'	132			196.2		196.2		
221+51 Lt.	TYPE "B"	54		20	15'-25'	60							
221+62 Rt.	TYPE "B"	54		20	15'-25'	60							
221+62 Lt.	TYPE "B"	54		20	15'-25'	60							
221+87, 210' Rt.	CLASS II	5		12	15'-25'	22			49.6		49.6		
274+82 Rt.	TYPE "B"	54		20	38.0'	60			246.5			246.5	
274+82 Lt.	TYPE "B"	54		20	38.0'	60			246.9			246.9	
312+70 Rt.	CLASS V	30		12	15'-25'	132							
312+70 Lt.	CLASS II	6		12	15'-25'	48			84.2		84.2		
327+13 Rt.	TYPE "B"	54		20	38.0'	60			246.5			246.5	
327+27, 210' Rt.	CLASS V	5		12	15'-25'	132							
346+00 Rt.	CLASS II	6		12	15'-25'	132			196.2		196.2		
346+00 Lt.	CLASS V	5		12	15'-25'	74							
365+50 Lt.	CLASS II	5		12	15'-25'	48			84.2		84.2		
380+00 Rt.	SR 18, TYPE "B"												
380+00 Lt.	SR 18, TYPE "B"												
385+75 Rt.	CLASS II	6		12	15'-25'	132			196.2		196.2		
385+75 Lt.	CLASS II	6		12	15'-25'	48			84.2		84.2		
415+15 Rt.	CLASS II	6		12	15'-25'	132			196.2		196.2		
415+15 Lt.	CLASS V	5		12	15'-25'	48							
442+15 Rt.	CLASS II	97		12	15'-25'	132			196.2		196.2		
451+80 Lt.	CLASS II	70		12	15'-25'	48			84.2		84.2		



R.O.W. PLAT NO. 3
DIVISION OF LAND ACQUISITION
INDIANA STATE HIGHWAY COMMISSION
PROJECT NO. F-69(60)
U.S.R. 52 From Dual Lane Pavement At Fowler N.W.
to S. End Of Interchange With U.S.R. 41 (3.10 Miles)
AERIAL MOSAIC
SCALE 1" = 400' SHEET 1 OF 7
PHOTOGRAPHED BY CLYDE WILLIAMS & ASSOC. -



Walden Farms

Genevieve Reifel

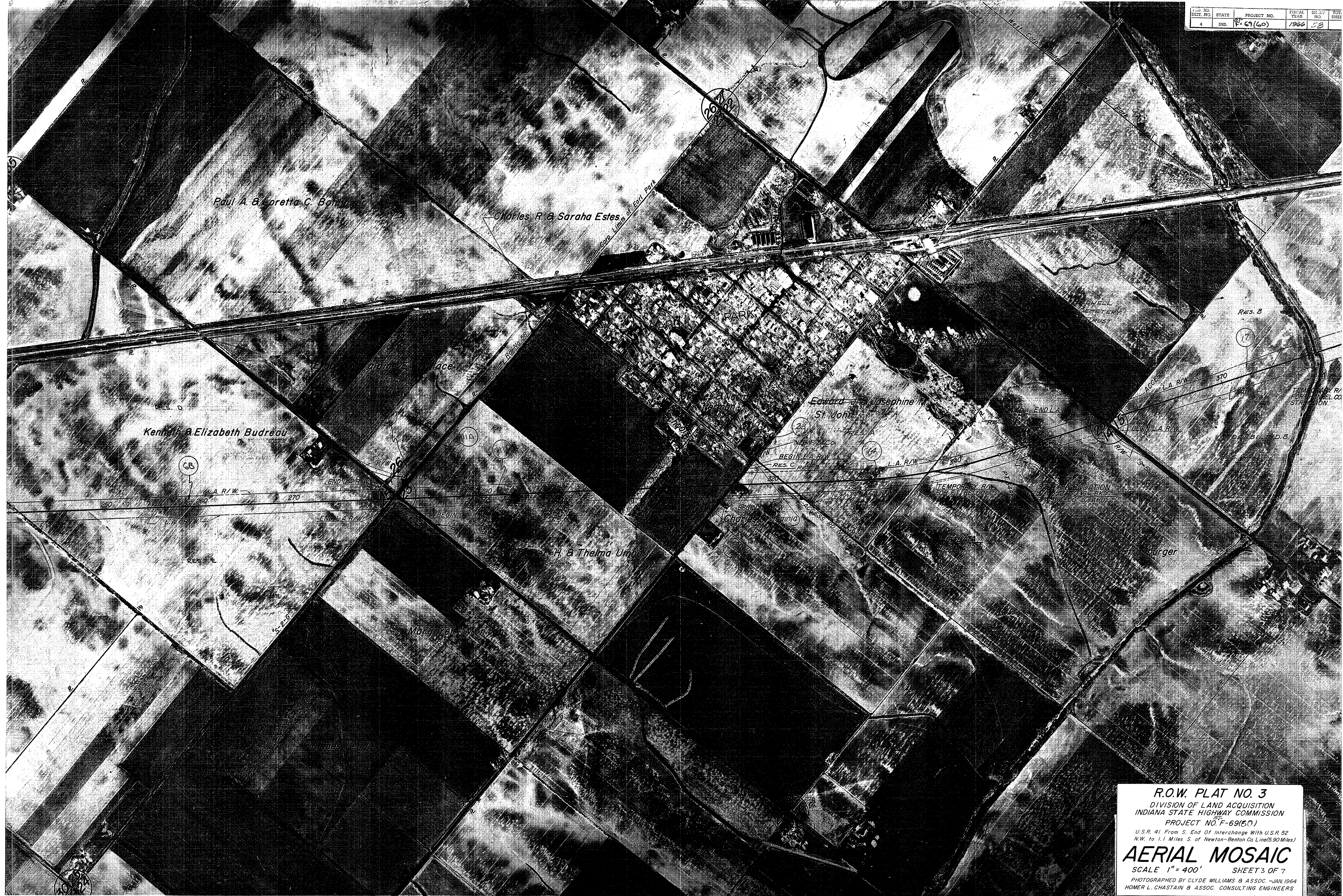
Walter K & Lulu Marie Smith

George Budeau

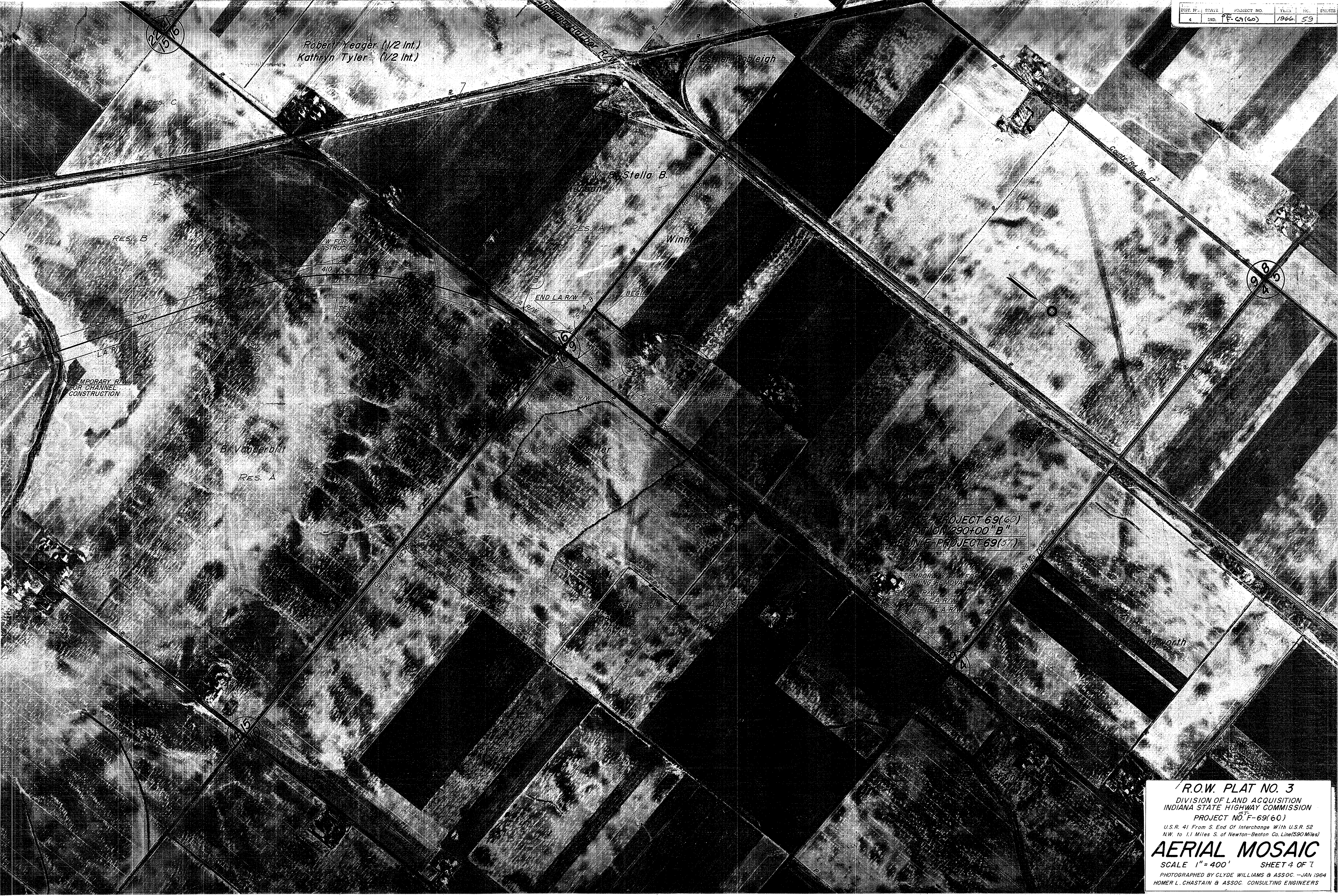
Charles E. & Bessie B. Wolfe

PT 162+27.80' 6L
PT 172+59.14' 6R
PT 166+38.86' 6R
PT 178+60.57' 6R
PT 211+82.33' 6L
PT 210+90' 6L
PT 225+11.36' 6L
PT 225+11.36' 6L
PT 225+11.36' 6L

R.O.W. PLAT NO. 3
 DIVISION OF LAND ACQUISITION
 INDIANA STATE HIGHWAY COMMISSION
 PROJECT NO. F-69(60)
 U.S.R. 41 From S. End Of Interchange With U.S.R. 52
 N.W. To 1.1 Miles S. Of Newton - Benton Co. Line (3.90 Miles)
AERIAL MOSAIC
 SCALE 1" = 400' SHEET 57 OF 7
 PHOTOGRAPHED BY CLYDE WILLIAMS & ASSOC. - JAN 1964
 HOMER L. CHASTAIN & ASSOC. CONSULTING ENGINEERS



R.O.W. PLAT NO. 3
 DIVISION OF LAND ACQUISITION
 INDIANA STATE HIGHWAY COMMISSION
 PROJECT NO. F-69(60)
 U.S.R. 41 From S. End Of Interchange With U.S.R. 52
 N.W. to 1.1 Miles S. of Newton-Benton Co. Line (5.90 Miles)
AERIAL MOSAIC
 SCALE 1" = 400' SHEET 3 OF 7
 PHOTOGRAPHED BY CLYDE WILLIAMS & ASSOC. - JAN 1964
 HOMER L. CHASTAIN & ASSOC. CONSULTING ENGINEERS



Robert Yeager (1/2 Int.)
Kathryn Tyler (1/2 Int.)

Stella B. Erson

Winn

J. B. Vanderbilt

J. Gilbert Keller

Esther Cobleigh

North

PROJECT 69(60)
STATION 290+00 "B"
BEGIN - PROJECT 69(57)

TEMPORARY R/W FOR
BUILDING REMOVAL
END L.A.R.W.
BEGIN L.A.R.W.
END L.A.R.W.

R.O.W. PLAT NO. 3
DIVISION OF LAND ACQUISITION
INDIANA STATE HIGHWAY COMMISSION
PROJECT NO. F-69(60)
U.S.R. 41 From S. End Of Interchange With U.S.R. 52
N.W. to 1.1 Miles S. of Newton-Benton Co. Line(590 Miles)
AERIAL MOSAIC
SCALE 1" = 400' SHEET 4 OF 7
PHOTOGRAPHED BY CLYDE WILLIAMS & ASSOC. - JAN 1964
HOMER L. CHASTAIN & ASSOC. CONSULTING ENGINEERS



F.T. Fence

WATLAND FARMS INC.
RES.

RIGHT-OF-WAY PLAT #3
PROJECT F-69(60)
MOSAIC SCALE 1" = 400' SHEET 5 OF 7
DATE OF PHOTOGRAPHY: -
SOURCE:
UNCONTROLLED PHOTO MOSAIC
INDIANA STATE HIGHWAY COMMISSION
BUREAU OF PHOTOGRAMMETRIC & ELECTRONIC PROCESSES



320
L.A. R/W A.C.L. & F.F.T. Fence

370
L.A. R/W A.C.L. & F.F.T. Fence

390
L.A. R/W A.C.L. & F.F.T. Fence

395
L.A. R/W A.C.L. & F.F.T. Fence

400
L.A. R/W A.C.L. & F.F.T. Fence

435
L.A. R/W A.C.L. & F.F.T. Fence

60' R/W A.C.L. & F.F.T. Fence

5818

RIGHT-OF-WAY PLAT #3
 PROJECT F-69 (60)
 MOSAIC SCALE 1" = 400' SHEET 6 OF 7
 DATE OF PHOTOGRAPHY:
 SOURCE:
 UNCONTROLLED PHOTO MOSAIC
 INDIANA STATE HIGHWAY COMMISSION
 BUREAU OF PHOTOGRAMMETRIC & ELECTRONIC PROCESSES

410

435
440

90' L.A.R. W. ACCL. FENCE
L.A.R. W. ACCL. FENCE
80' L.A.R. W. ACCL. FENCE

90' L.A.R. W. ACCL. FENCE
L.A.R. W. ACCL. FENCE

L.A. W. A. C. F. FENCE

WATLAND FARMS, INC.
RES. K

WATLAND FARMS, INC.

RIGHT-OF-WAY PLAT #3
PROJECT: F-69 (60)
MOSAIC SCALE 1" = 400' SHEET 7 OF 7
DATE OF PHOTOGRAPHY:
SOURCE:
UNCONTROLLED PHOTO MOSAIC
INDIANA STATE HIGHWAY COMMISSION
BUREAU OF PHOTOGRAMMETRIC & ELECTRONIC PROCESSES