

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(5)64	1959	1	35

Rev 3-6-61 Sheets 1-10 Beginning of Project
 Rev 10-3-51 Sheet 11 R/W Rev.

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SHEET NO.	TITLE
1	TITLE SHEET
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	STD. REINF. CONC. BOX CULVERT, SKEWED END AND WING DETAILS, SKEW
	STD. REINF. CONC. CULV.—SLAB TOP TYPE WITHOUT FILL (10'-0" TO 20'-0" SPAN)
	STD. REINF. CONC. CULV.—SLAB TOP TYPE UNDER FILL 1'-0" TO 5'-0" (10'-0" TO 20'-0" SPAN)
	STD. REINF. CONC. CULV.—SLAB TOP TYPE UNDER FILL 1'-0" TO 5'-0" (10'-0" TO 20'-0" SPAN) 15° SKEW
	STD. REINF. CONC. CULV.—SLAB TOP TYPE UNDER FILL 1'-0" TO 5'-0" (10'-0" TO 20'-0" SPAN) 30° SKEW
	STD. REINF. CONC. CULV.—SLAB TOP TYPE UNDER FILL 1'-0" TO 5'-0" (10'-0" TO 20'-0" SPAN) 45° SKEW
	STD. REINF. CONC. CULV.—SLAB TOP TYPE UNDER FILL 1'-0" TO 5'-0" (10'-0" TO 20'-0" SPAN) 45° SKEW
	STD. GUARD RAIL JULY 1956
	STEEL BEAM GUARD RAIL REV. 10-57
53	DATA FOR SUPERELEVATING AND WIDENING OF CURVES SEPT 1952
54-55	STD. DETOUR SIGNS SHEET 1 REV. 2-11-54, SHEET 2 REV. 11-12-58
56-183	CROSS SECTIONS

R/W PLANS

STATE OF INDIANA
STATE HIGHWAY DEPARTMENT

CODE 0238

PLAN AND PROFILE OF PROPOSED

STATE HIGHWAY I-74-2(5)64 PE PIERCE GRUBER & BEAM INC.
I-PROJECT NO. I-05-2(15)64 CONST. INDIANAPOLIS, INDIANA

PLANS PREPARED BY

PIERCE GRUBER & BEAM INC. CONSULTING STRUCTURAL ENGINEERS
 INDIANAPOLIS, INDIANA

BEGINNING AT A POINT 1447.8 FEET SOUTH OF THE NORTH LINE AND 117.3 FEET WEST OF THE EAST LINE OF SECTION 3, TOWNSHIP 16 NORTH, RANGE 1 EAST IN HENDRICKS COUNTY, AND RUNNING IN A SOUTH EASTERLY DIRECTION 25,880 FEET TO A POINT 2918.8 FEET SOUTH OF THE NORTH LINE AND 1539.7 FEET EAST OF THE WEST LINE OF SECTION 16, TOWNSHIP 16 NORTH, RANGE 2 EAST IN HENDRICKS AND MARION COUNTIES.

- (A) Type "A" Barricade
- (B) Type "B" Barricade
- (C) Typical Sign Standard

SHEET NO.	TITLE
1	TITLE SHEET
2-3	TYPICAL CROSS SECTION
9	R/W PLAT No. 1 FOR DESIGN
10	R/W PLAT No. 2 FOR R/W
11-21	PLAN AND PROFILE
22	PLAN AND PROFILE S-5-AA
23	PLAN AND PROFILE S-6-AA SEPARATE CONTRACT 136-G9-4434
24	PLAN AND PROFILE S-7-AA SEPARATE CONTRACT 136-G9-4435
25	PLAN AND PROFILE S-8-AA SEPARATE CONTRACT 136-G9-4436

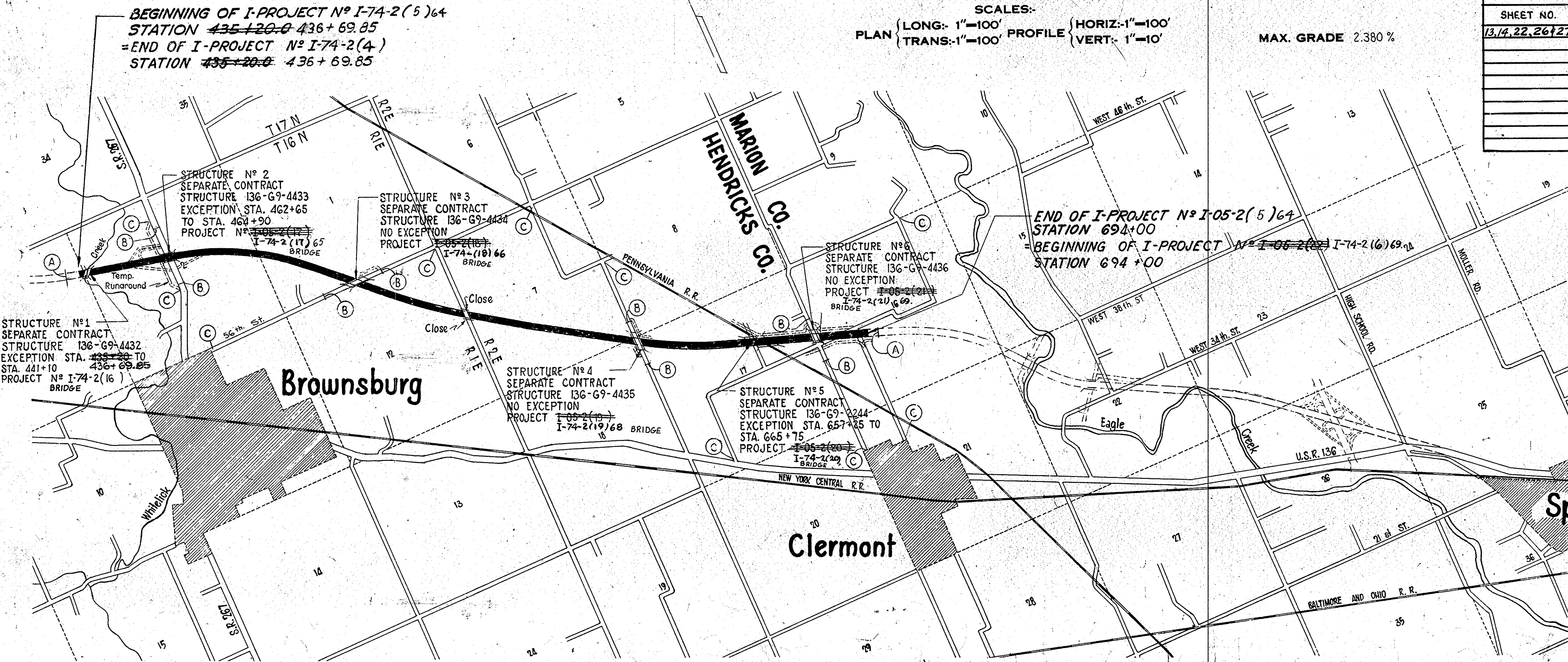
GROSS LENGTH - 4.922 MI. 4.874
 NET LENGTH - 4.886 MI.

SCALES:-
 PLAN { LONG- 1"=100' PROFILE { HORIZ.- 1"=100'
 TRANS.- 1"=100' VERT.- 1"=10'

MAX. GRADE 2.380 %

SHEET NO.	DATE	REVISED
13, 14, 22, 26, 27	12-9-71	R/W Disposition

DESIGN DATA - FOR USR 136
 A.D.T. 5,289
 A.D.T. 15,708
 DIRECTIONAL 0.6
 DESIGN SPEED 70 M.P.H.
 ACCESS CONTROL FULL
 COMMERCIAL 7%



SHEET	DESIGNATION
1	TITLE SHEET
2	PARCEL LISTING
3	PLAT NO. 1
4-10	TYPICAL CROSS SECTIONS
11-25	PLAN & PROFILE SHEETS
26-28	INTERCHANGE SHEETS
29-34	DETAIL SHEETS
35	STRUCTURE DATA

R/W PLANS FOR THIS PROJECT INCLUDE:
 R/W FOR BRIDGE PROJECTS
 136-G9-4432 I-74-2(15)64
 136-G9-4433 I-74-2(17)65
 136-G9-4434 I-74-2(18)66
 136-G9-4435 I-74-2(19)68
 136-G9-2244 I-74-2(20)69
 136-G9-4436 I-74-2(21)69

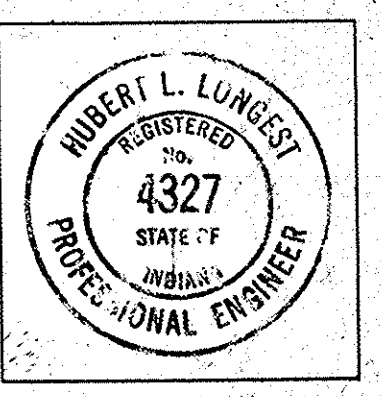
APPROVED AND ADOPTED 12/9/58
 BY STATE HIGHWAY DEPARTMENT OF INDIANA.
 CHAIRMAN STATE HIGHWAY DEPARTMENT OF INDIANA
 APPROVED 12/9/58
 C.E. Fogelstrom
 CHIEF ENGINEER - STATE HIGHWAY DEPARTMENT OF INDIANA

STATE HIGHWAY DEPARTMENT OF INDIANA.
 STANDARD SPECIFICATIONS DATED 1957
 TO BE USED WITH THESE PLANS.

Scale: 1" = 2000'

RECOMMENDED FOR APPROVAL 11-26-58
 W.H. Behrens
 ENGINEER OF ROAD DESIGN, STATE HIGHWAY DEPARTMENT OF INDIANA

Code 0238
 I-74-2(5)64
 I-74 Marion & Hendricks Co.
 35 sheets



CERTIFIED DATE Nov. 6 1958
 Hubert L. Lunge
 PIERCE GRUBER & BEAM INC. CONSULTING STRUCTURAL ENGINEERS

INDIANA STATE HIGHWAY COMMISSION
DIVISION OF LAND ACQUISITION
LAND ACQUISITION ELECTRONIC DATA PROCESSES

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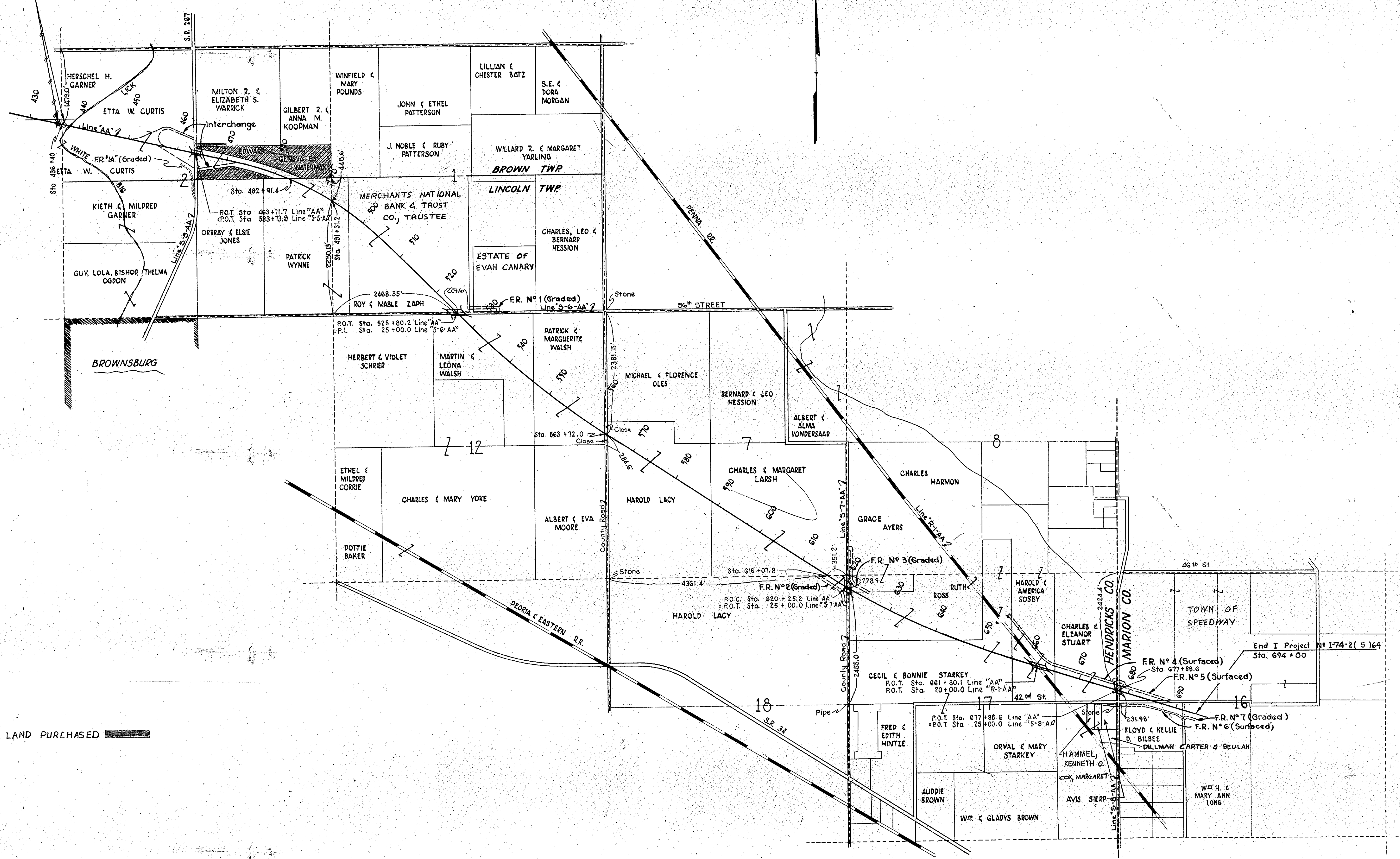
FS = FEE SIMPLE TITLE
PE = PERMANENT R/W
TE = TEMPORARY R/W

PARCEL NUMBER	GRANTOR	LINE CENTER STATION TO STATION L+R	SHEET NUMBER	TOTAL AREA	R/W NATURE OF AREA	RESIDUE LEFT	RESIDUE RIGHT
1	GARNER, HERSCHEL ET UX. AA	77.850AC	44	72.180AC	PE	1.140AC	
2	ACQUISITION THEREOF ENTIRELY UNDER SAID PROJECT 1-74-2(5) AND PARCEL 44 ON PROJECT 1-74-2(4) COVER THE SAME LAND, WITH						
2	CURTIS, ETTA W.	139.500AC	24	46.720AC	PE	68.287AC	
21	ICNWAB				TE		
211	ICNWAB				FS		
212	ICNWAB				FS		
3	GARNER, KEITH ET UX. S5AA	86.129AC	1	84.224AC	PE		
4	WARRICK, MILTON R. ET UX. S5AA	58.780AC		0.560AC	PE		
5	WATERMAN, E. L. ET UX. AA	41.500AC	41	41.500AC	FS		
6	JONES, ORBRAY P. ET UX. S5AA	60.860AC		0.188AC	PE		
6T	S5AA				TE		
7	WYNE, PATRICK	100.000AC	6	5.650AC	PE		
8	MERCHANTS BANK, TRUSTEE AA	175.850AC	14	119.260AC	PE		
9	EVAN E. CANARY ESTATE S6AA	40.000AC		1.990AC	PE		
10	SCHRIBER, H. D. ET UX. S6AA	120.000AC		0.284AC	PE		
11	WALSH, MARTIN J. ET UX. AA	80.000AC		9.654AC	PE		
11A	S6AA				PE		
11B	S6AA				PE		
12	WALSH, PATRICK T. ET UX. AA	80.000AC		7.610AC	PE		
13	LACY, HAROLD A. ESTATE AA	402.610AC	11	21.940AC	PE		
13A	S7AA				PE		
14	LARSH, CHARLES R. ET UX. AA	160.000AC	12	112.610AC	PE		
14A	S7AA				PE		
15	AYERS, GRACE M.	76.480AC	2	2.790AC	PE		
16	ROSS, RUTH	67.420AC	11	11.050AC	PE		
17	STARKEY, CECIL M. ET UX. AA	161.670AC	5	5.540AC	PE		
18	SOSBY, HAROLD ET UX. AA	39.950AC	2	2.180AC	PE		
18A	AA				PE		
19	STUART, CHARLES E. ET UX. AA	80.000AC	9	4.29AC	PE		
19A	FR6				PE		
20	SIERP, AVIS M.	74.870AC		0.846AC	PE		
21	BILBEE, FLOYD ET UX. AA	30.000AC	5	5.422AC	PE		
22	TOWN OF SPEEDWAY	161.250AC	5	5.800AC	PE		
23	HAMMEL, KENNETH OMAR	1.830AC		0.081AC	PE		
24	COX, MARGARET E.	1.960AC		0.109AC	PE		
25	DILLMAN, CARTER ET UX. S8AA	2.680AC		0.122AC	PE		
26	PARCEL 26 ON PROJECT 1-74-2(5) AND PARCEL 1 ON PROJECT 1-74-2(6) ACQUISITION THEREOF ENTIRELY UNDER SAID PROJECT 1-74-2(6) COVER THE SAME LAND, WITH						

FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(5)64	1959	3	35

Design Revision 3-25-59
Rev. 3-6-61 Beginning of Project

Begin I Project No I-74-2(5) Item 2
Sta. 433+00.0
436+69.85



EXCESS LAND PURCHASED

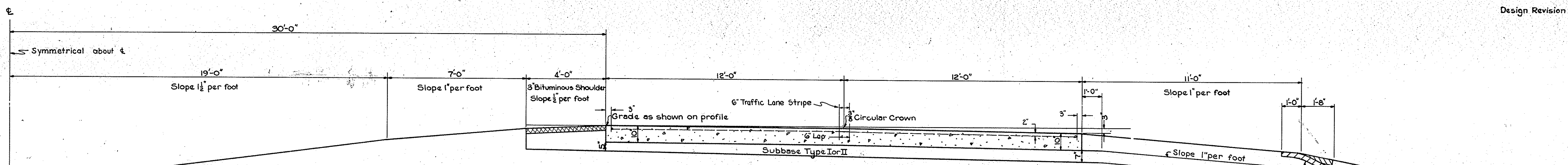
LEGEND
Buy in Fee Simple

PROJECT No I-74-2(5)
PLAT No 1
FOR DESIGN DEPT.
Scale: 1"=1000'

WMB 11-21-58

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(2)64	1959	4	35

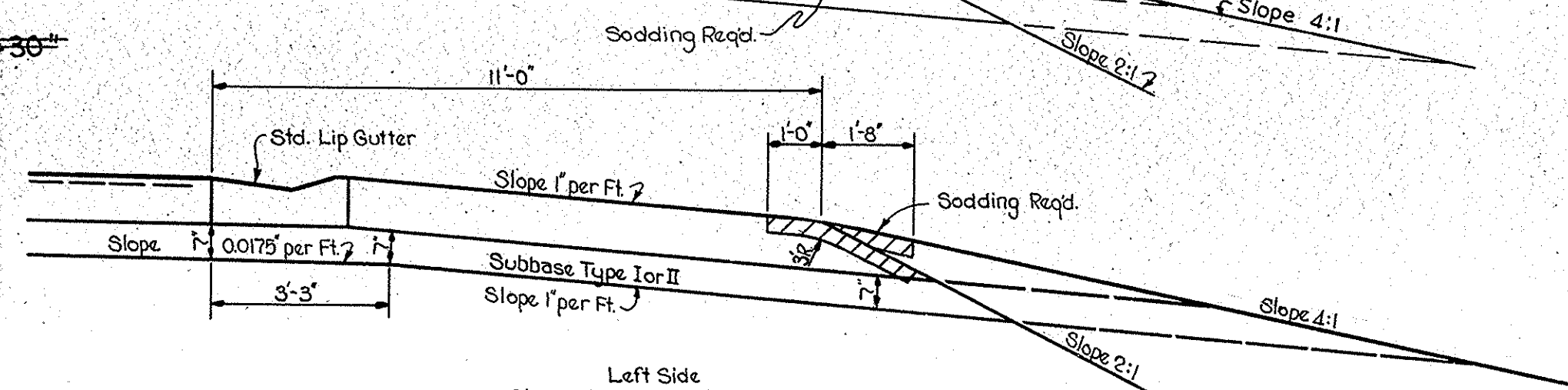
Design Revision: 3/20/59



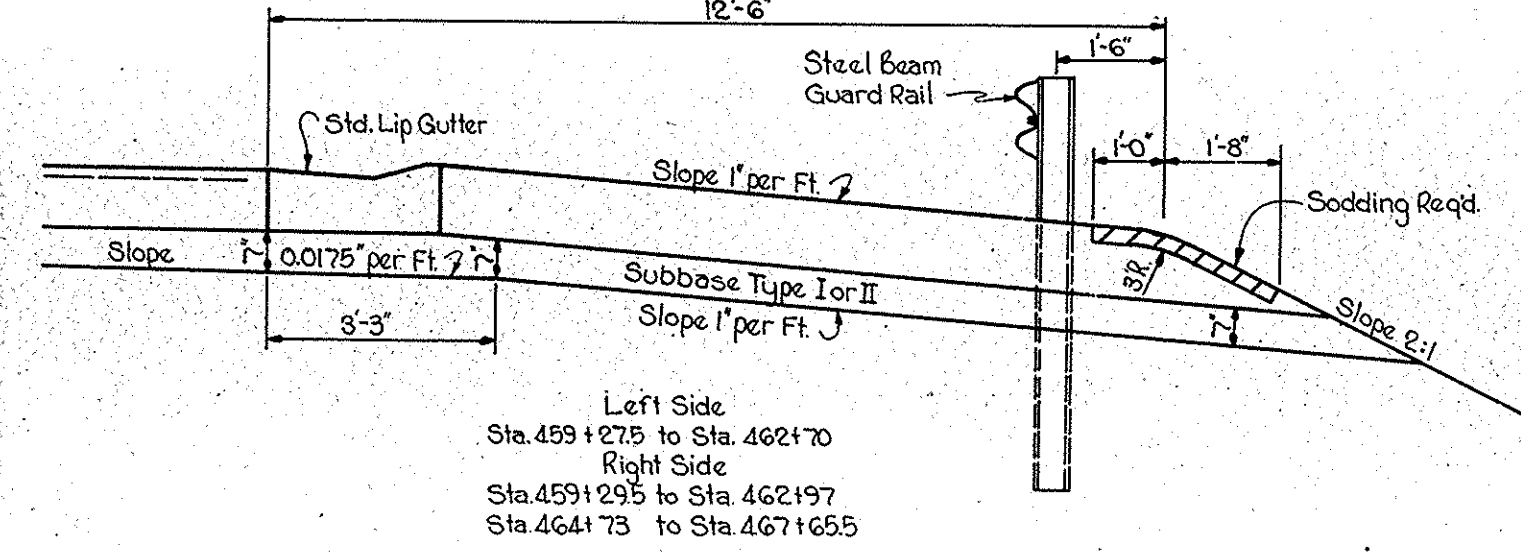
TYPICAL HALF SECTION INSIDE INTERCHANGE LINE "AA"

Left Side
Sta. 448+26.23 to Sta. 462+69.62
Right Side
Sta. 453+76.79 to Sta. 462+97.22
Sta. 464+73.78 to Sta. 476+62.89

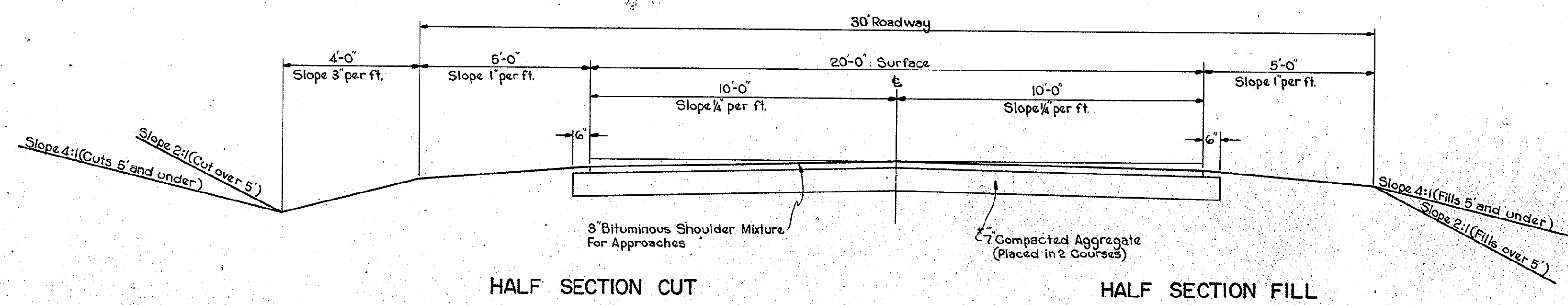
For Additional Details See Sheets 22, 24, 25, 28 & 30



Left Side
Sta. 458+69.62 to Sta. 459+27.5
Right Side
Sta. 456+48 to Sta. 459+29.5
Sta. 467+65.5 to Sta. 469+150

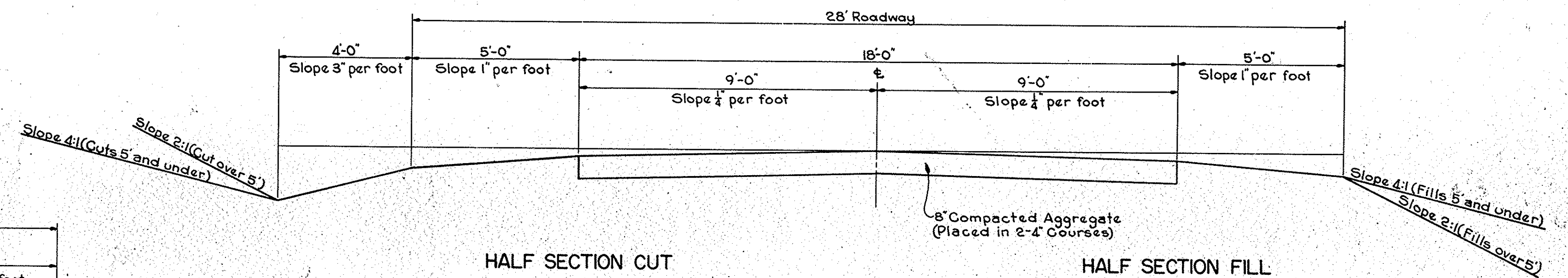


Left Side
Sta. 459+27.5 to Sta. 462+70
Right Side
Sta. 459+29.5 to Sta. 462+97
Sta. 464+75 to Sta. 467+65.5



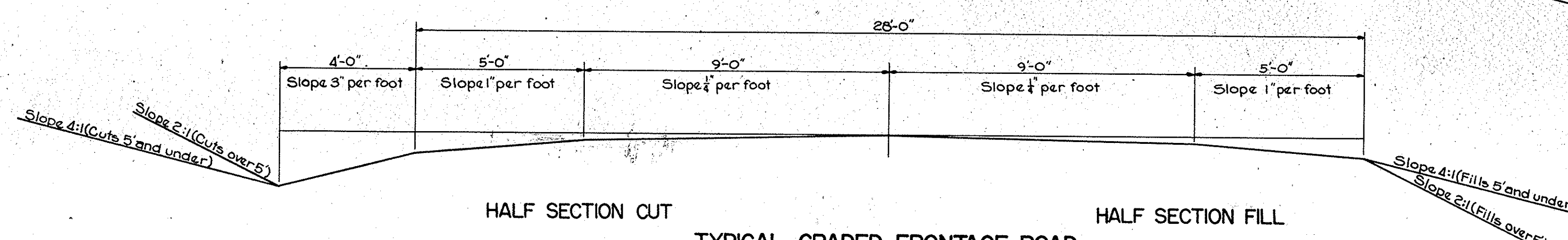
TYPICAL SURFACED FRONTAGE ROAD

Fr. Rd. #6 - 130' Rt. of Sta. 684+10 Line "AA" to Sta. 693+92.29 on Fr. Rd. #6



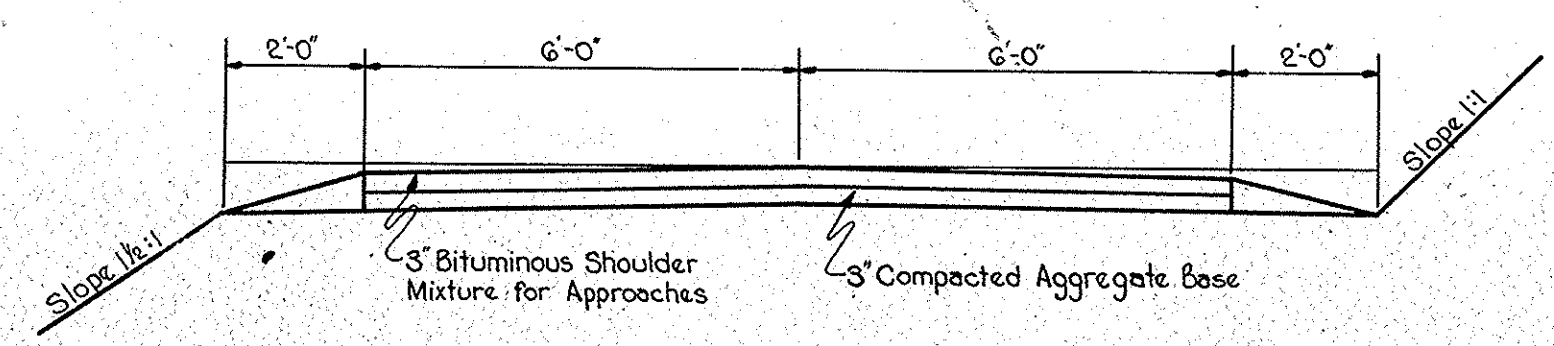
TYPICAL SURFACED FRONTAGE ROAD

Fr. Rd. #4 - 93' Lt. of Sta. 13+17.47 Line "R1-AA" to 100' Rt. of Sta. 22+00.18 Line "S-8-AA" Fr. Rd. #5 Sta. 676+95.44 to Sta. 690+72.66



TYPICAL GRADED FRONTAGE ROAD

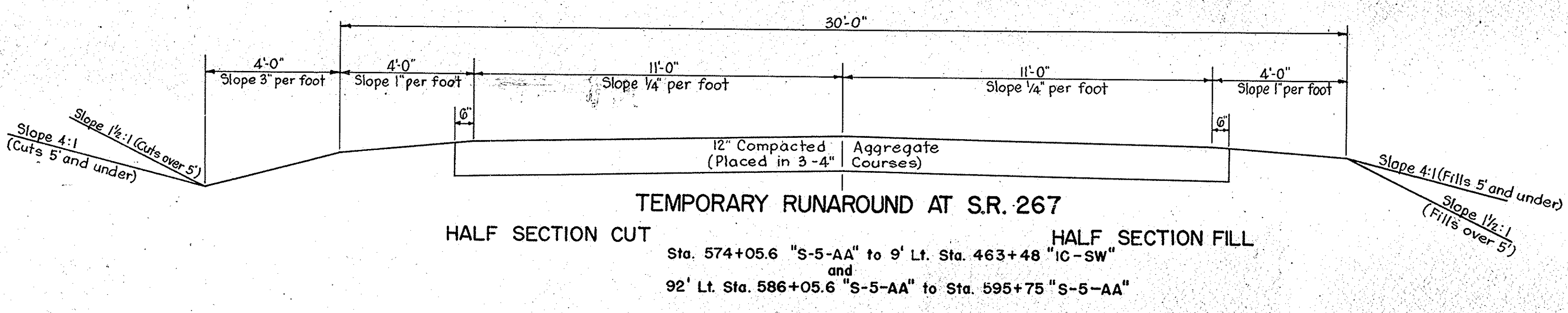
Fr. Rd. 2 - 120' Rt. of Sta. 613+28 Line "AA" to 100' Lt. of Sta. 27+37.69 Line "S-7-AA"
Fr. Rd. 3 - 100' Lt. of Sta. 22+60.03 Line "S-7-AA" to 100' Lt. of Sta. 624+31.13 Line "AA"
Fr. Rd. 7 - Sta. 692+20 Fr. #7 to 120' Rt. of Sta. 694+00 Line "AA"
Fr. Rd. 1-A 100' Lt. of Sta. 578+70.77 Line "S-5-AA" to 75' Rt. of Sta. 461+75 Line "IC-SW"



TYPICAL SECTION PRIVATE DRIVE ON LINE "S-5-AA"

TYPICAL CROSS SECTIONS

SCALE: $\frac{3}{8} = 1'-0"$



TEMPORARY RUNAROUND AT S.R. 267

Sta. 574+05.6 "S-5-AA" to 9' Lt. Sta. 463+48 "IC-SW" and 92' Lt. Sta. 586+05.6 "S-5-AA" to Sta. 595+75 "S-5-AA"

APPROVED *C. E. ...*
CHIEF ENGINEER - STATE HIGHWAY COMMISSION OF INDIANA

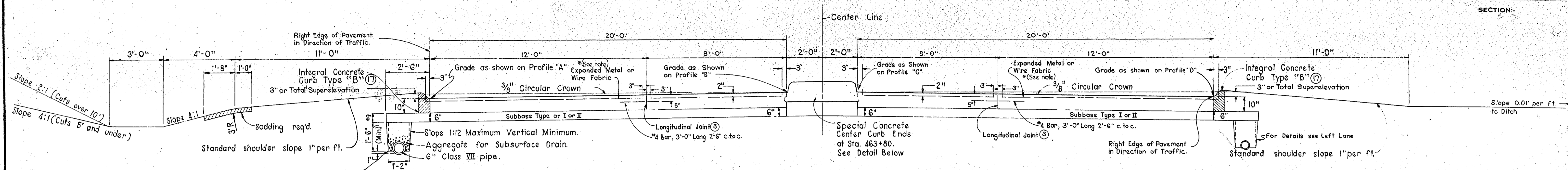
RECOMMENDED FOR APPROVAL 11-26-58

APPROVED *W. H. ...*
CHAIRMAN - STATE HIGHWAY COMMISSION OF INDIANA

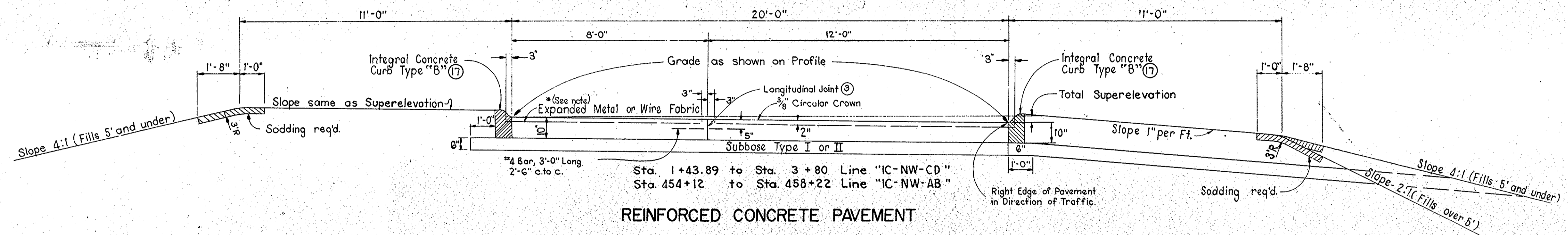
W. H. ...
ENGINEER OF ROAD DESIGN, STATE HIGHWAY COMMISSION OF INDIANA

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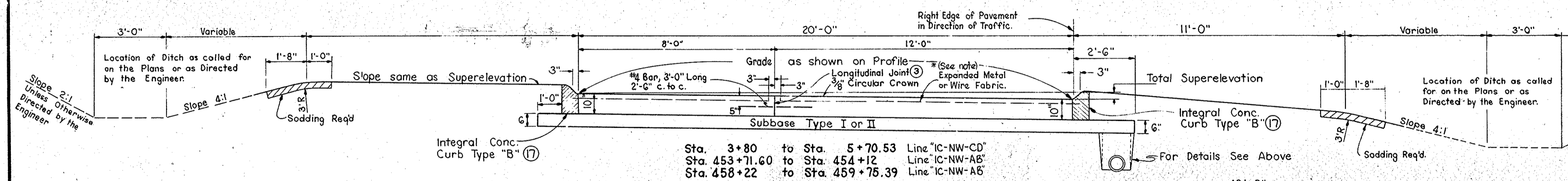
SECTION:



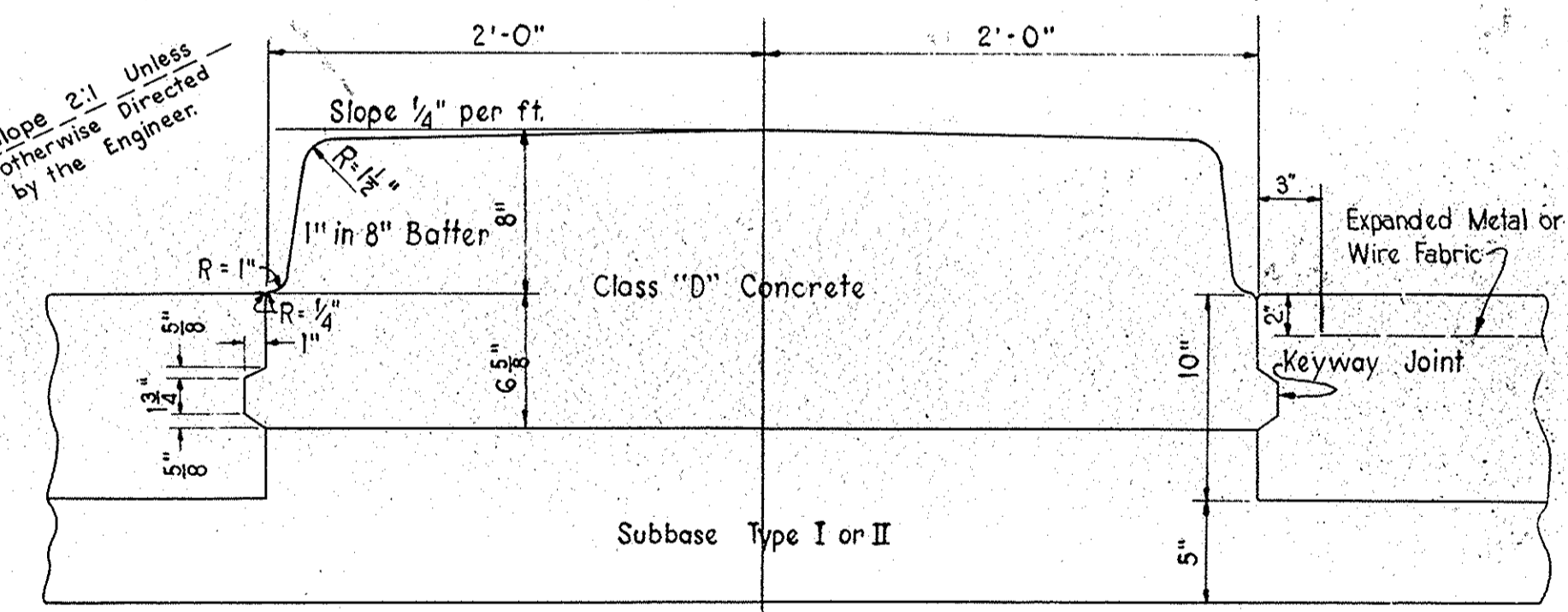
Sta. 459+75.39 to Sta. 464+07.48
REINFORCED CONCRETE PAVEMENT
 Line "IC-NWAB & CD"



Sta. 443.89 to Sta. 458+22
REINFORCED CONCRETE PAVEMENT
 Line "IC-NW-AB"

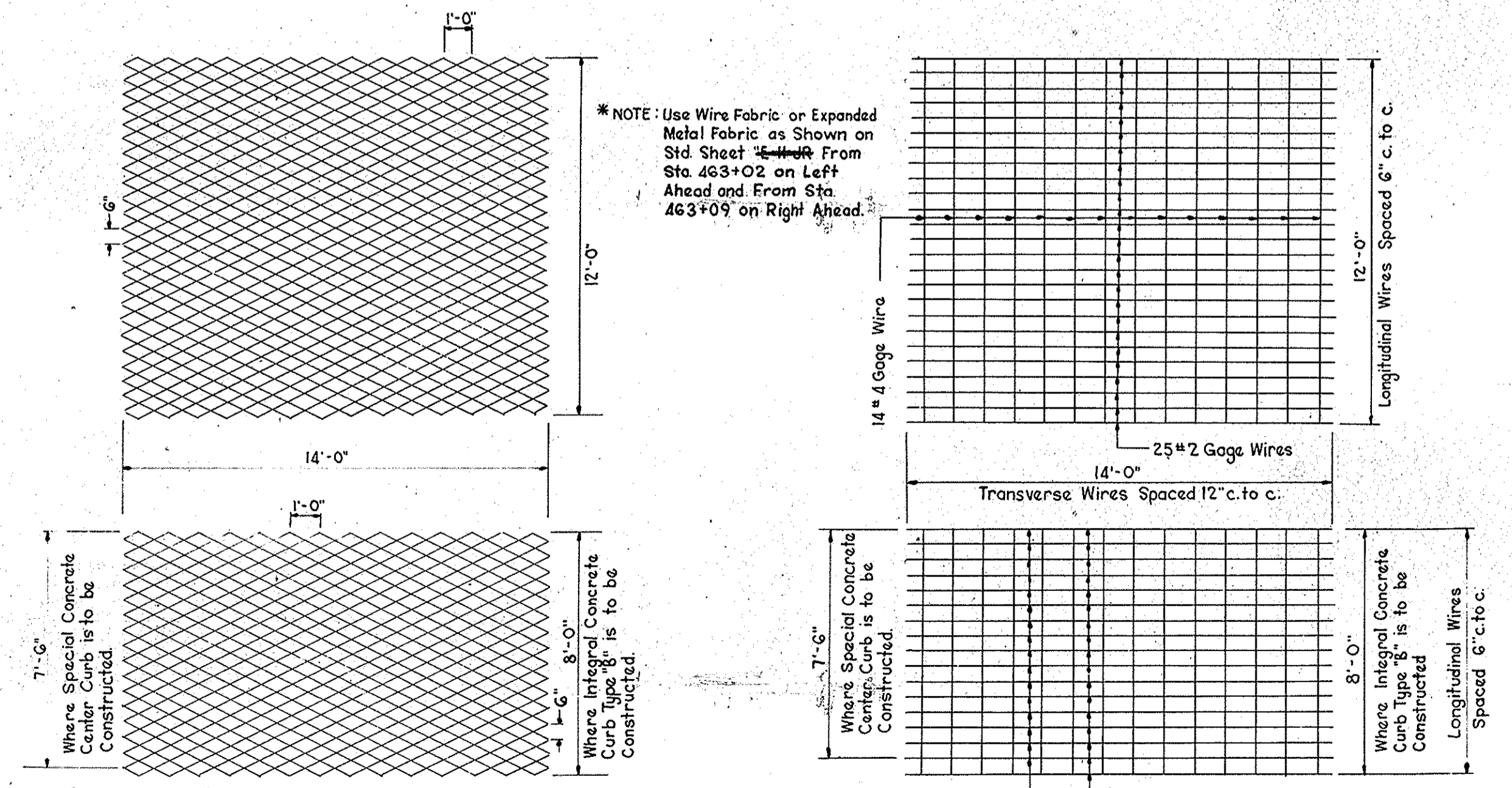


Sta. 3+80 to Sta. 459+75.39
 Line "IC-NW-AB"



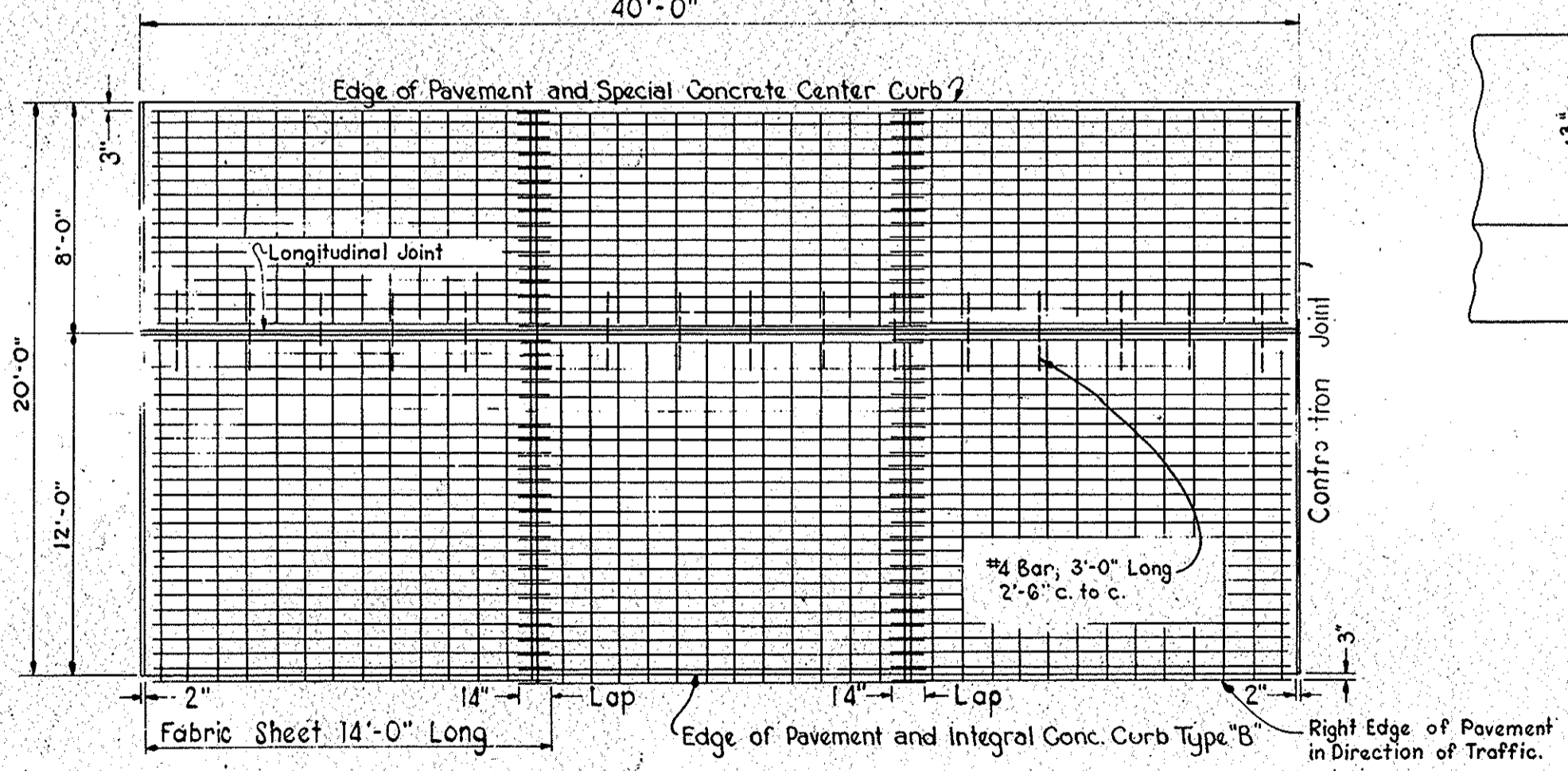
DETAIL OF SPECIAL CONCRETE CENTER CURB
 Scale: 1/2" = 1'-0"

Note: For Additional Details see Sheets 22, 23, 24, 25, 26 & 27



TYPICAL SHEET EXPANDED METAL FABRIC
 Scale: 1/4" = 1'-0"

TYPICAL SHEET WIRE FABRIC
 Scale: 1/4" = 1'-0"



PLAN OF REINFORCED CONCRETE SLAB
 Scale: 3/16" = 1'-0"

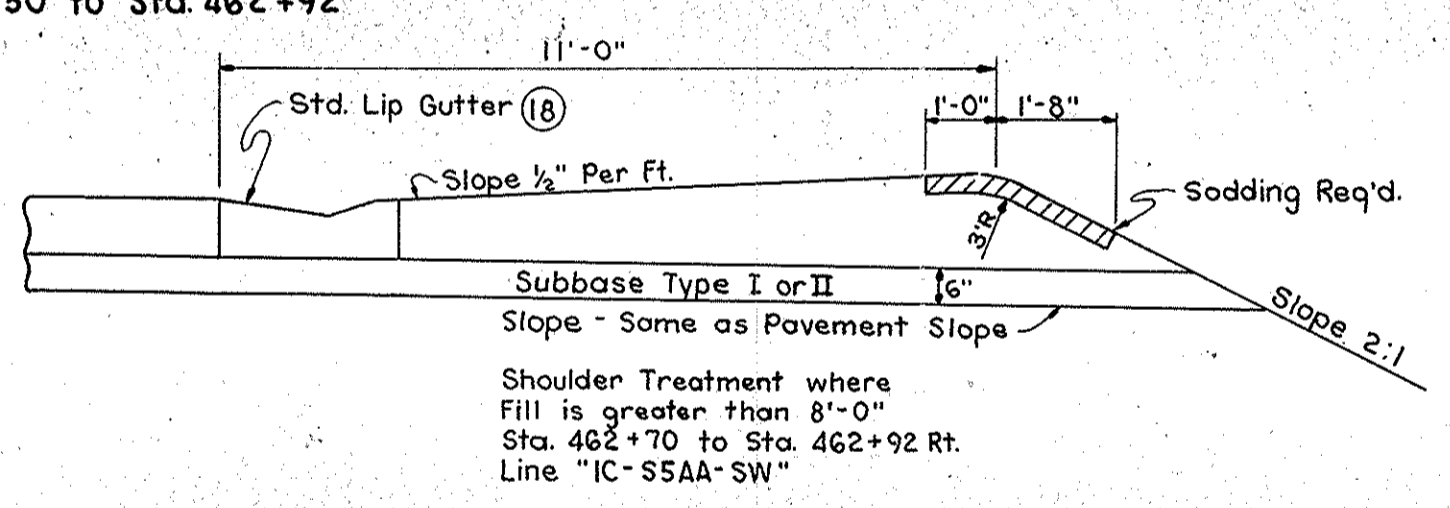
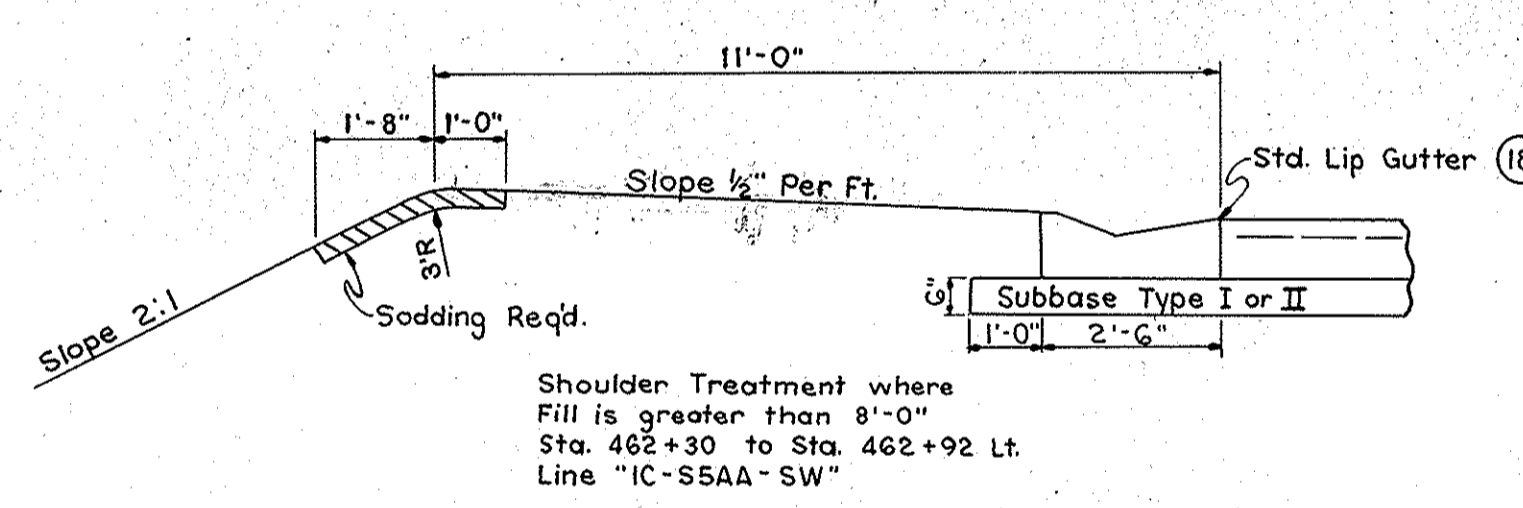
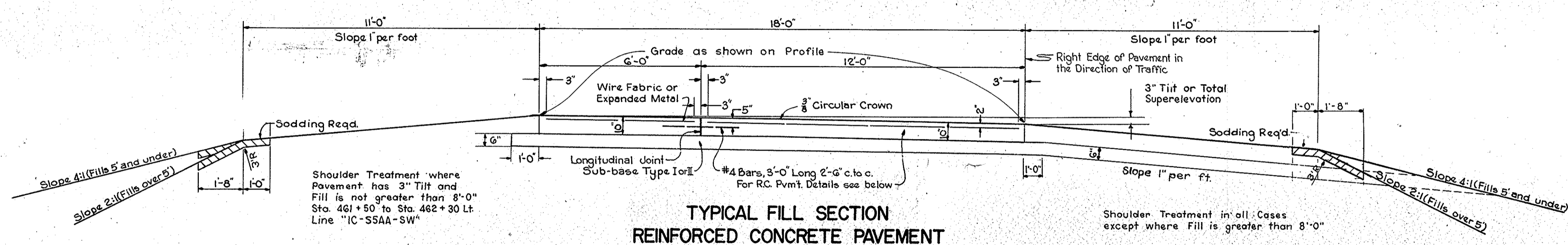
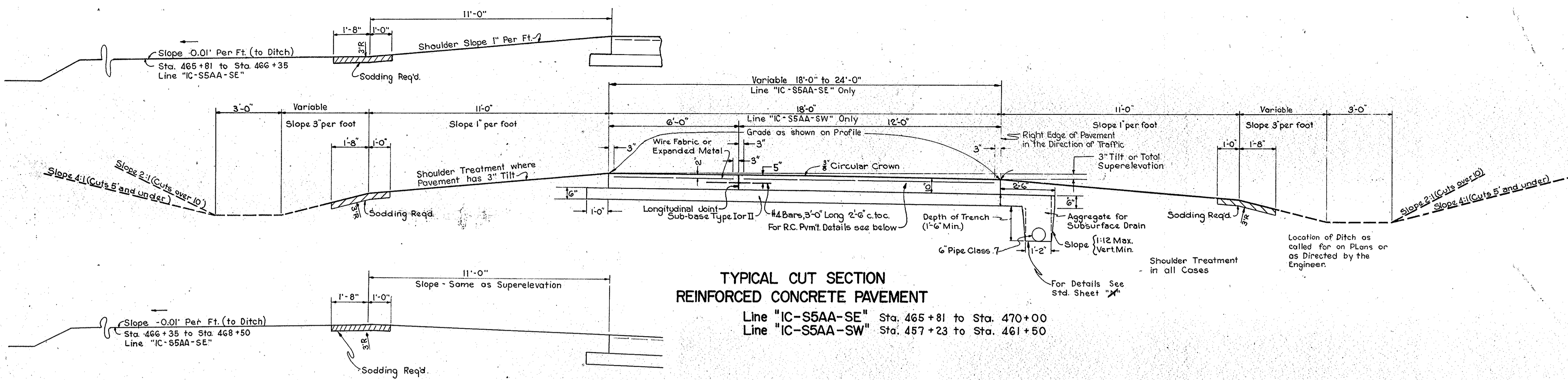
TYPICAL CROSS SECTIONS

SCALE: 3/8" = 1'-0" except as noted.

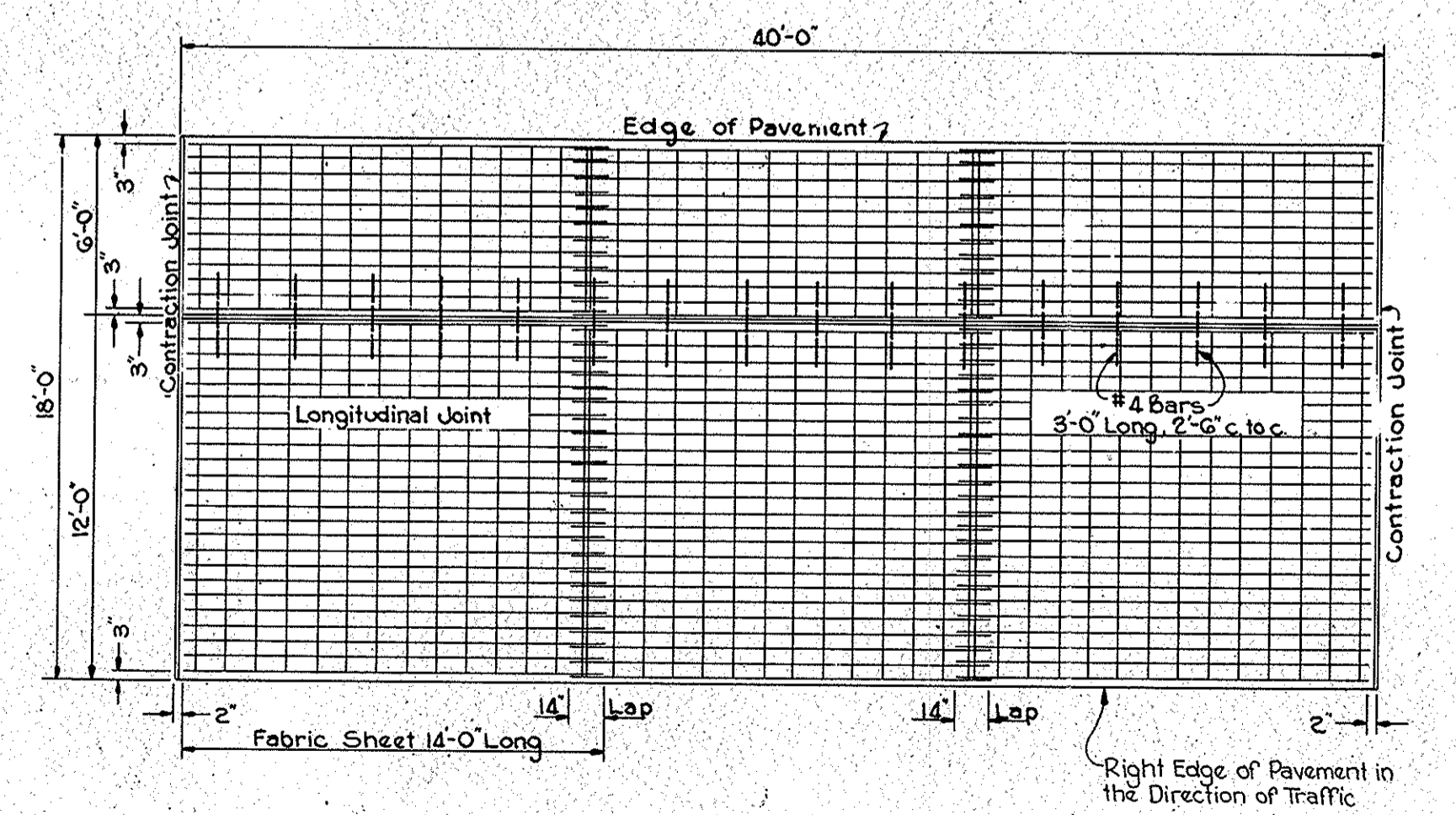
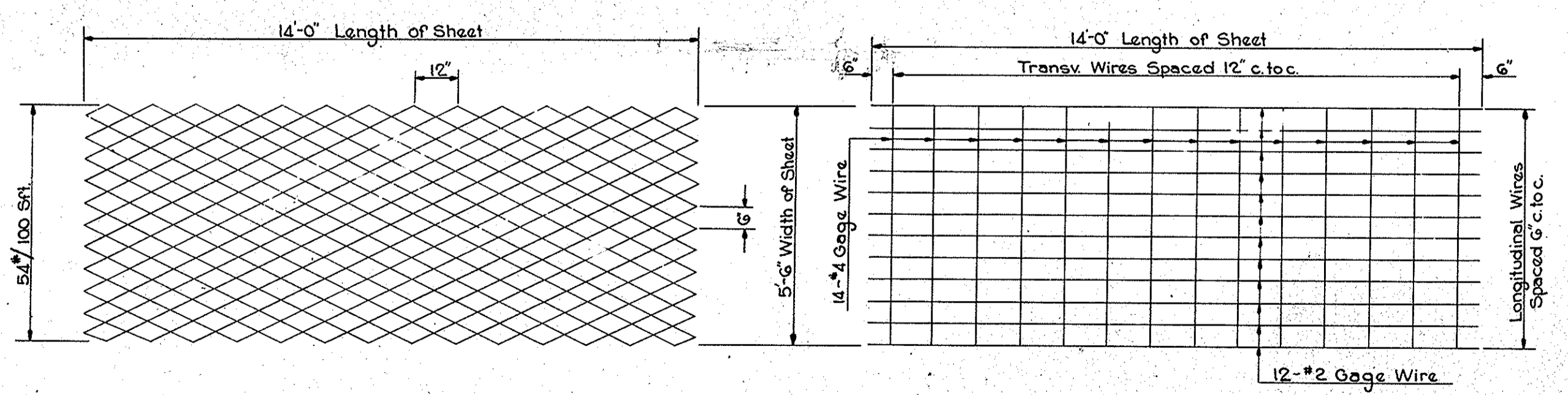
APPROVED *[Signature]*
 CHIEF ENGINEER - STATE HIGHWAY COMMISSION OF INDIANA

RECOMMENDED FOR APPROVAL 11-25-58
[Signature]
 ENGINEER OF ROAD DESIGN, STATE HIGHWAY COMMISSION OF INDIANA

FEDERAL ROAD DIVISION NO	STATE	PROJ. NO	FISCAL YEAR	SHEET NO	TOTAL SHEETS
4	IND.	I-74-2(5)64	1959	6	35



FOR DETAILS OF WIRE FABRIC AND EXPANDED METAL FABRIC SHEETS FOR 12' PAVEMENT WIDTH SEE STD. SHEET 22-23-24-28-29-30-31



Note: For Additional Details see Sheets 22-23-24-28-29-30-31

SCALE: 3/8" = 1'-0" except as noted.

TYPICAL CROSS SECTIONS

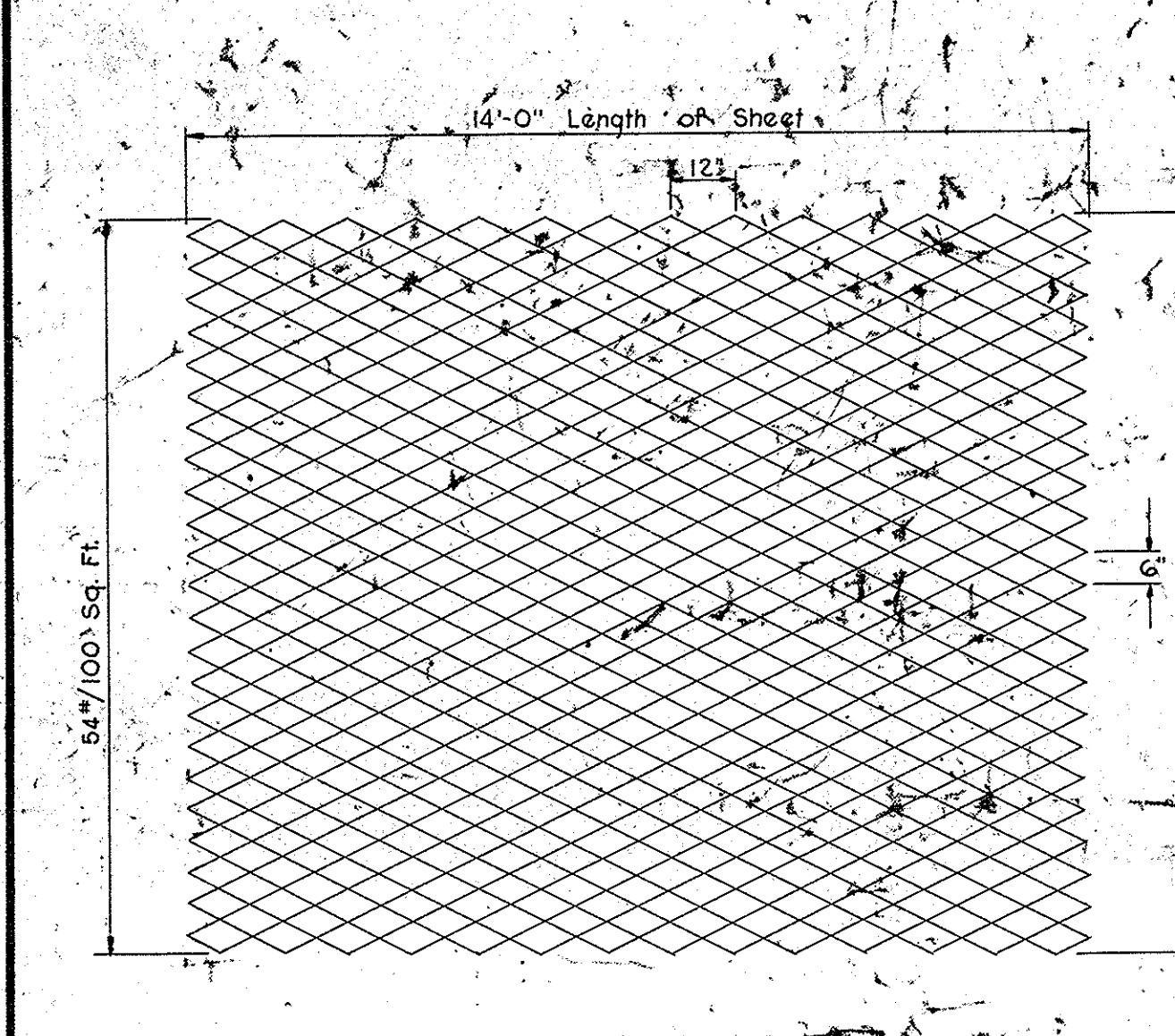
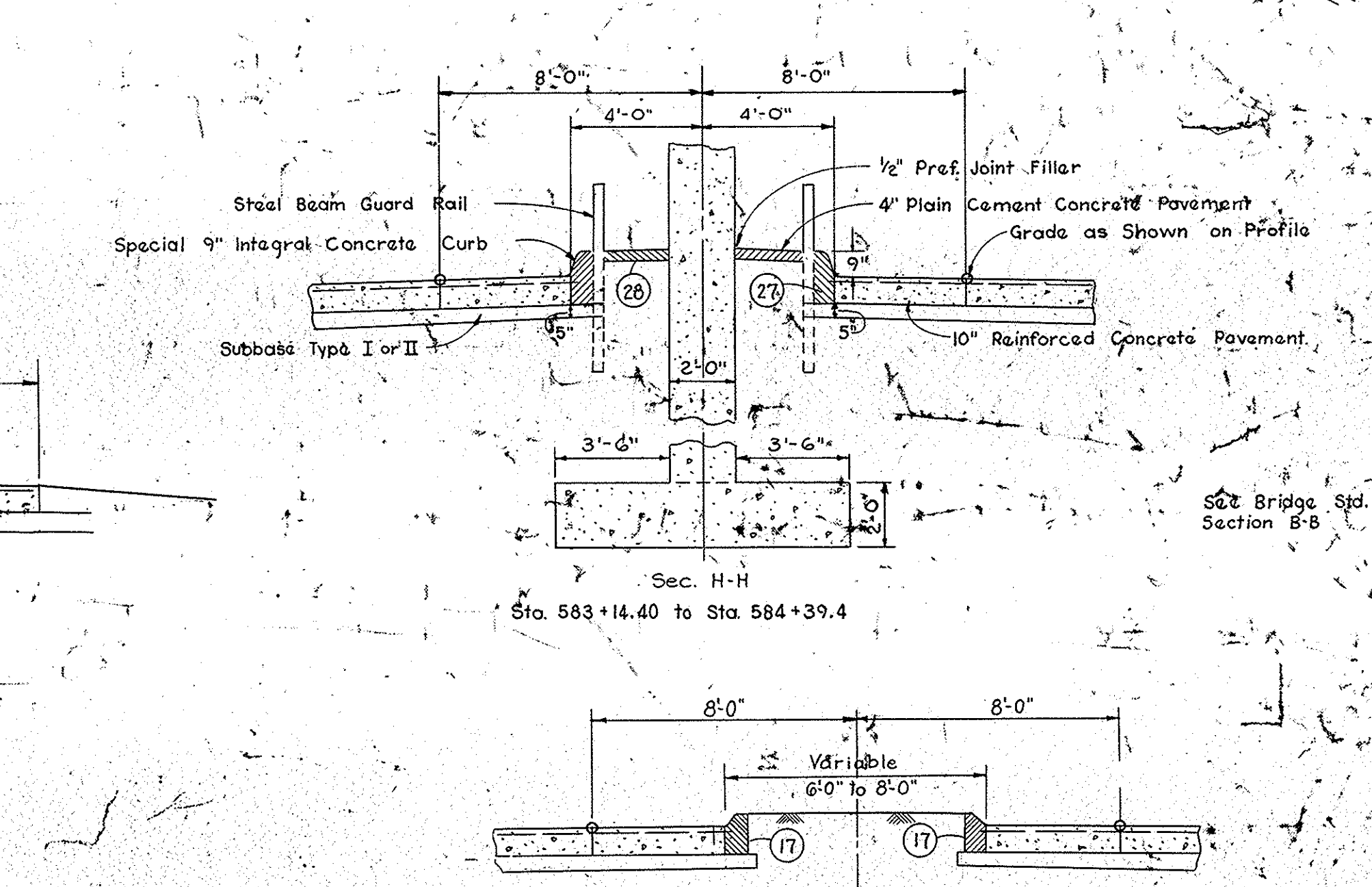
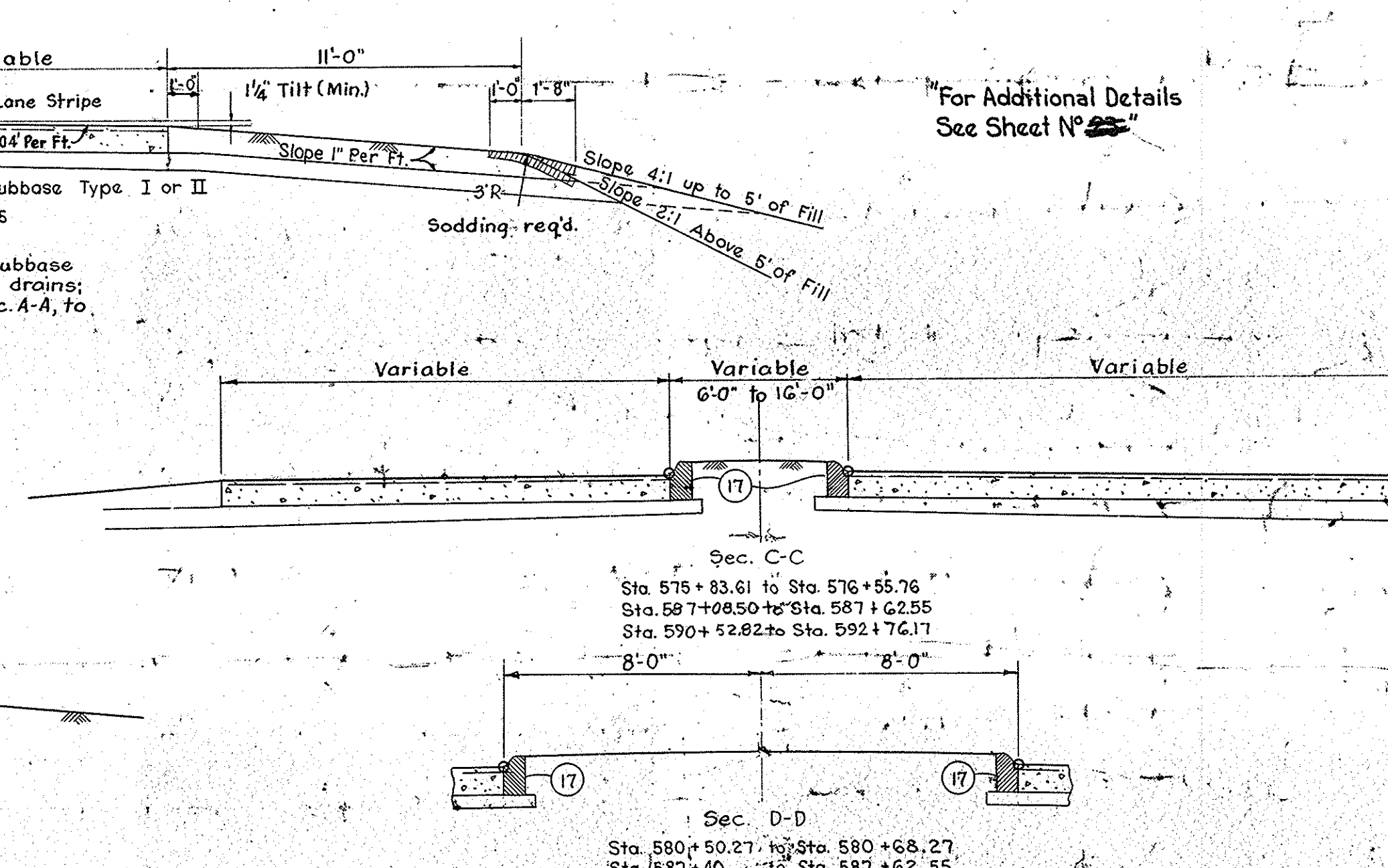
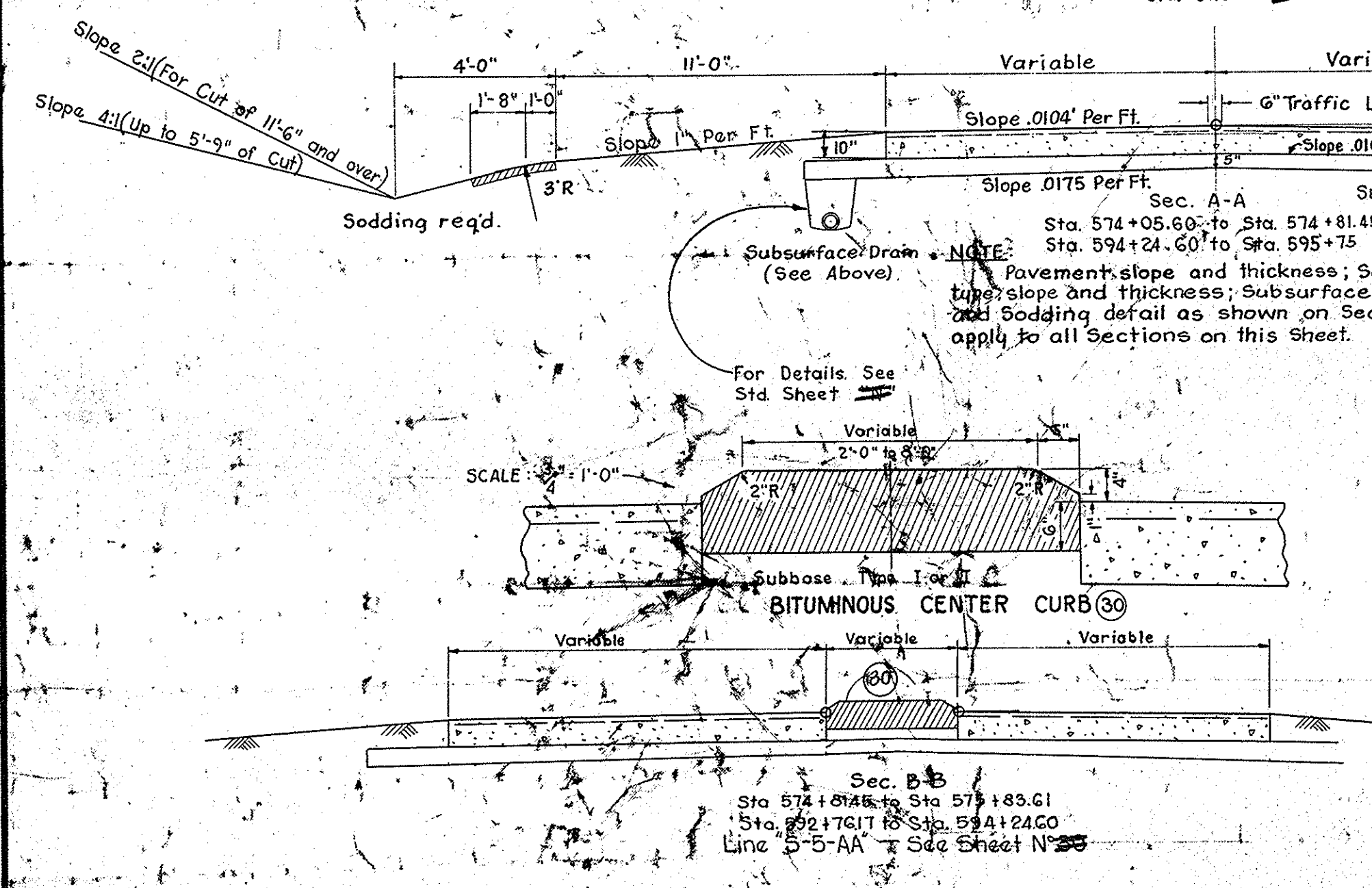
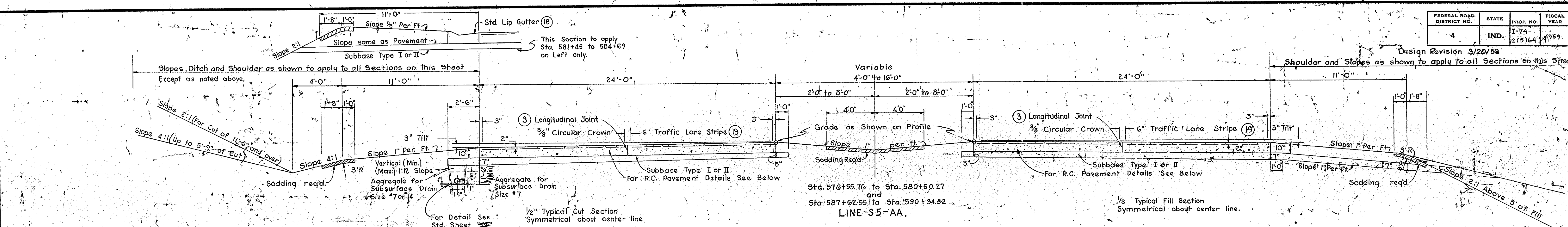
APPROVED *[Signature]*
CHIEF ENGINEER STATE HIGHWAY COMMISSION OF INDIANA

APPROVED *[Signature]*
CHIEF ENGINEER STATE HIGHWAY COMMISSION OF INDIANA

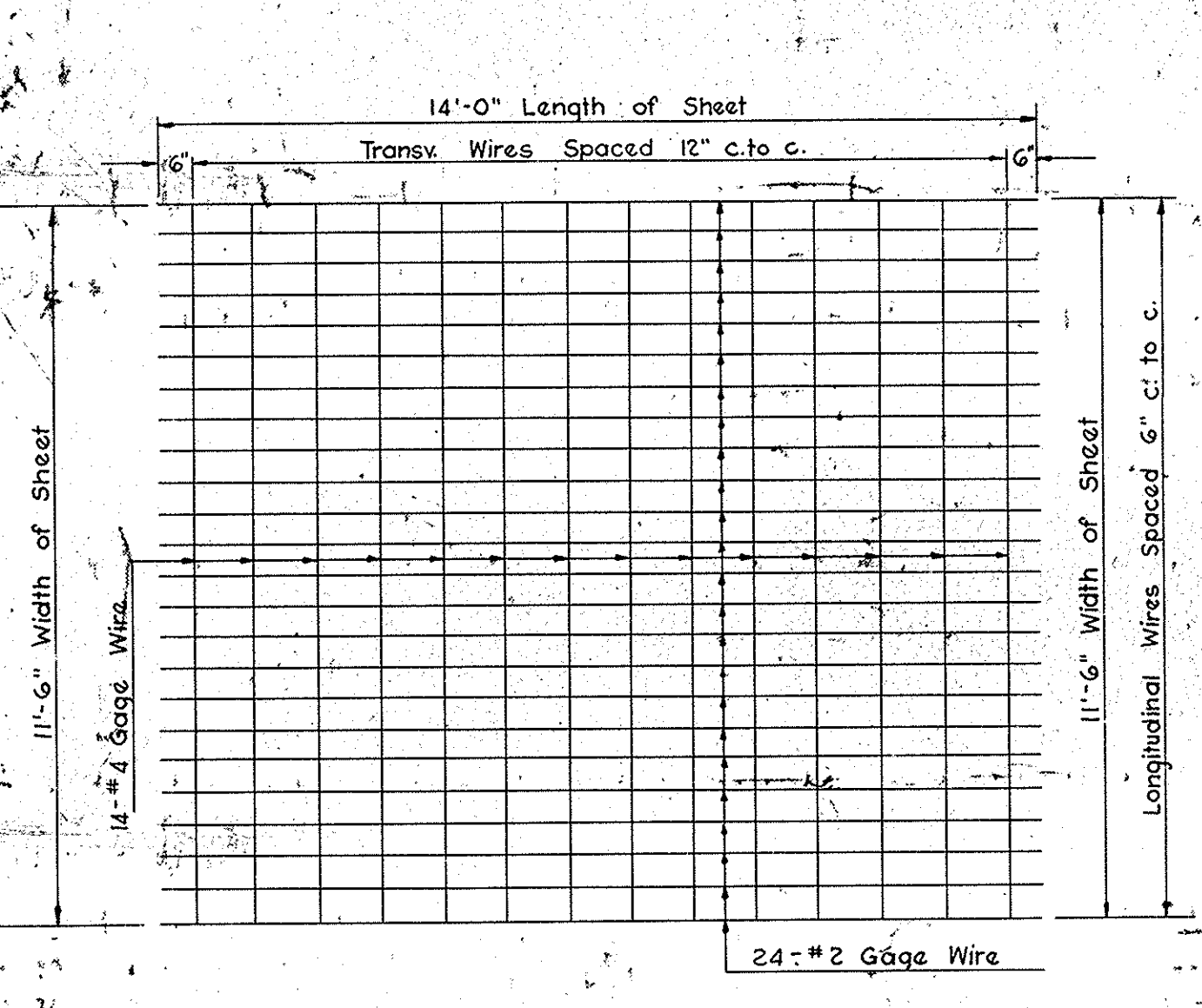
RECOMMENDED FOR APPROVAL 11-26-58
[Signature]
ENGINEER OF ROAD DESIGN STATE HIGHWAY COMMISSION OF INDIANA

FEDERAL ROAD DISTRICT NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74 2(5)6A	1959	7	35

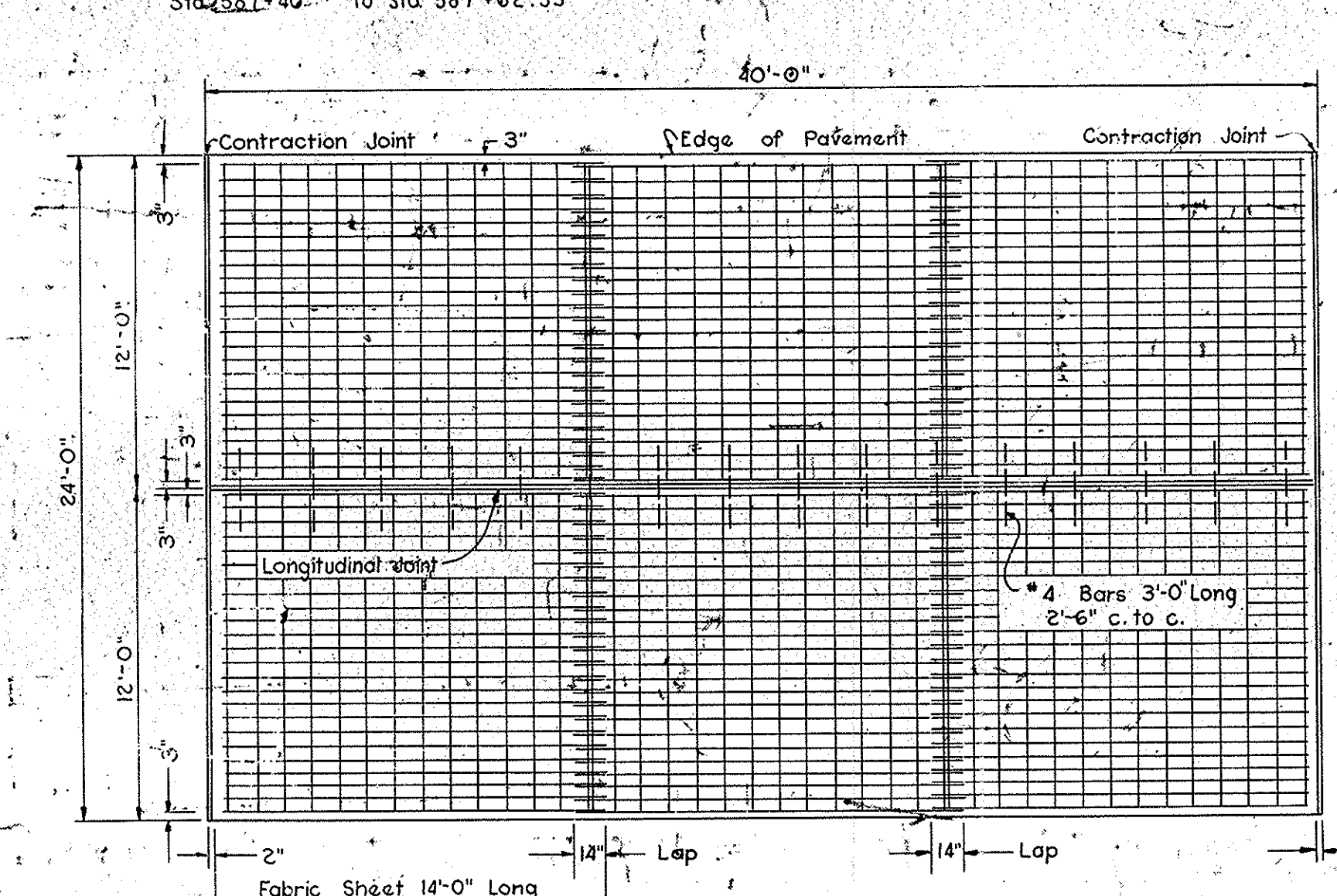
Design Revision 3/20/53



TYPICAL SHEET
EXPANDED METAL FABRIC
Scale: 3/8" = 1'-0"



TYPICAL SHEET
WIRE FABRIC
Scale: 3/8" = 1'-0"



PLAN OF REINFORCED CONCRETE SLAB
Scale: 3/16" = 1'-0"

- LEGEND**
- o Grade as shown on Profile
 - (17) Integral Concrete Curb Type "B"
 - (25) Concrete Center Curb Type "B"
 - (27) Special 9" Integral Concrete Curb
 - (28) 4" Plain Cement Concrete Pavement
 - (30) Bituminous Center Curb

NOTE
Dimensions shown as Variable on this sheet are at locations where Pavement edges are on opposite direction Curves. See Sheets 25 and 27 for Curve Data and locations of Sections designated by Letters.

TYPICAL CROSS SECTIONS

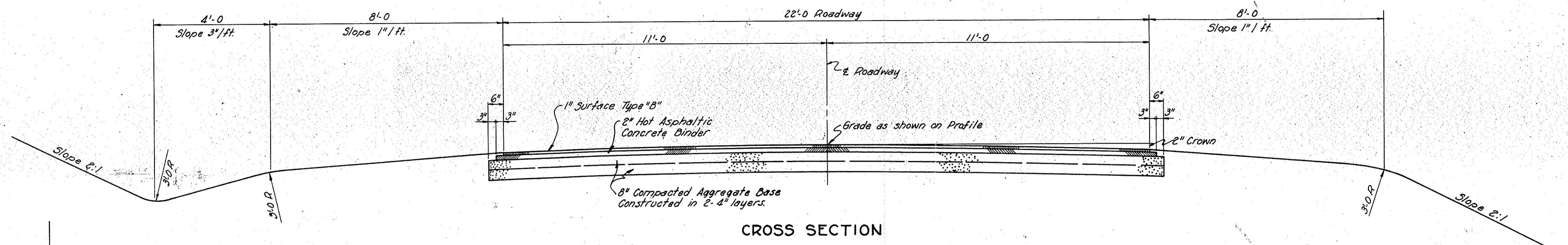
Line "S-5-AA" STATE ROUTE 267

SCALE: 1/4" = 1'-0" except as noted.

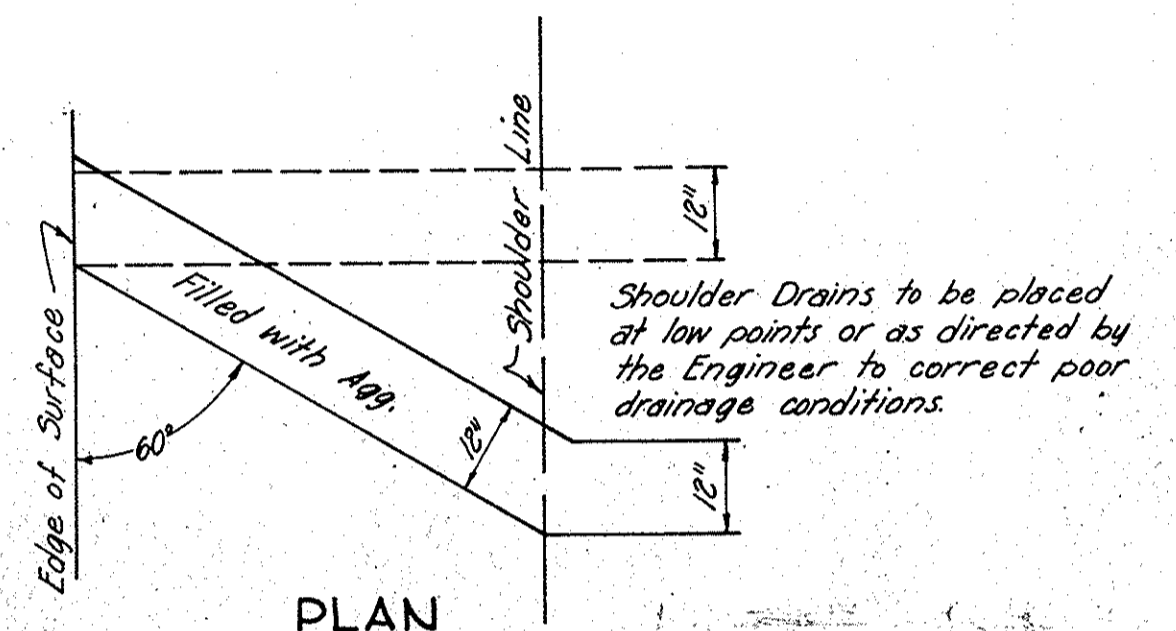
APPROVED: *C.E. VanLaningham*
CHIEF ENGINEER, STATE HIGHWAY COMMISSION OF INDIANA

RECOMMENDED FOR APPROVAL: 11-21-58
W.H. Roberts
ENGINEER OF ROAD DESIGN, STATE HIGHWAY COMMISSION OF INDIANA

BRIDGES OVER 20' SPAN					
PUB. ROAD DIV. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(5)64	1959	8	35

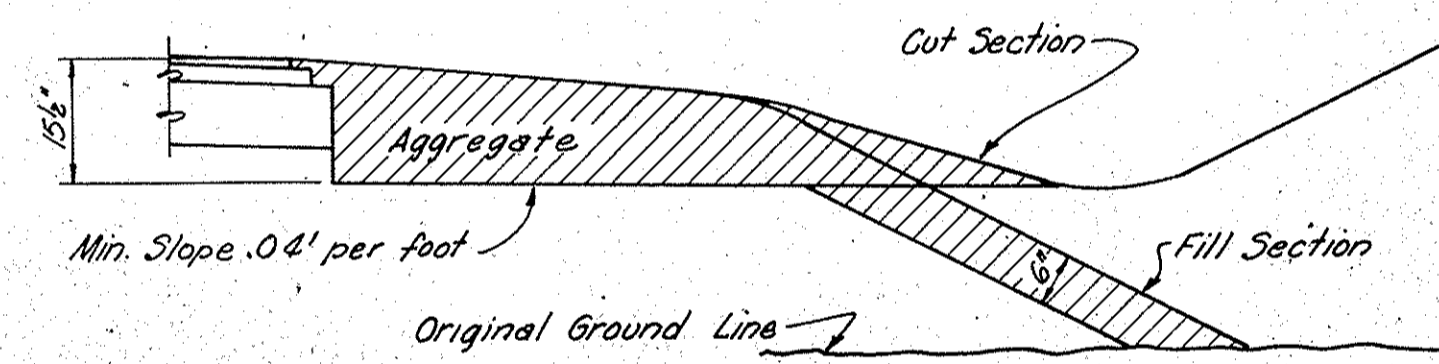


CROSS SECTION

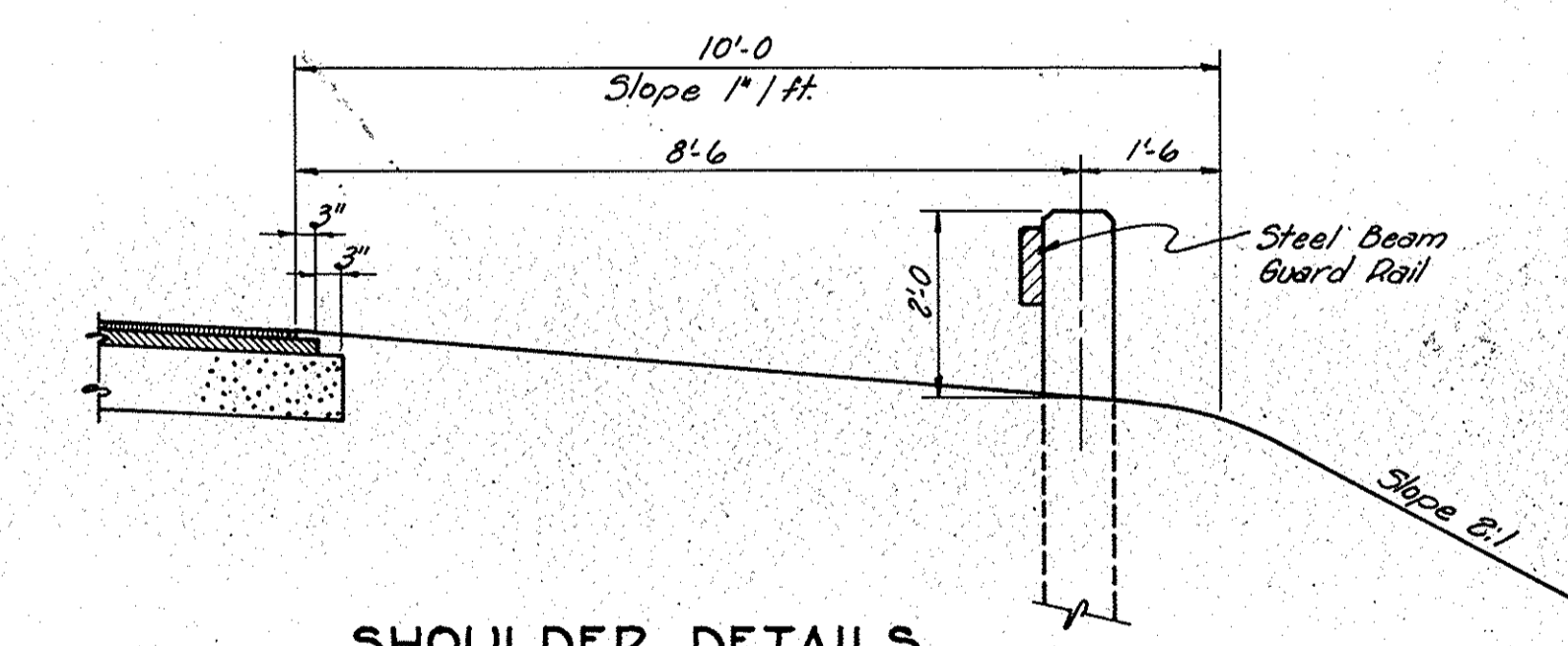
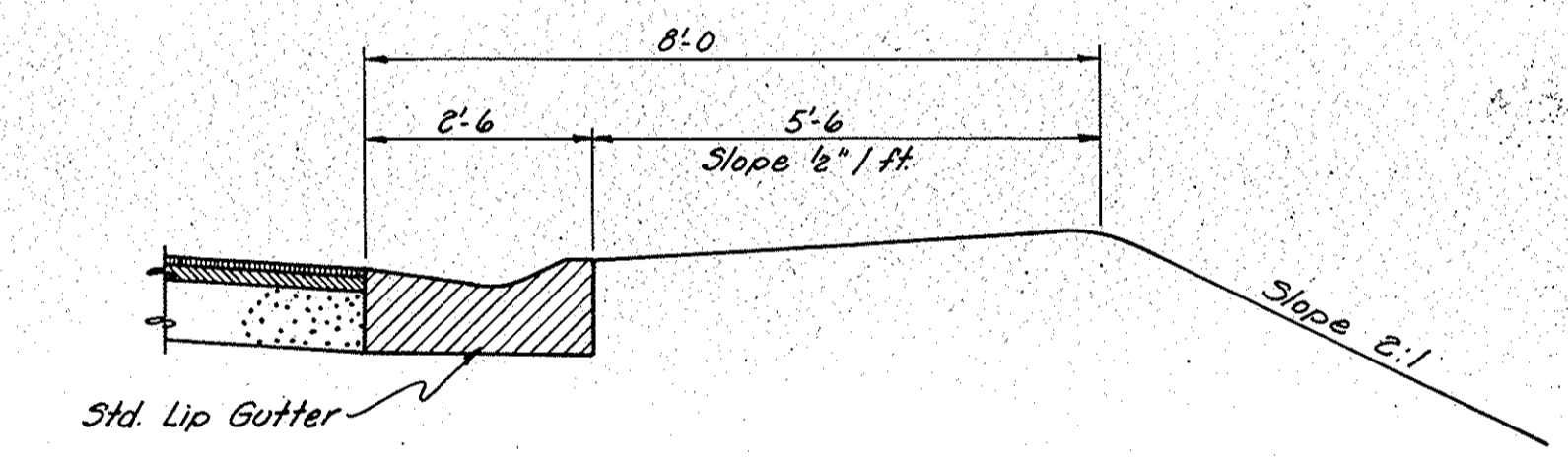
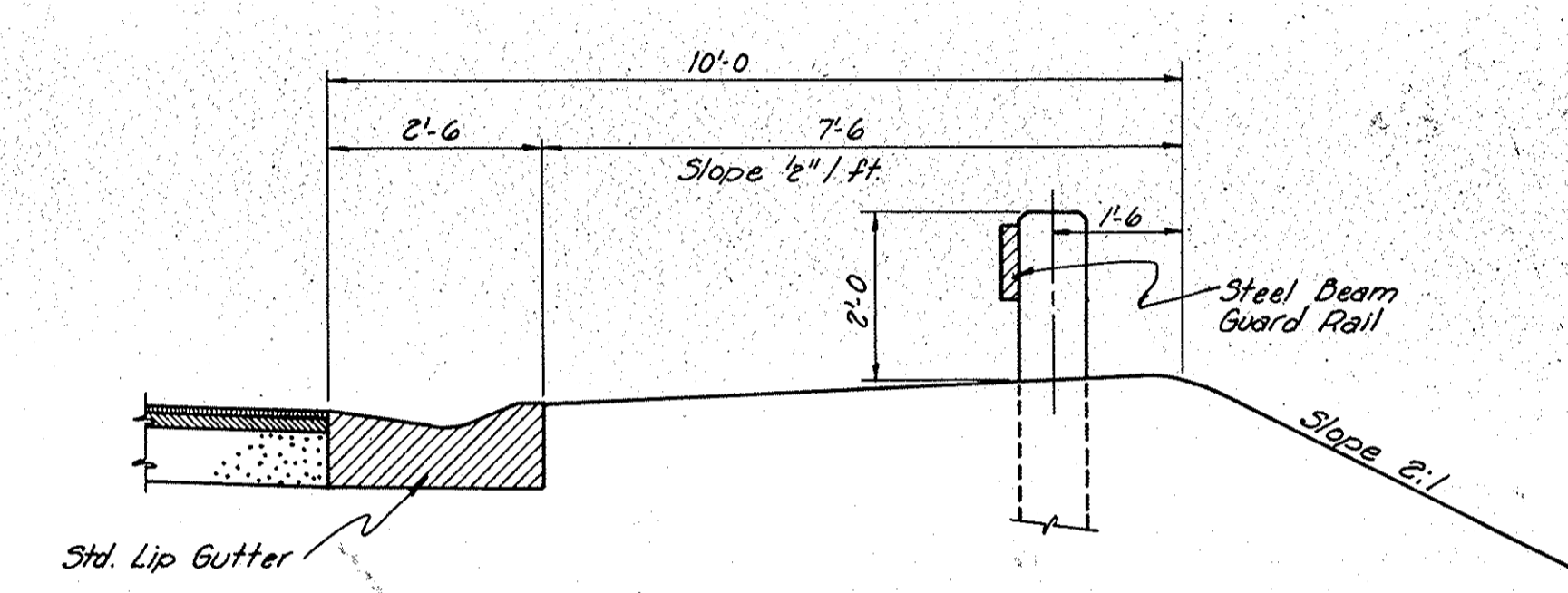


PLAN

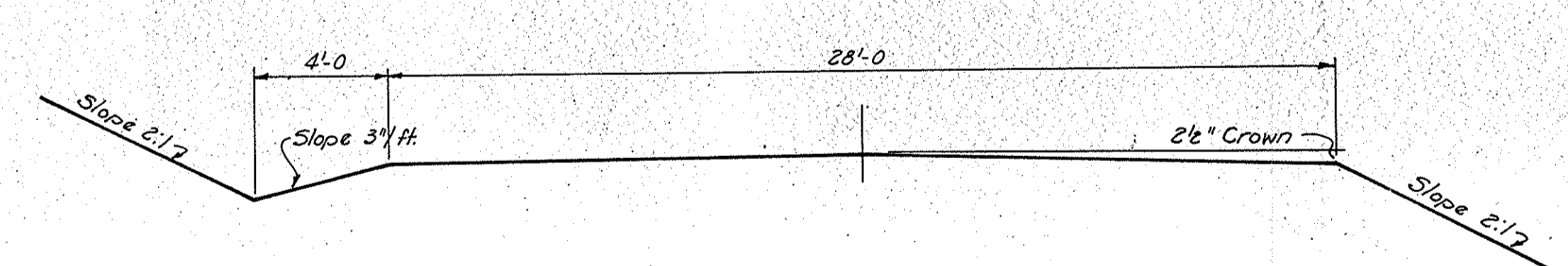
NOTE: Drains to be constructed at 60° angle on grade of 1% or over. Grades less than 1% have drains at right angles to $\frac{1}{2}$ or Road



SECTION
SHOULDER DRAINS



SHOULDER DETAILS



FRONTAGE ROAD-GRADED SECTION

Scale: 1/4" = 1'-0"

TYPICAL CROSS SECTION

SCALE: 1/2" = 1'-0"

FEBRUARY 16, 1959

Walter M. Beem

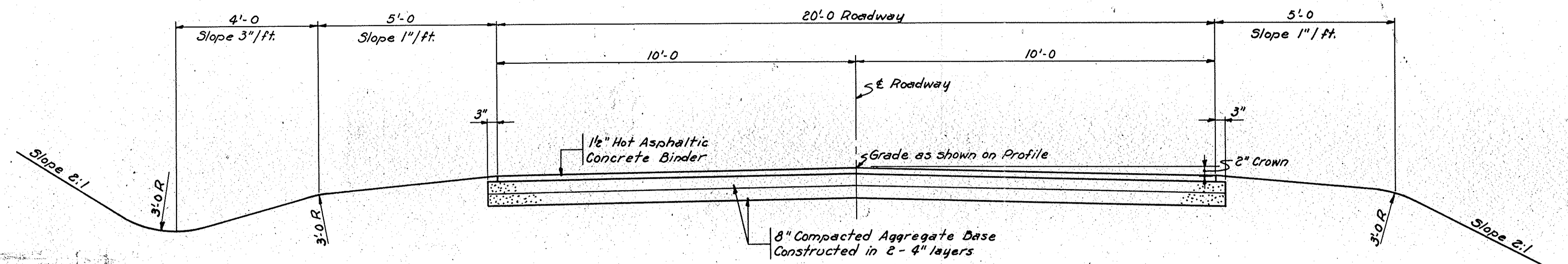
PROJECT: I-74-2 (18) BRIDGE
BRIDGE CONTRACT NO. 4640
BRIDGE FILE: 156-69-4434



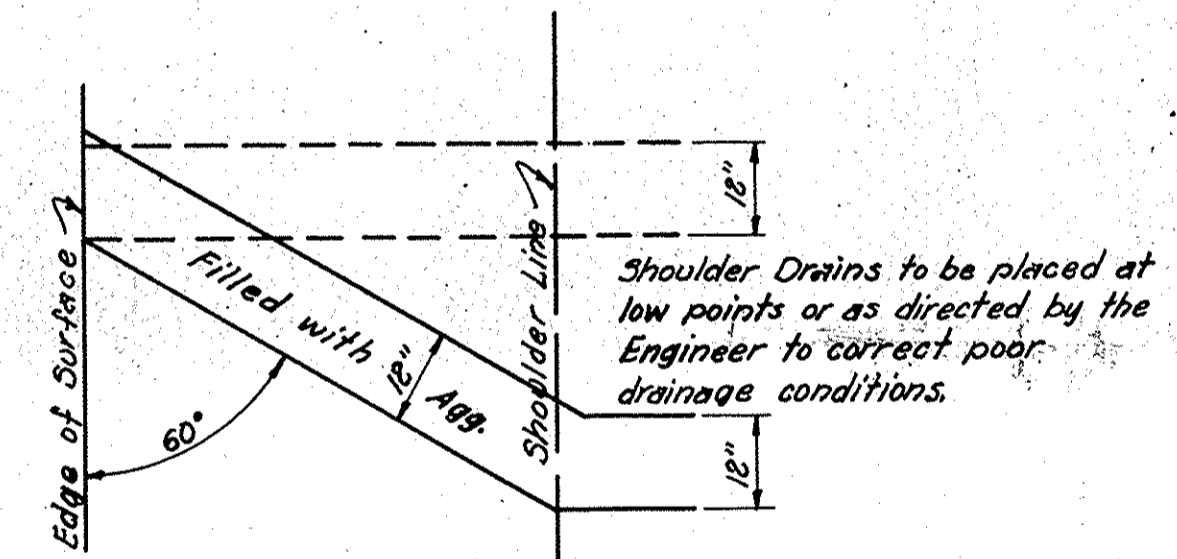
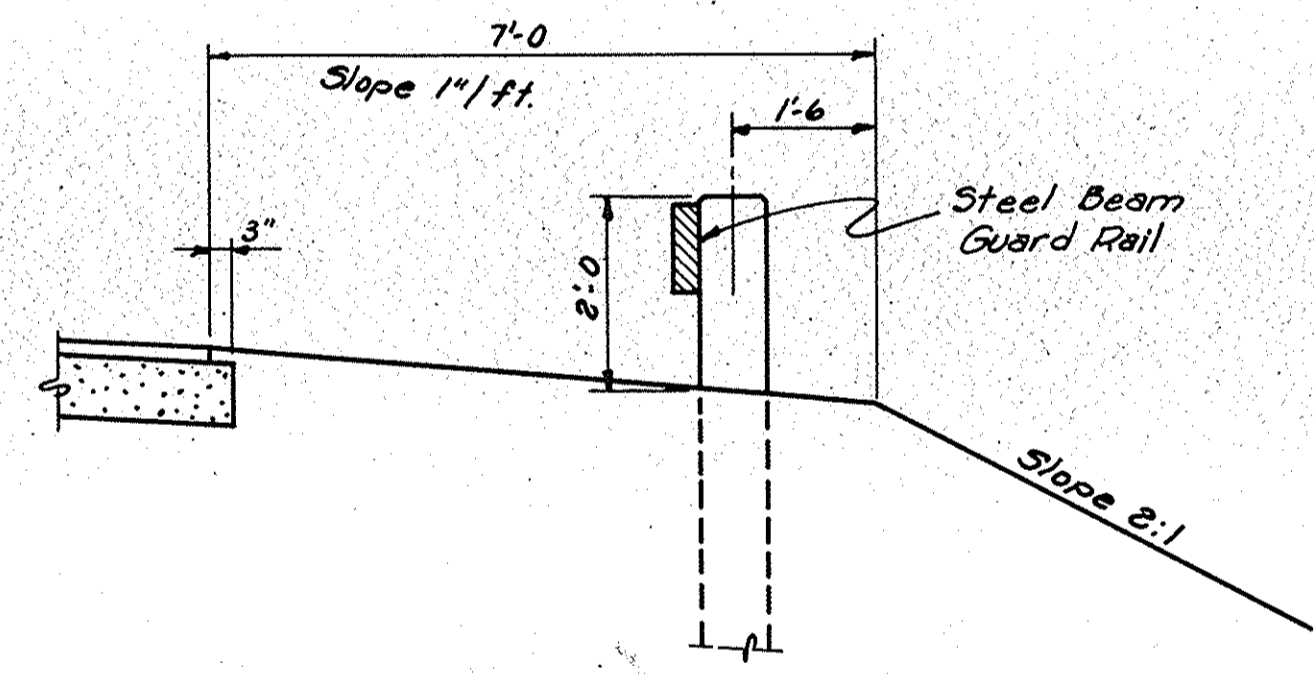
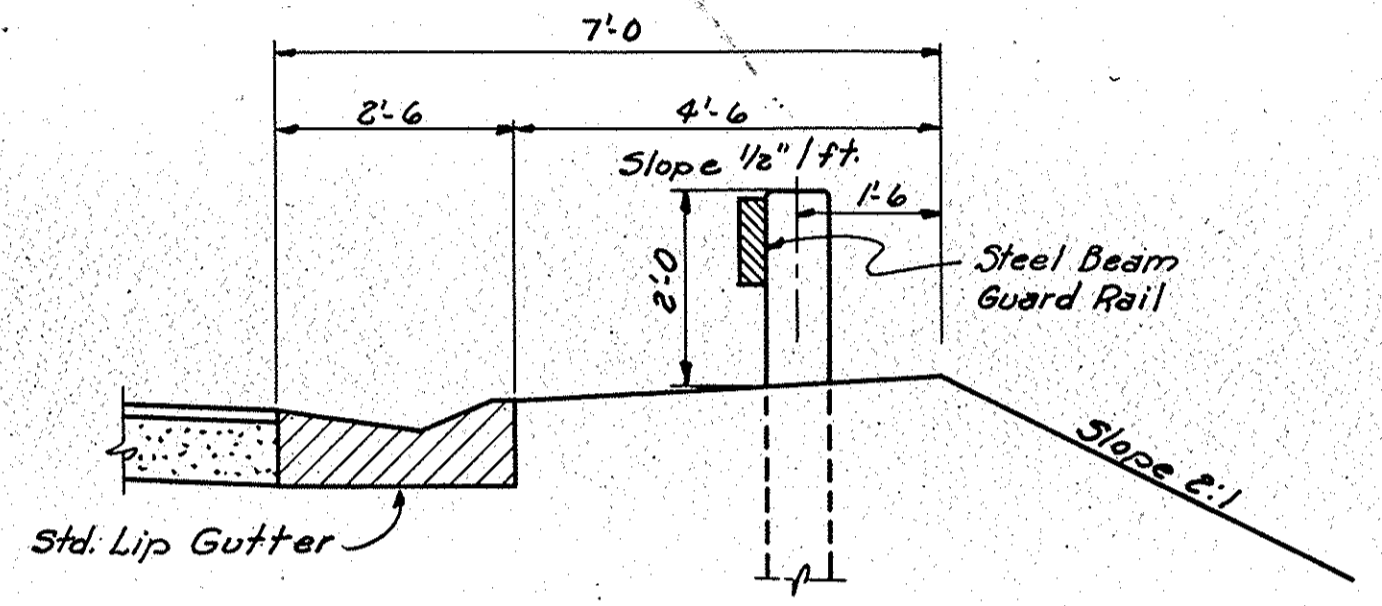
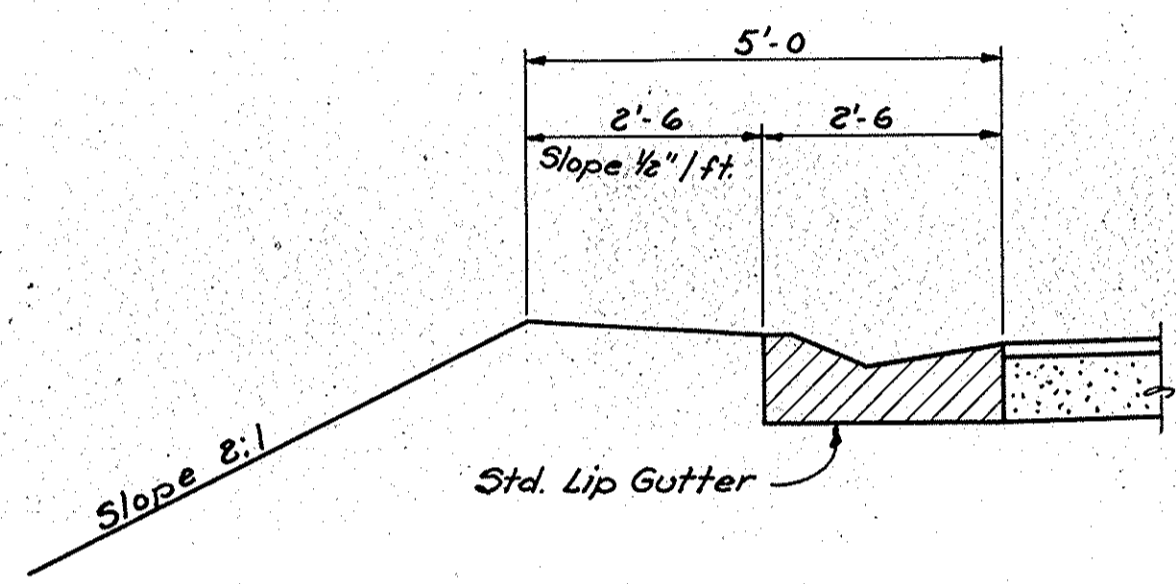
DESIGNED	C.K.D.
DRAWN	C.K.D.
TRACED	C.K.D.

I-74-66

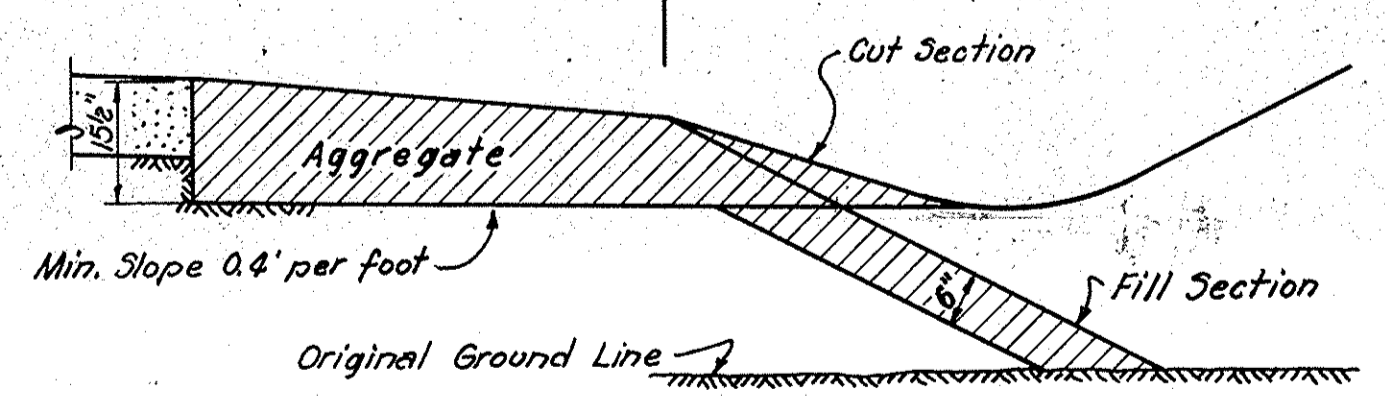
BRIDGES OVER 20' SPAN					
PUB. ROAD REG. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(15)64	1959	9	35



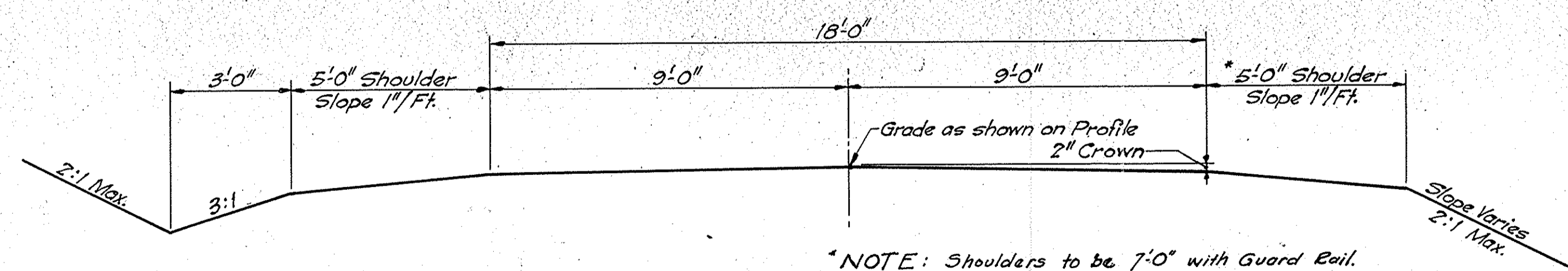
CROSS SECTION



NOTE: Drains to be constructed at 60° angle on grade of 1% or over. Grades less than 1% have drains at right angles to \pm of Road.



SECTION SHOULDER DRAINS



TYPICAL SECTION-18' GRADED FRONTAGE ROAD
Scale: 3/8" = 1'-0"

*NOTE: Shoulders to be 7'-0" with Guard Rail.

TYPICAL CROSS SECTION
STATE HIGHWAY DEPARTMENT OF INDIANA

SCALE: 1/2" = 1'-0"

AUGUST 5, 1958
Walter M. Beam

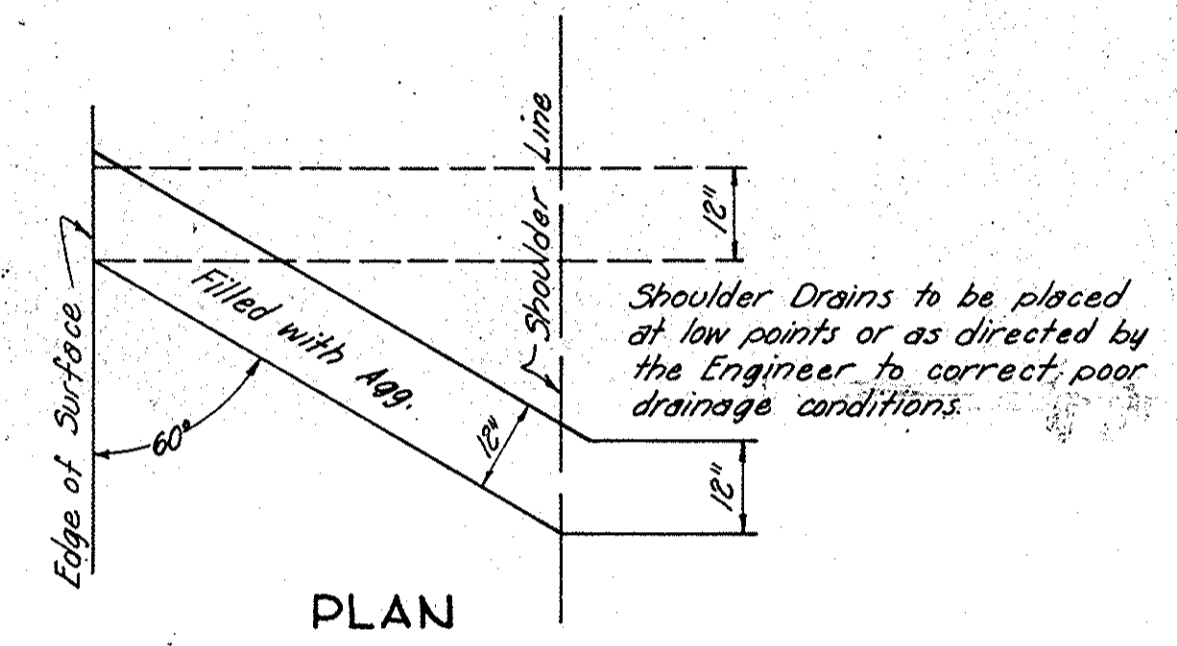
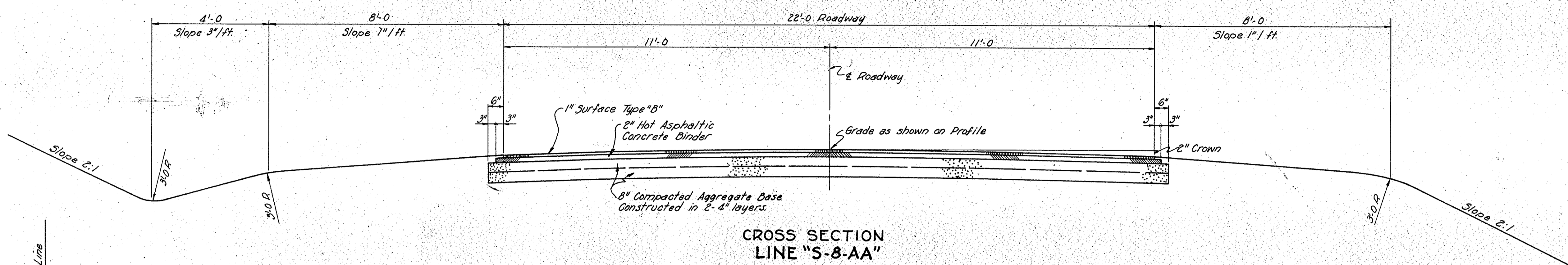
PROJECT: I-74-2 (19) BRIDGE
BRIDGE CONTRACT NO. 4641
BRIDGE FILE: 136-69-4435



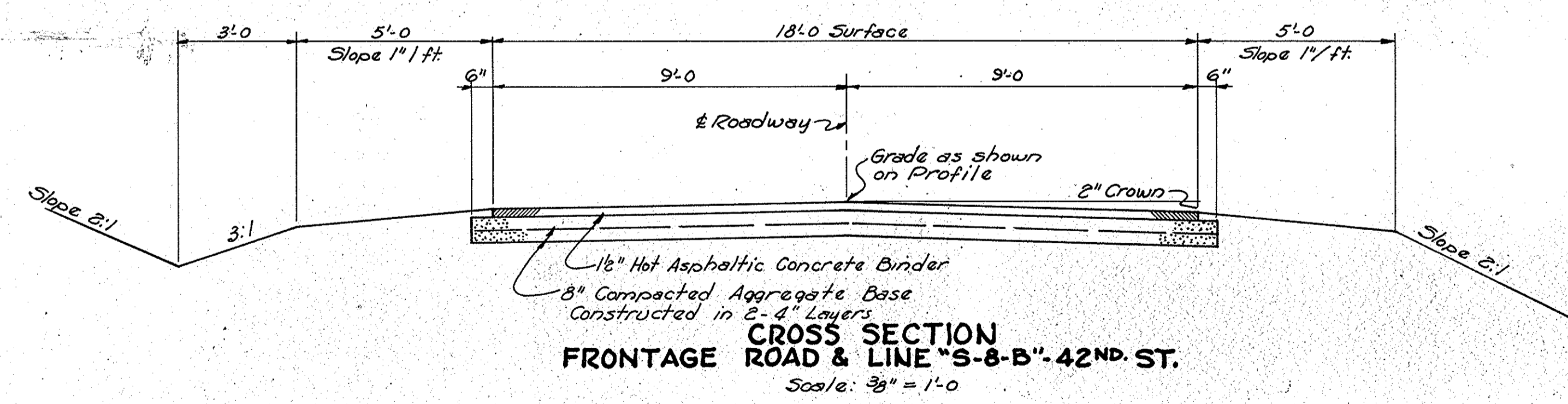
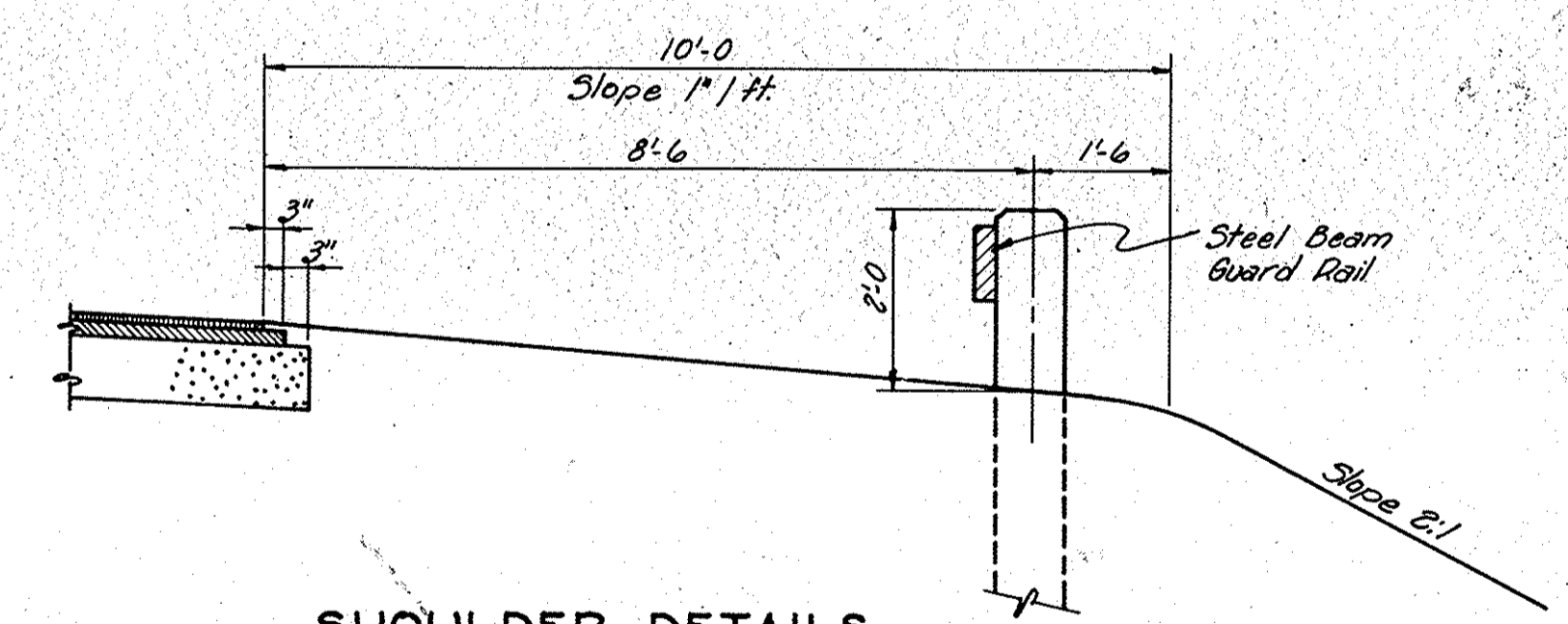
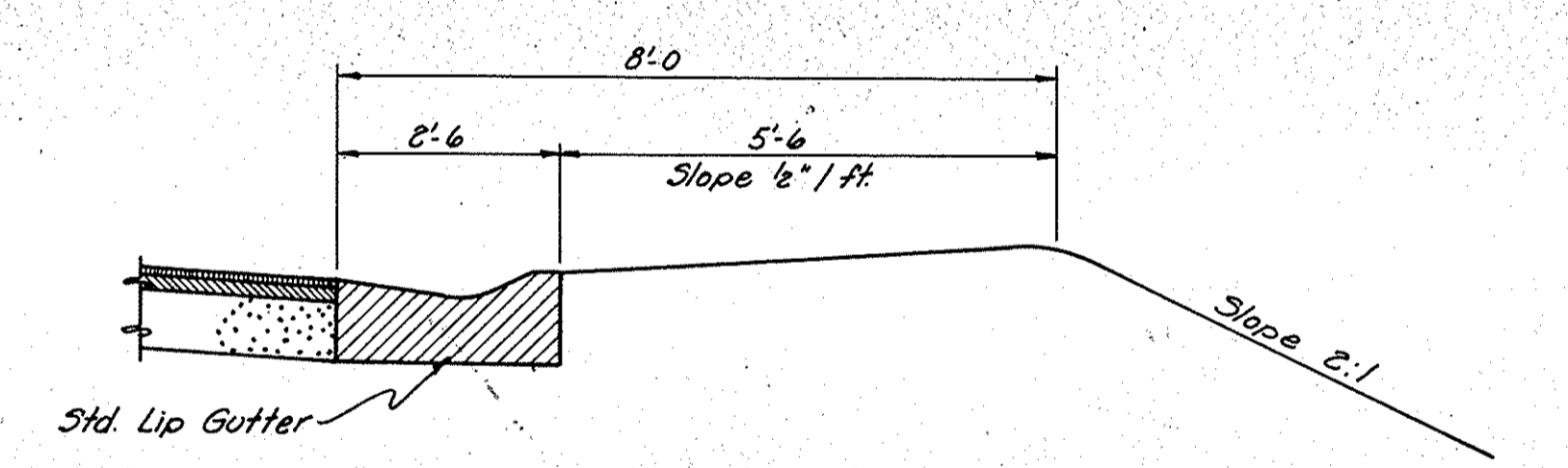
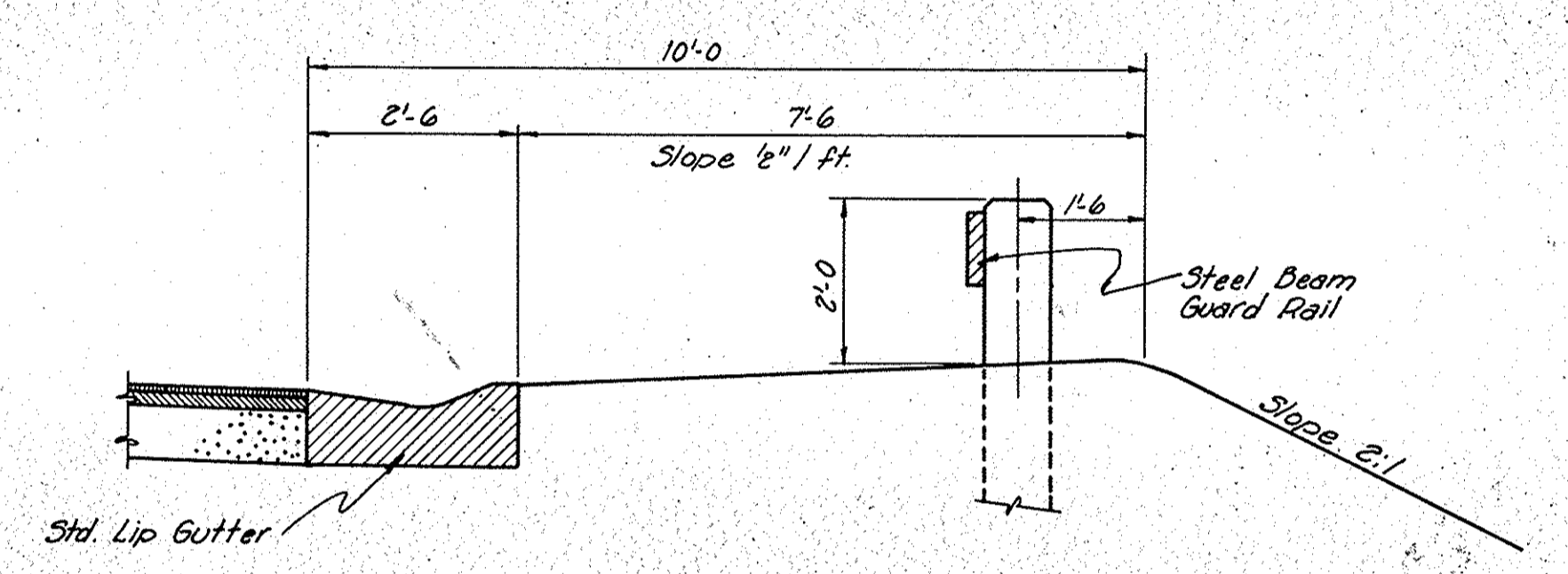
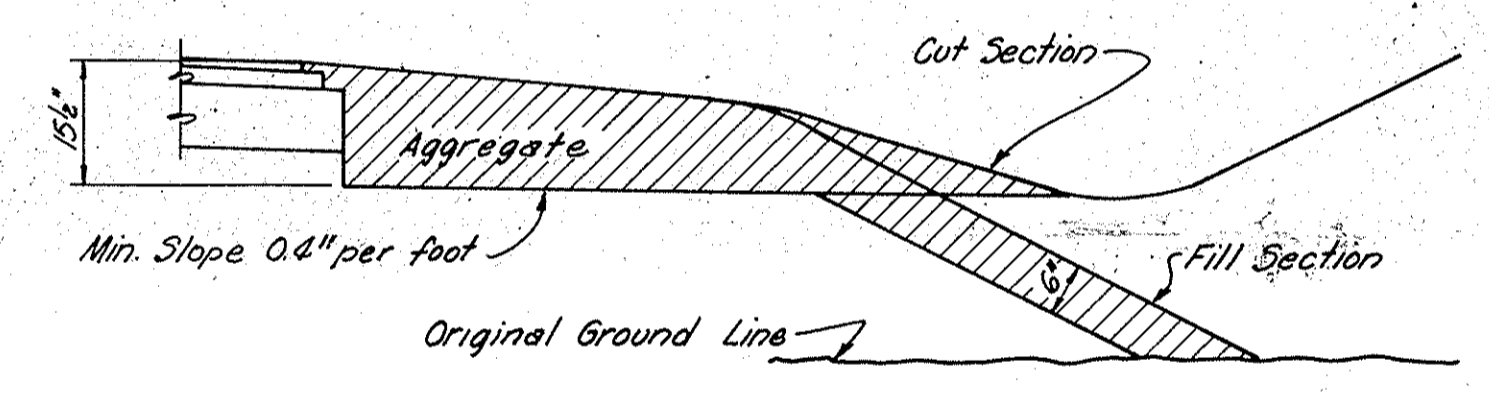
DESIGNED	CKD
DRAWN	CKD
TRACED	CKD

I-74-68

BRIDGES OVER 20' SPAN					
PUB. ROAD RES. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2 (21) 64	1959	10	35



NOTE: Drains to be constructed at 60° angle on grade of 1% or over. Grades less than 1% have drains at right angles to & of Road.



TYPICAL CROSS SECTION

SCALE: 1/2" = 1'-0" Unless Noted
AUGUST 5, 1958

Walter M. Beam

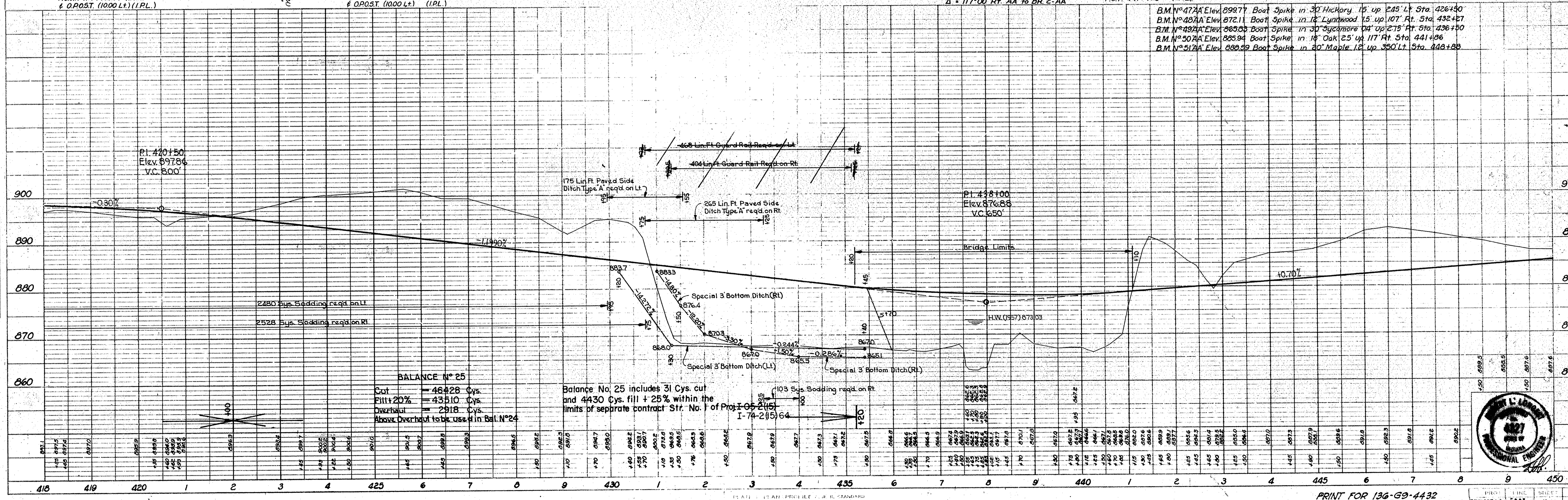
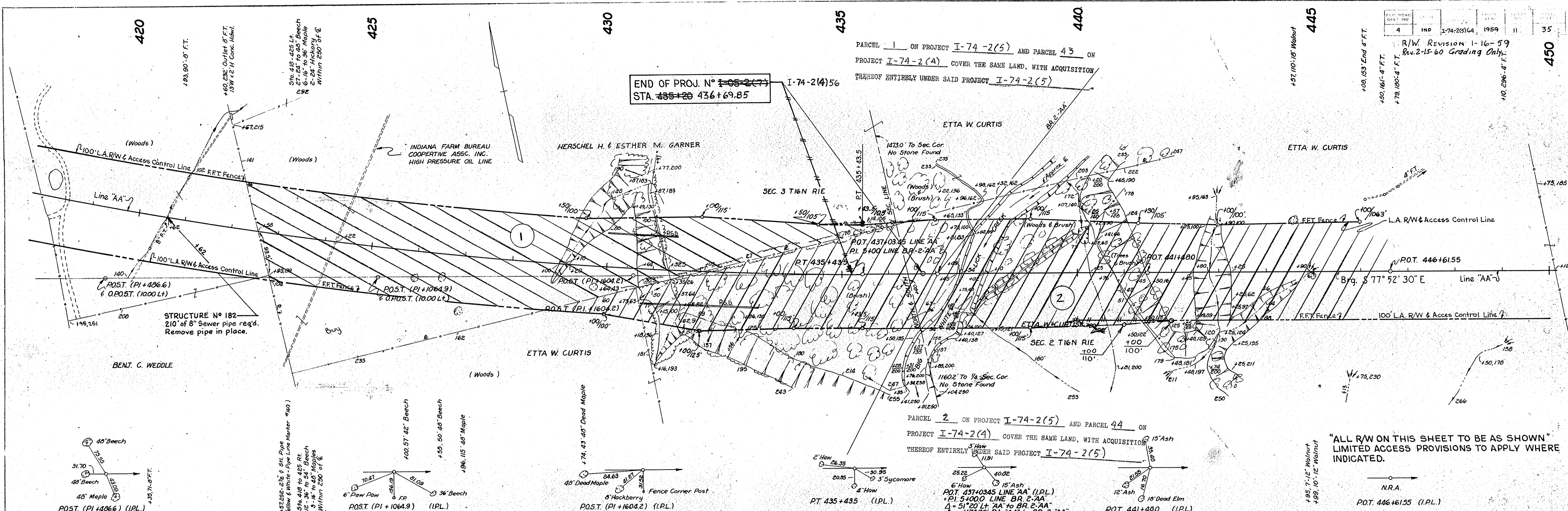
DESIGNED	C'K'D
DRAWN	C'K'D
TRACED	C'K'D

PROJECT: I-74-2 (21) BRIDGE
BRIDGE CONTRACT NO. 4659
BRIDGE FILE: 436-69-4436



PLAN
 DRAWN BY: Pierce Gruber & Beam, Inc.
 CHECKED BY: P. G. BEAM
 DATE: 1-16-59
 PROJECT: I-74-2(5)

PROFILE
 DRAWN BY: Pierce Gruber & Beam, Inc.
 CHECKED BY: P. G. BEAM
 DATE: 1-16-59
 PROJECT: I-74-2(5)



PRINT FOR 136-G9-4432

PROJECT: I-74-2(5) AA
 SHEET: 11
 DATE: 1-16-59

STANDARD DIVIDED LANE SECTIONS FOR FEDERAL AID INTERSTATE PROJECTS
 Rev. 11-26-57 And STANDARD PAVEMENT SECTIONS E-11-JR Adopted Dec. 1956
 TO BE USED WITH THIS SET OF PLANS.
 TYPICAL CROSS SECTIONS AS SHOWN ON SHEETS 4,5,6,7 TO BE USED
 WITH THIS SET OF PLANS.

State Highway Department of Indiana Standard Specifications dated 1957 to be
 used with these plans.
 Standards, under dates as listed in index on Title Sheet to be used on this project.
 Grade line as shown on profile represents top of finished surface.
 All ditches of 1% and over shall be sodded except where ditch is in rock cut or
 where Paved Side Ditch is to be constructed. (See Detail on Sheet 35).
 All shoulders, cut and fill slopes shall be plain or mulch seeded except where
 sodding is specified.
 Shoulders are to be sodded as shown on Miscellaneous Standard Sheet "B".
 Sodding shall be placed along paved side ditch as shown on Miscellaneous Standard
 Sheet "E".

Excavation quantities as shown on plans profile sheets include estimated
 excavation for private and public approaches. (See Table on Sheet No. 19)
 Paper Relocations, Ramps and Frontage Roads to be cross-sectioned by
 Project Engineer before construction, unless covered by original survey.
 Curves shall be super-elevated as shown on these plans. Curves of 5' and over
 (except Ramps) to be widened according to Standards of 1932.
 Private Drive and Mail Box Approaches shall be constructed to a depth of 6".
 The top 3" shall consist of Bituminous Shoulder Mixture for Approaches and
 the bottom 3" shall consist of Compacted Aggregate Base. Other details
 to be same as shown on Miscellaneous Standard Sheets "B" and "H".
 Existing roadway surface to be removed by the Contractor as shown on the plans
 or as directed by the Engineer.
 For kinds of pipe permitted for each size and classification as shown in structure
 notes, see Miscellaneous Standard Sheet "P".
 Quantities for Pipe Culvert Headwalls are based on using Standard Headwalls for
 retaining 2:1 slopes and Private Drive Headwalls for retaining 4:1 slopes.
 All Limited Access Right-of-Way (L.A.R/W) to be fenced with Farm Field Type
 Fence (F.F.T. Fence) as specified in the plans.
 For location of the Item "d" Plain Cement Concrete Pavement see Sheets 7 and 28.

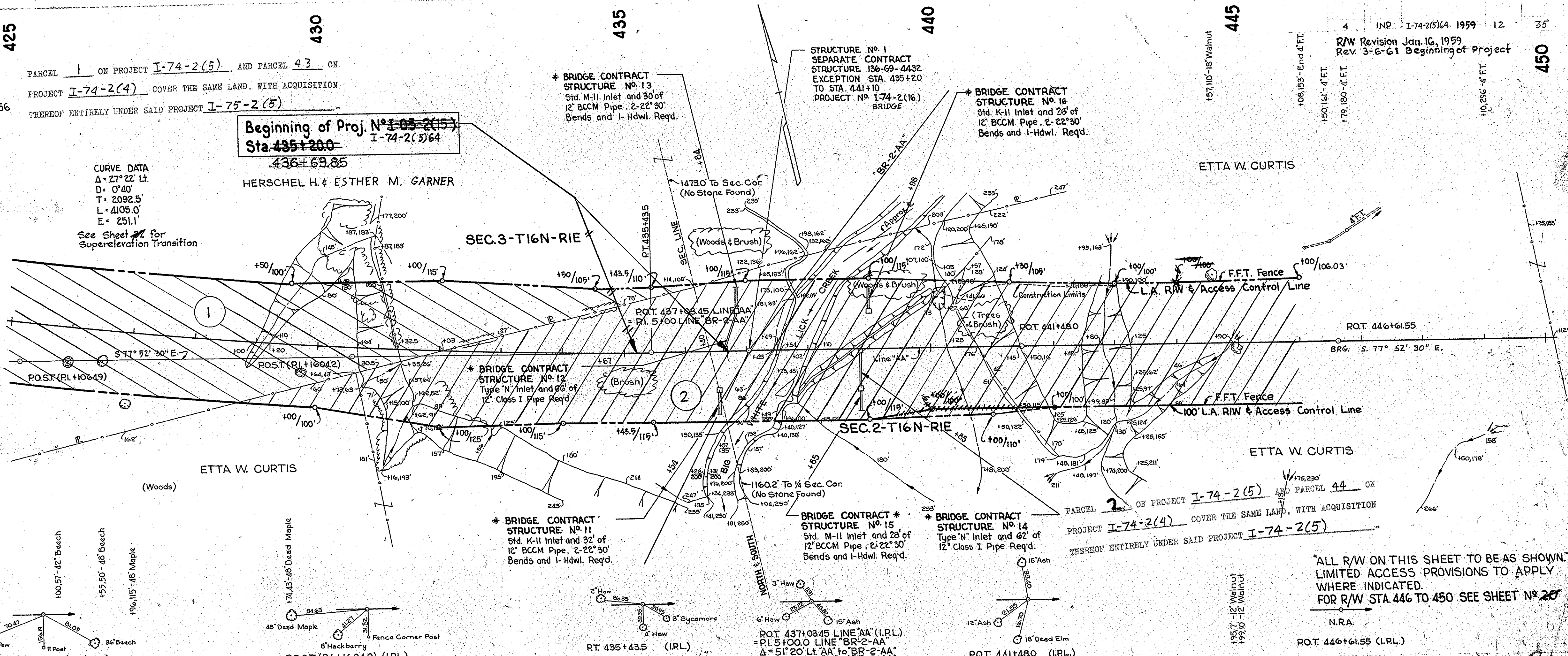
FARCEL 1 ON PROJECT I-74-2(5) AND FARCEL 43 ON
 PROJECT I-74-2(4) COVER THE SAME LAND, WITH ACQUISITION
 THEREOF ENTIRELY UNDER SAID PROJECT I-75-2(5)

Beginning of Proj. No. 1-85-2(15)
 Sta. 435+200 I-74-2(5)64

HERSCHEL H. & ESTHER M. GARNER

CURVE DATA
 $\Delta = 27^{\circ}22'11''$
 $D = 0'40''$
 $T = 2092.5'$
 $L = 4105.0'$
 $E = 251.1'$

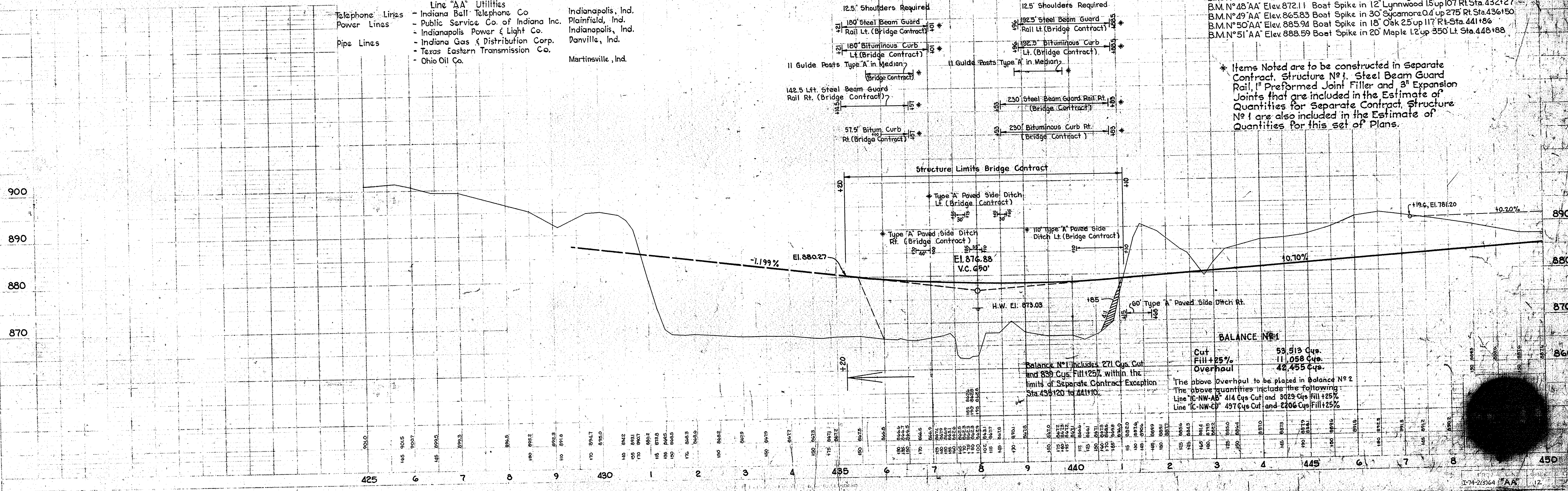
See Sheet #2 for
 Super-elevation Transition



- Line "AA" Utilities
- Telephone Lines - Indiana Bell Telephone Co.
 - Power Lines - Public Service Co. of Indiana Inc.
 - Indianapolis Power & Light Co.
 - Indianapolis, Ind.
 - Indiana Gas & Distribution Corp.
 - Texas Eastern Transmission Co.
 - Ohio Oil Co.
 - Martinsville, Ind.
- Pipe Lines

- 12.5' Shoulders Required
- 180' Steel Beam Guard Rail Lt. (Bridge Contract)
- 180' Bituminous Curb Lt. (Bridge Contract)
- 11 Guide Posts Type 'A' in Median (Bridge Contract)
- 142.5' Steel Beam Guard Rail Rt. (Bridge Contract)
- 57.5' Bitum. Curb Rt. (Bridge Contract)
- 230' Steel Beam Guard Rail Rt. (Bridge Contract)
- 230' Bituminous Curb Rt. (Bridge Contract)

* Items Noted are to be constructed in Separate Contract. Structure No. 1, Steel Beam Guard Rail, 1" Preformed Joint Filler and 3" Expansion Joints that are included in the Estimate of Quantities for Separate Contract. Structure No. 1 are also included in the Estimate of Quantities for this set of Plans.



"ALL R/W ON THIS SHEET TO BE AS SHOWN."
 LIMITED ACCESS PROVISIONS TO APPLY
 WHERE INDICATED.
 FOR R/W STA. 446 TO 450 SEE SHEET No. 28

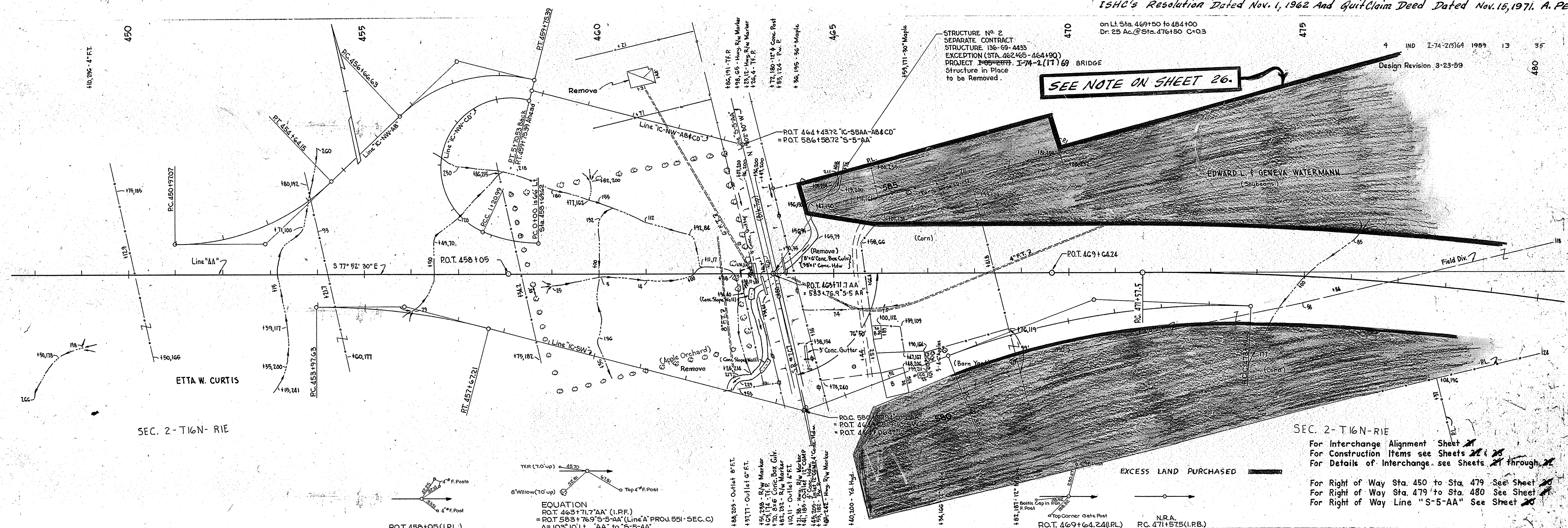
N.R.A.
 R.O.T. 446+61.55 (I.R.L.)

- B.M. No. 47 AA' Elev. 899.77 Boat Spike in 30' Hickory 15' up 245' Lt. Sta. 426+50
- B.M. No. 48 AA' Elev. 872.11 Boat Spike in 12' Lynnwood 15' up 107' Rt. Sta. 432+27
- B.M. No. 49 AA' Elev. 865.83 Boat Spike in 30' Sycamore 0' up 275' Rt. Sta. 436+50
- B.M. No. 50 AA' Elev. 885.94 Boat Spike in 18' Oak 25' up 117' Rt. Sta. 441+86
- B.M. No. 51 AA' Elev. 888.59 Boat Spike in 20' Maple 12' up 350' Lt. Sta. 448+88

SEE NOTE ON SHEET 26.

STRUCTURE NO. 2
SEPARATE CONTRACT
STRUCTURE 136-69-4433
EXCEPTION (STA. 462+65 - 464+90)
PROJECT I-74-20(64) BRIDGE
Structure in Place
to be Removed.

on Lt. Sta. 469+50 to 484+00
Dr. 25 Ac. @ Sta. 476+50 C=0.3

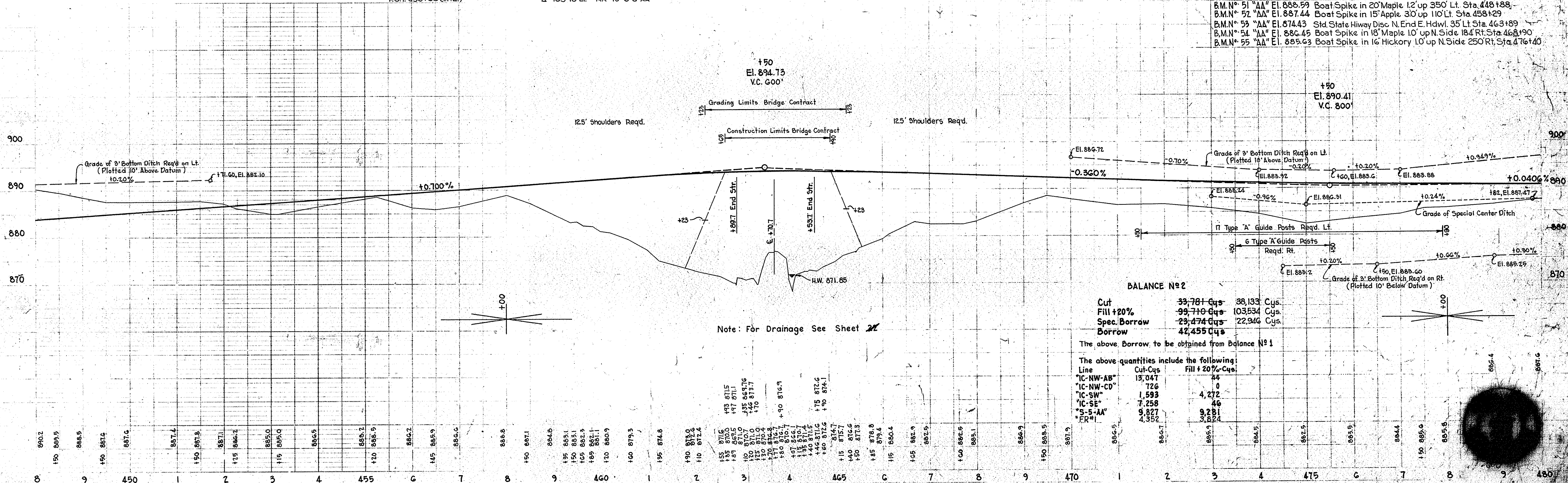


ETTA W. CURTIS
SEC. 2-T16N-R1E

EDWARD L. GENEVA WATERMANN
(Sublease)
SEC. 2-T16N-R1E
For Interchange Alignment Sheet 21
For Construction Items see Sheets 21 & 22
For Details of Interchange see Sheets 21 through 22
For Right of Way Sta. 450 to Sta. 479 See Sheet 21
For Right of Way Sta. 479 to Sta. 480 See Sheet 22
For Right of Way Line "S-5-AA" See Sheet 22

EQUATION
P.O.T. 465+71.7 "AA" (I.P.F.)
= P.O.T. 583+76.9 "S-5-AA" (Line "A" PROJ. 551-SEC. C)
 $\Delta = 103^{\circ}10'$ Lt. "AA" to "S-5-AA"

- B.M.N. 51 "AA" El. 888.59 Boat Spike in 20' Maple 12' up 350' Lt. Sta. 448+88
- B.M.N. 52 "AA" El. 887.44 Boat Spike in 15' Apple 30' up 110' Lt. Sta. 458+29
- B.M.N. 53 "AA" El. 874.43 Std. State Hiway Disc N. End E. Hdwl. 35' Lt. Sta. 463+89
- B.M.N. 54 "AA" El. 886.45 Boat Spike in 18' Maple 10' up N. Side 184' Rt. Sta. 468+90
- B.M.N. 55 "AA" El. 885.63 Boat Spike in 16' Hickory 10' up N. Side 250' Rt. Sta. 476+40



Note: For Drainage See Sheet 22

The above Borrow to be obtained from Balance No. 1

on Lt. Sta. 484+00 to 491+00
Dr. 40 Ac. @ Sta. 489+30 C=0.2

on Lt. Sta. 499+00 to 507+60
Dr. 25 Ac. @ Sta. 502+100 C=0.2

SEE NOTE ON SHEET 26.

SEC. 2-T16N-R1E

SEC. 1-T16N-R1E

EDWARD L. & GENEVA E. WATERMAN

PATRICK WYNNE

MERCHANTS NATIONAL BANK & TRUST CO., TRUSTEE

Dr. Area 40 Acres
C=0.2
STRUCTURE N° 52
156' of 36"x22" B.C.C.M.
pipe Arch and 15' on 30' Tee req'd.

Equalizer
STRUCTURE N° 54
158' of 22"x18" B.C.C.M.
pipe Arch req'd.
Construct inlet
ditch Lt.

Dr. Area 25 Acres
C=0.2
STRUCTURE N° 55
160' of 25"x16" B.C.C.M.
pipe Arch, Skew 10° Rt.,
1-15' on 25"x16" Tee req'd.
Const. inlet and outlet ditches.

(Plowed)
STRUCTURE N° 53
Std. "N" inlet and 4' of
15" B.C.C.M. pipe req'd.
Connect to Str. N° 52

STRUCTURE N° 56
Std. "N" inlet and 4' of
15" B.C.C.M. pipe req'd.
Connect to Str. N° 55

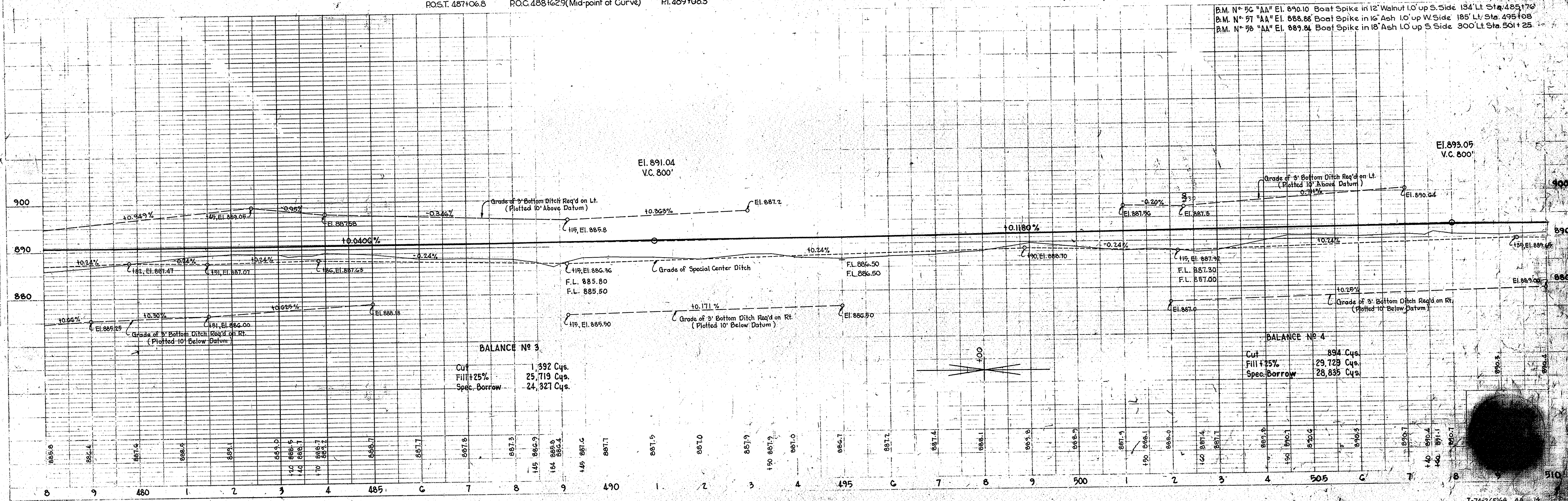
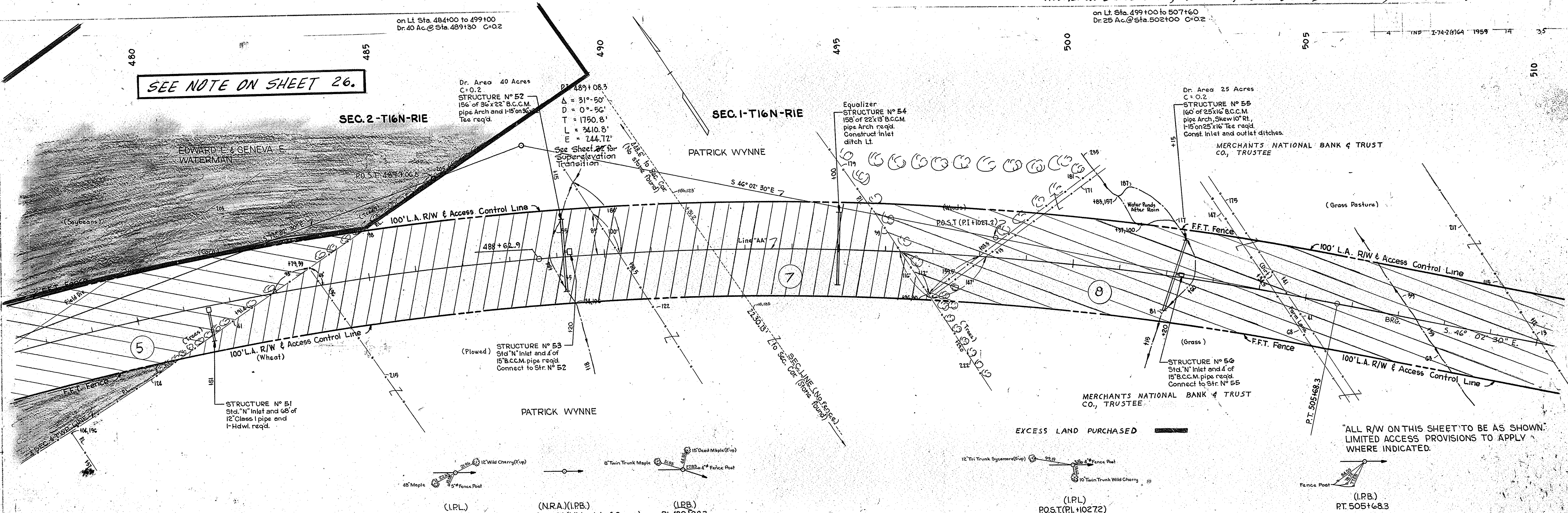
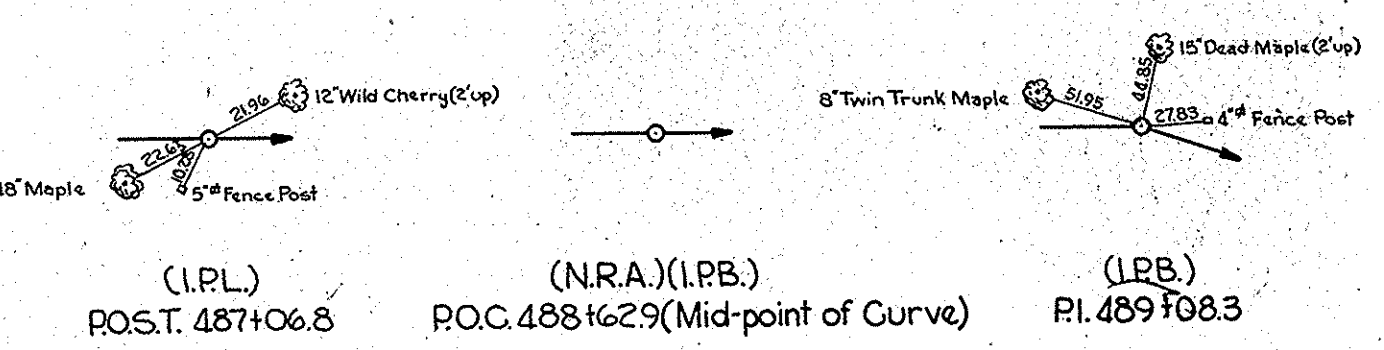
STRUCTURE N° 51
Std. "N" inlet and 68' of
12" Class I pipe and
1" Hdwl. req'd.

PATRICK WYNNE

MERCHANTS NATIONAL BANK & TRUST CO., TRUSTEE

"ALL R/W ON THIS SHEET TO BE AS SHOWN.
LIMITED ACCESS PROVISIONS TO APPLY
WHERE INDICATED."

EXCESS LAND PURCHASED



B.M. N° 50 "AA" El. 890.10 Boat Spike in 12" Walnut 1.0' up S. Side 134' Lt. Sta. 485+76
B.M. N° 51 "AA" El. 888.88 Boat Spike in 16" Ash 1.0' up W. Side 185' Lt. Sta. 495+08
B.M. N° 52 "AA" El. 889.84 Boat Spike in 18" Ash 1.0' up S. Side 300' Lt. Sta. 501+25

El. 893.05
V.C. 800'

PERRY
 PIAN
 7892 T
 PIERCE, GRUBER, & BOAM, INC.
 7893 L
 1000 W. WASHINGTON ST.
 CHICAGO, ILL. 60601

510

515

520

525

530

535

540

on Lt. Sta. 507160 to 538+30 Dn 100 Ac. C=0.2

4 IND I-74-2(5)64 1959 15 35

MERCHANTS NATIONAL BANK & TRUST
CO., TRUSTEE

SEC. 1-T16N-R1E

SEC. 12-T16N-R1E

STRUCTURE No. 3
SEPARATE CONTRACT
STRUCTURE 136-G9-4434
NO EXCEPTION
PROJECT No. I-74-2(1)8
BRIDGE

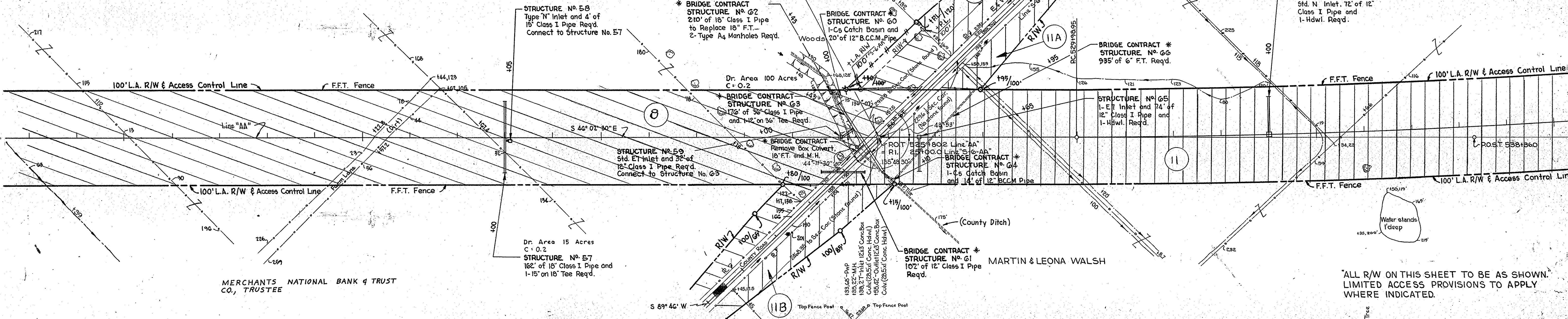
* BRIDGE CONTRACT
STRUCTURE No. 62
210' of 18" Class I Pipe
to Replace 18" F.T.
2-Type A4 Manholes Req'd.

BRIDGE CONTRACT *
STRUCTURE No. 60
1-C5 Catch Basin and
20' of 12" B.C.C.M. Pipe

BRIDGE CONTRACT *
STRUCTURE No. 66
935' of 6" F.T. Req'd.

MARTIN & LEONA WALSH
STRUCTURE No. 67
Std. N Inlet, 72' of 12"
Class I Pipe and
1-Hdwl. Req'd.

Δ=11'29"30" Lt.
D=0'20"
T=1729.665'
L=3447.143'
E=86.85'
See Sheet 14 for
Superelevation Transition



MERCHANTS NATIONAL BANK & TRUST
CO., TRUSTEE

MARTIN & LEONA WALSH

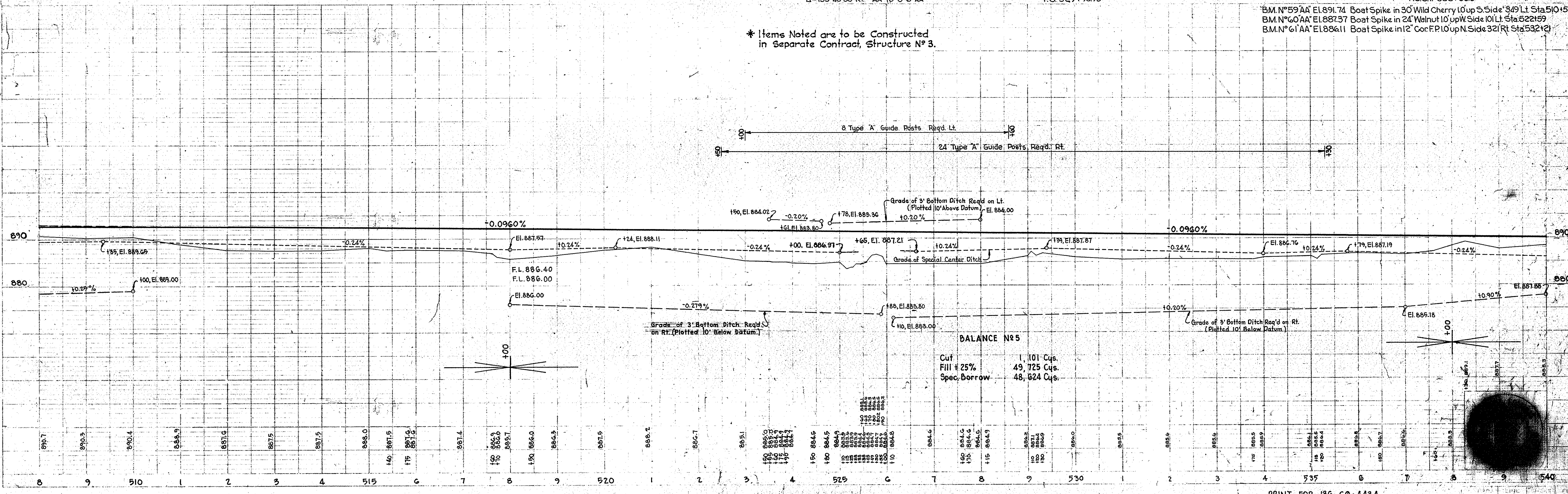
* ALL R/W ON THIS SHEET TO BE AS SHOWN.
LIMITED ACCESS PROVISIONS TO APPLY
WHERE INDICATED.

EQUATION
P.O.T. 525+802 'AA' (I.P.F.)
= R1 251000.0 'S-6-AA'
Δ=135°48'30" Rt. 'AA' to 'S-6-AA'

8" Elm
Fence Post (I.P.B.)
P.C. 529+9895

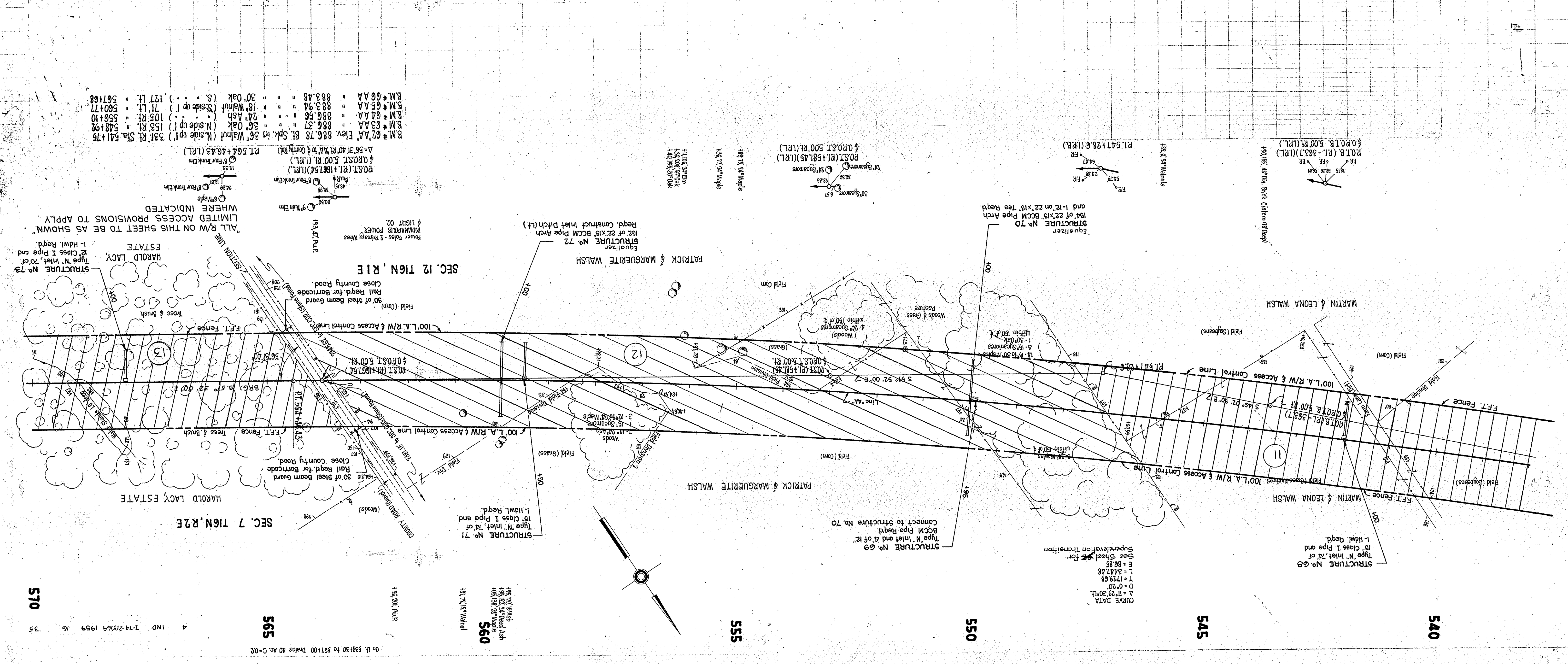
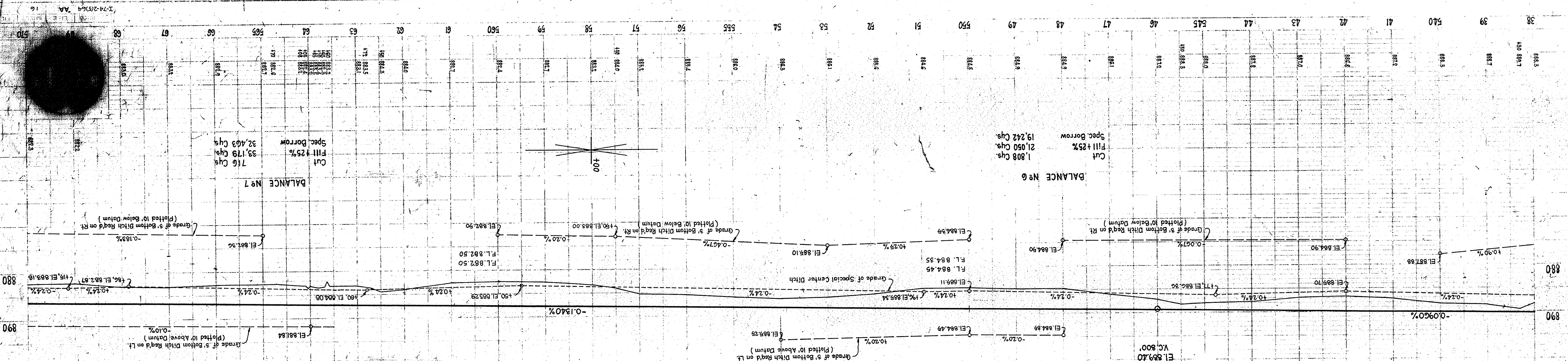
* Items Noted are to be Constructed
in Separate Contract, Structure No. 3.

BM. No. 59 'AA' El. 891.74 Boat Spike in 30' Wild Cherry 10' up S. Side 349' Lt. Sta. 510+55
BM. No. 60 'AA' El. 887.37 Boat Spike in 24' Walnut 10' up W. Side 101' Lt. Sta. 521+59
BM. No. 61 'AA' El. 886.11 Boat Spike in 12' Con. F.P. 10' up N. Side 321' Rt. Sta. 532+12



PRINT FOR 136-G9-4434

I-74-2(5)64 "AA" 15



B.M. # 67 A	Elev.	886.78	Bl. Spk. in 36" Walnut (N. side up I.)	331' Rl. Sta. 541+75
B.M. # 63 A	Elev.	886.37	36" Oak (N. side up I.)	153' Rl. Sta. 548+92
B.M. # 64 A	Elev.	886.56	24" Ash	105' Rl. Sta. 556+10
B.M. # 65 A	Elev.	883.94	18" Walnut (S. side up I.)	71' Lt. Sta. 560+77
B.M. # 66 A	Elev.	883.48	30" Oak (S. side up I.)	127' Lt. Sta. 567+68

"ALL R/W ON THIS SHEET TO BE AS SHOWN"
 LIMITED ACCESS PROVISIONS TO APPLY
 WHERE INDICATED

ON LI 538130 TO 567100 Plains 40 Ac. C-02
 IND I-74-2(5) 1959 16
 565
 570

570

575

580

585

590

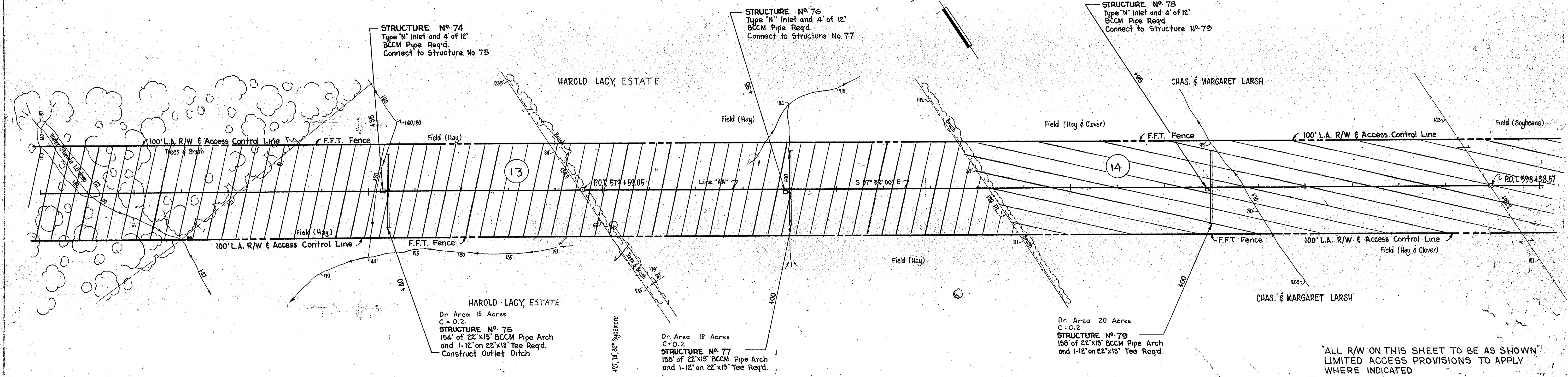
595

600

On Lt. Sta. 567+00 to Sta. 578+00 Dr. 15 Ac. C=0.2

On Rt. Sta. 578+00 to Sta. 598+00 Dr. 20 Ac. C=0.2

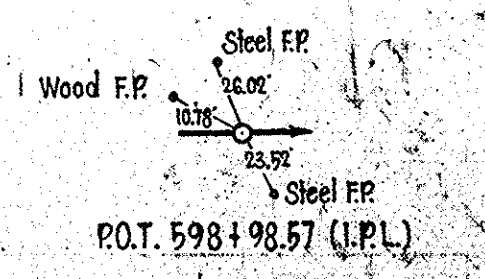
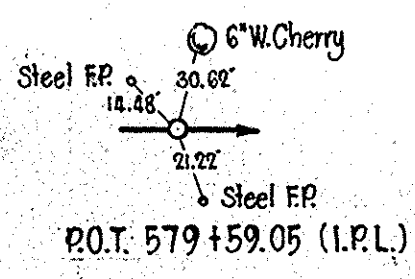
4 IND. 1-74-2(6)64 1959 17 35



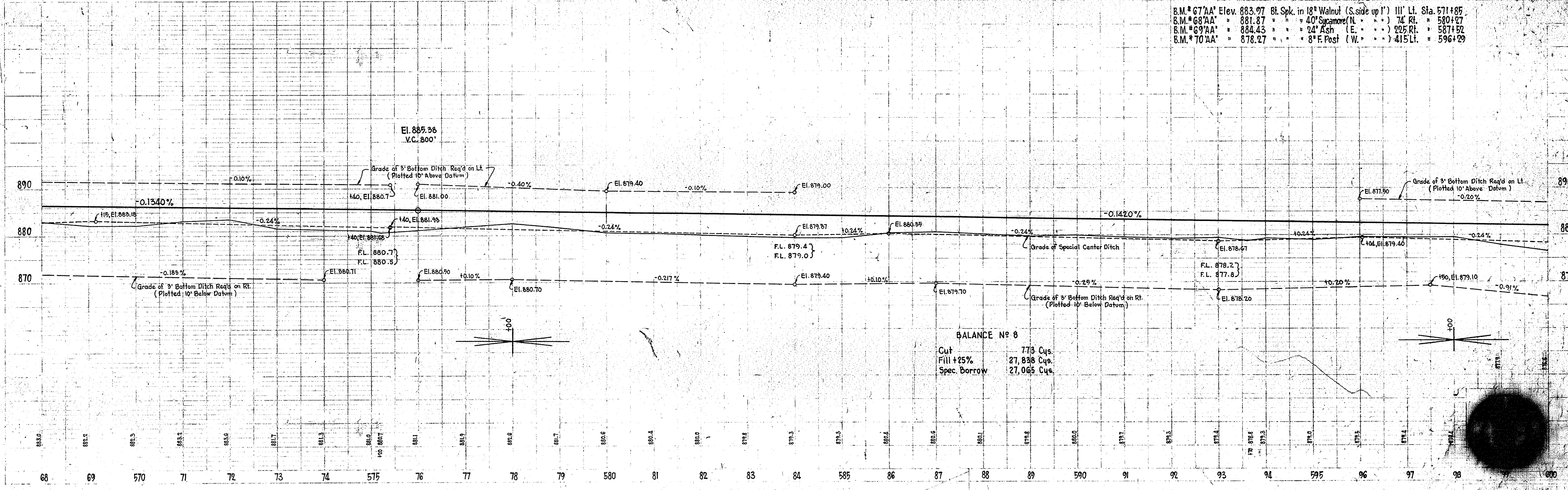
SEC. 7-T16N-R2E

SEC. 7-T16N-R2E

ALL R/W ON THIS SHEET TO BE AS SHOWN LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED



B.M. #67'AA'	Elev. 883.97	Bl. Spk. in 18" Walnut (S. side up 1')	III' Lt. Sta. 571+85
B.M. #68'AA'	881.37	40' Sycamore (N.)	74' Rt. 580+27
B.M. #69'AA'	884.43	24' Ash (E.)	225' Rt. 587+52
B.M. #70'AA'	878.27	8' F. Post (W.)	415' Lt. 596+29

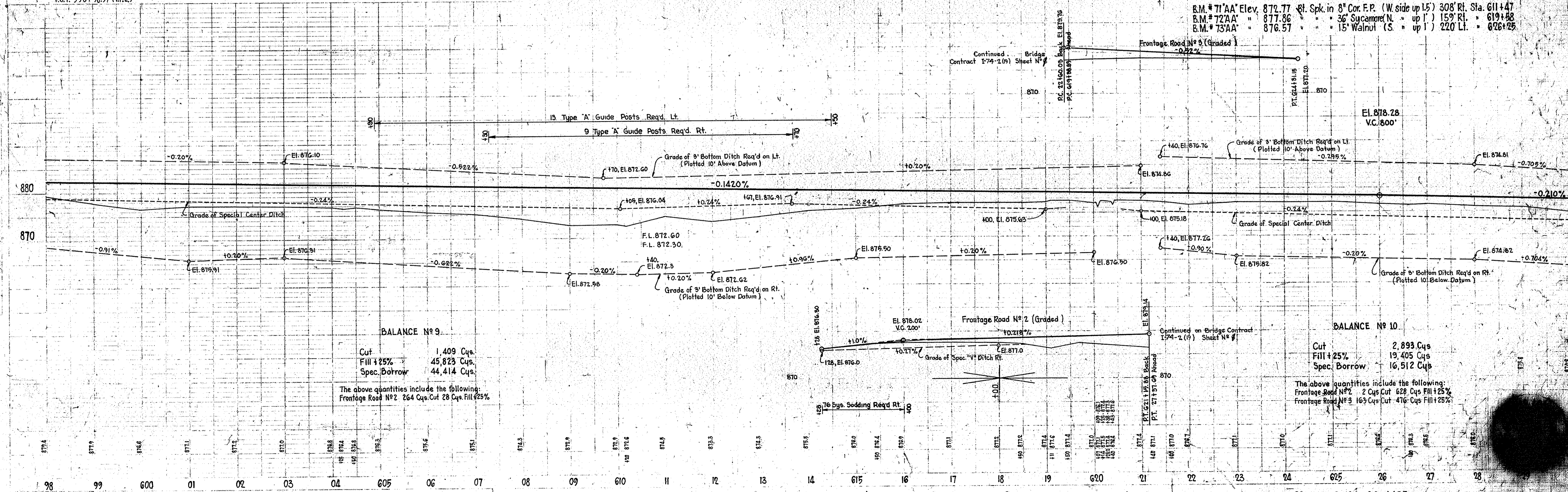
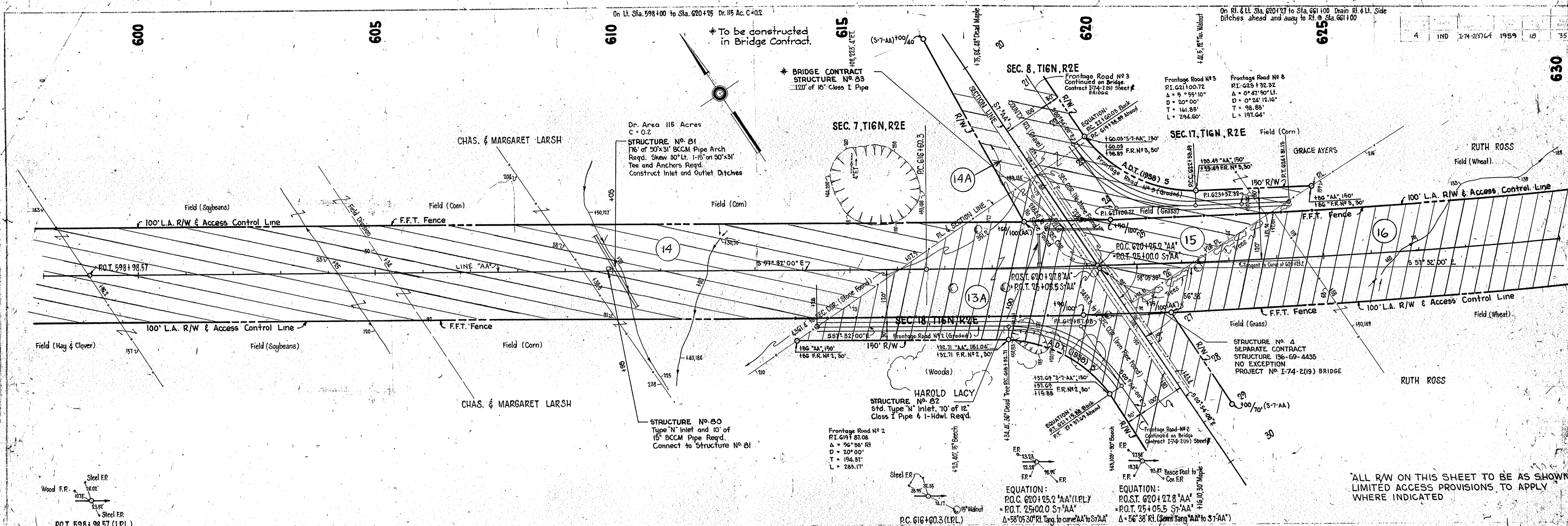


P.L.A.H. Pierce, Gruber & Baum, Inc.

PROF. Pierce, Gruber & Baum, Inc.

PIERCE, GRUBER & BEAM, INC.
1111 N. W. 11th St., Indianapolis, Ind. 46202

PIERCE, GRUBER & BEAM, INC.
1111 N. W. 11th St., Indianapolis, Ind. 46202



ALL R/W ON THIS SHEET TO BE AS SHOWN
LIMITED ACCESS PROVISIONS TO APPLY
WHERE INDICATED

BALANCE No. 9

Cut	1,409 Cys.
Fill +25%	45,823 Cys.
Spec. Borrow	44,414 Cys.

The above quantities include the following:
Frontage Road No. 2 264 Cys. Cut 28 Cys. Fill +25%

BALANCE No. 10

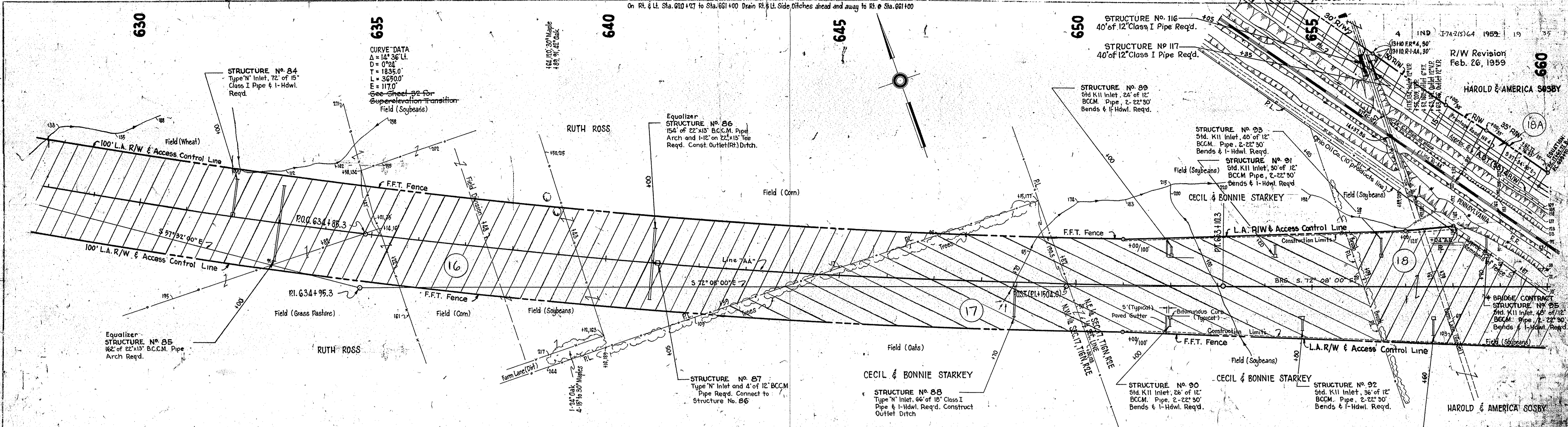
Cut	2,893 Cys.
Fill +25%	19,405 Cys.
Spec. Borrow	16,512 Cys.

The above quantities include the following:
Frontage Road No. 2 2 Cys. Cut 628 Cys. Fill +25%
Frontage Road No. 3 163 Cys. Cut 476 Cys. Fill +25%

On Rt. & Lt. Sta. 620+127 to Sta. 651+00 Drain Rt. & Lt. Side Ditches ahead and away to Rt. @ Sta. 651+00

CECIL & BONNIE STARKEY
Sta. 10+50 Begin Const. Frontage Road No. 4.

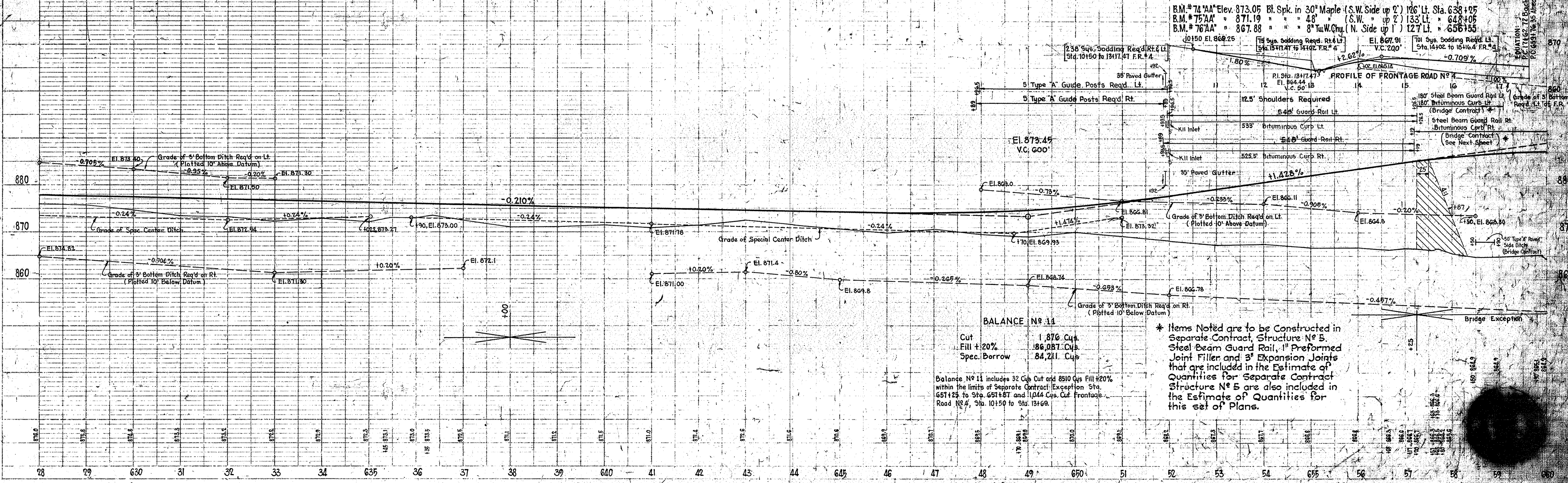
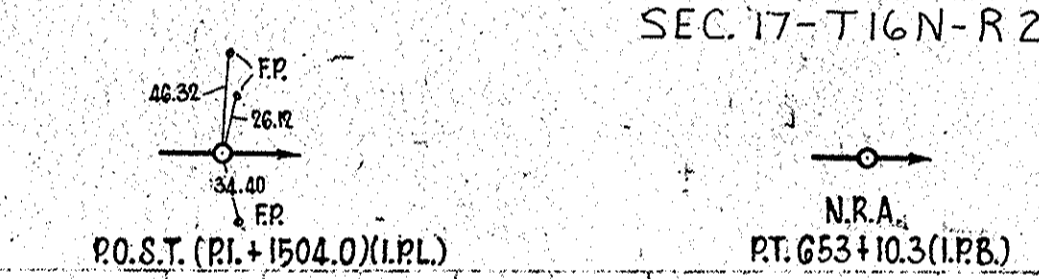
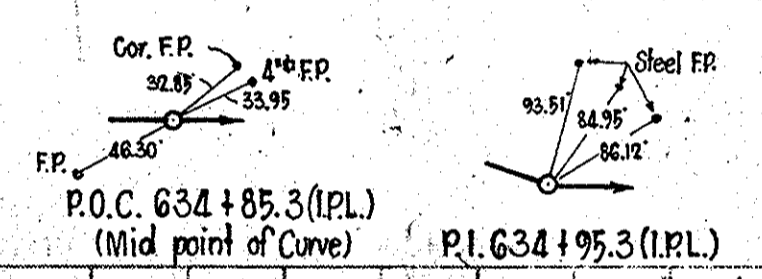
IND 1742(5)64 1959 19
R/W Revision Feb. 26, 1959
HAROLD & AMERICA SOSBY



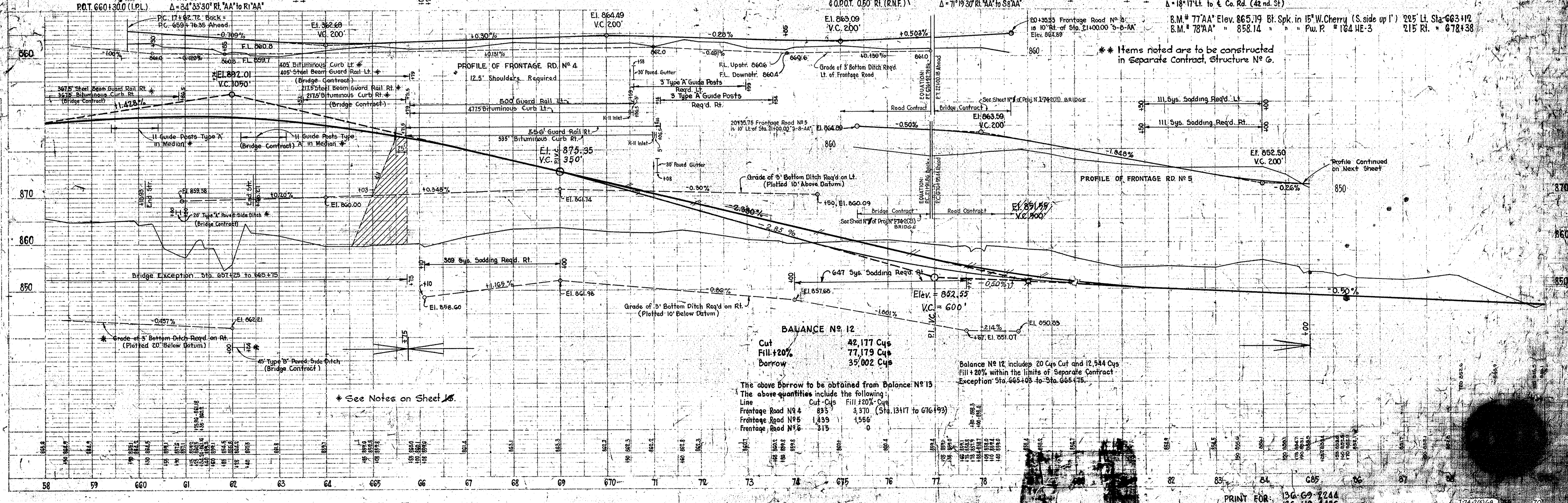
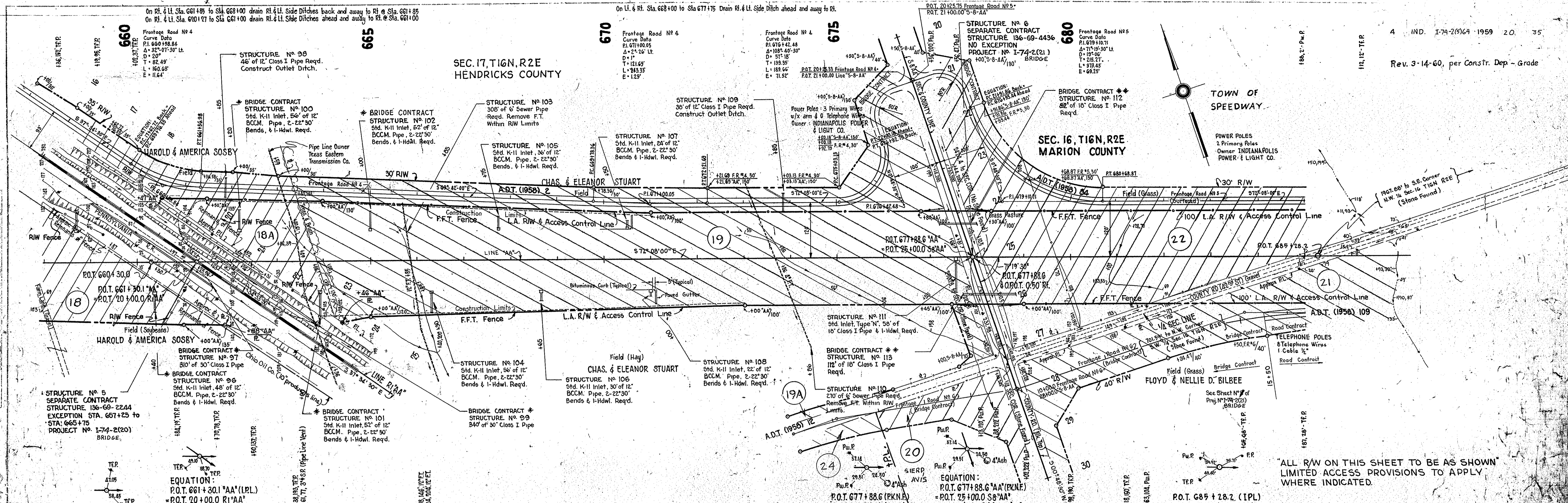
SEC. 17-T16N-R2E

SEC. 17-T16N-R2E

ALL R/W ON THIS SHEET TO BE AS SHOWN LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED

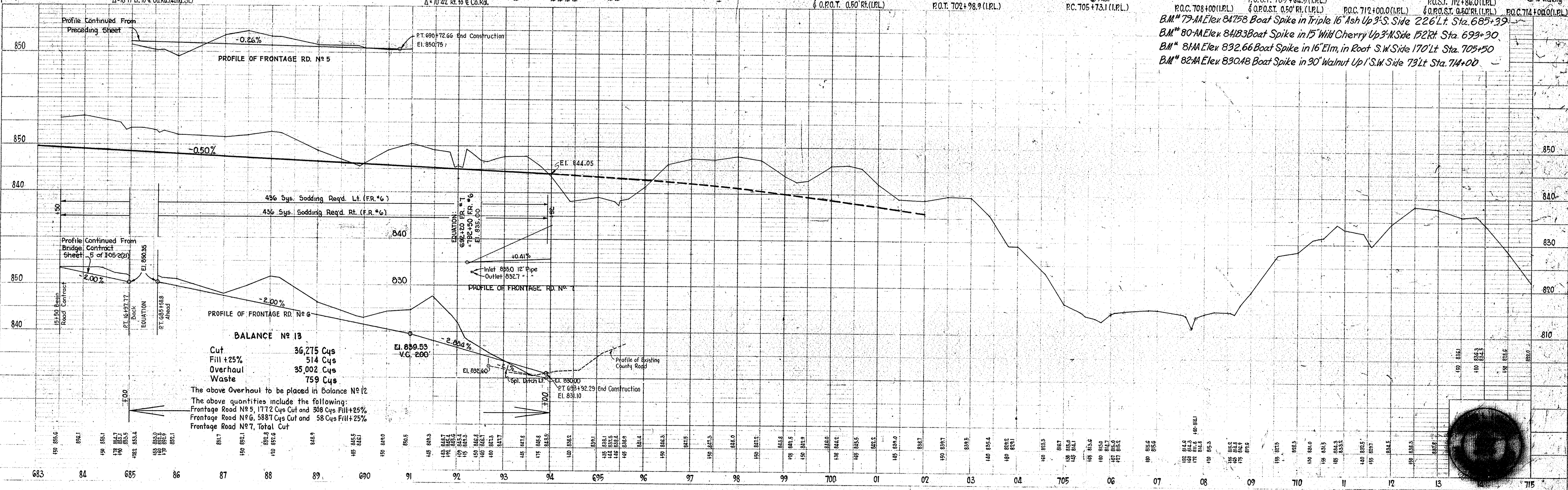
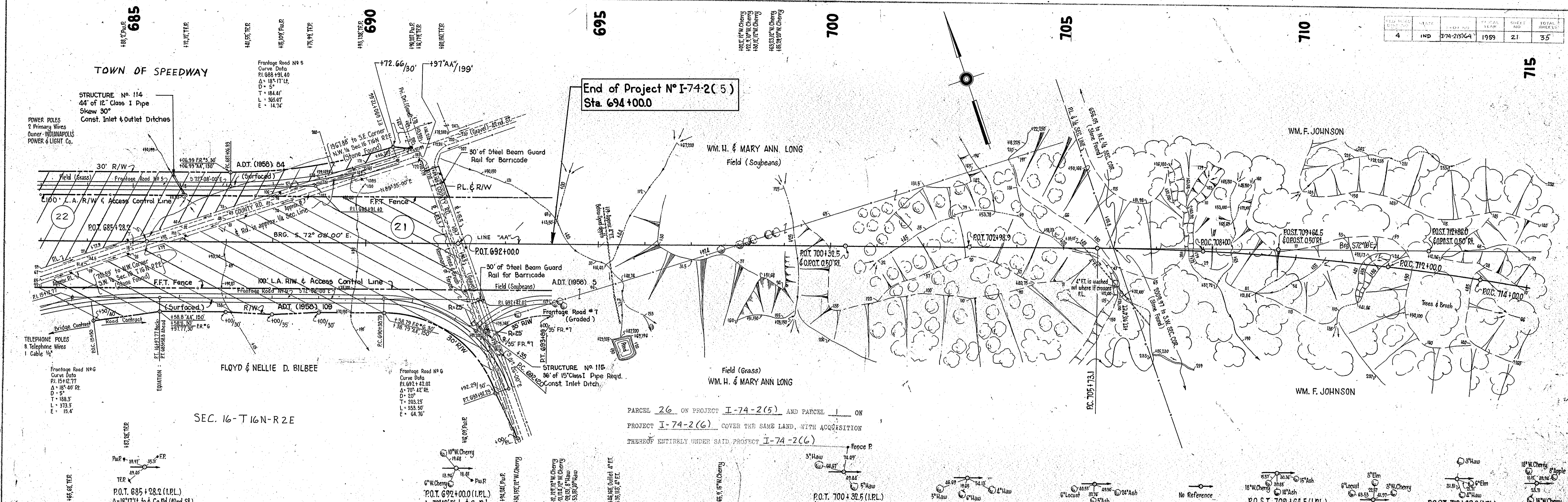


PLAN: Paces, Gruber & Gason, Inc.
 PROFILE: Paces, Gruber & Gason, Inc.
 DATE: 1959



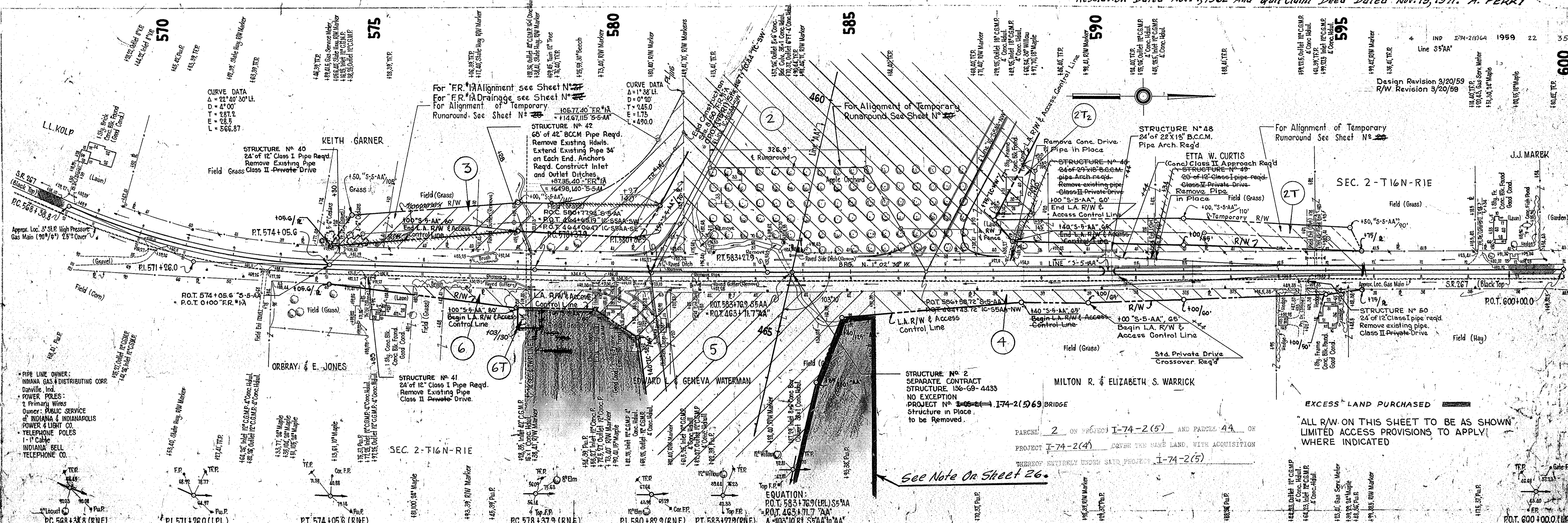
PLAN
SURVEYED PIERCES, GRUBER & BEAM, INC.
NOTED: ALL DIMENSIONS IN FEET UNLESS OTHERWISE NOTED.
NOISE BOOK: 5 HOURS NOTED.
INDUSTRIAL INSURANCE CO. OF INDIANA, INDIANAPOLIS, IN.

PROFILE
SURVEYED PIERCES, GRUBER & BEAM, INC.
NOTE: GRADES CHECKED.
NOISE BOOK: 5 HOURS NOTED.
INDUSTRIAL INSURANCE CO. OF INDIANA, INDIANAPOLIS, IN.



Reference
P.O.C. 708+00 (I.P.L.)
B.M. 79-AA Elev 847.58 Boat Spike in Triple 16" Ash Up 3'-S. Side 226 Lt. Sta. 685+39
B.M. 80-AA Elev 848.33 Boat Spike in 15" Wild Cherry Up 3'-S. Side 172 Rt. Sta. 693+30
B.M. 81-AA Elev 832.66 Boat Spike in 16" Elm, in Root S.W. Side 170 Lt. Sta. 705+50
B.M. 82-AA Elev 830.48 Boat Spike in 30" Walnut Up 1'-S.W. Side 73 Lt. Sta. 714+00





PIPE LINE OWNER:
INDIANA GAS & DISTRIBUTING CORP.
Danville, Ind.
POWER POLES:
2 Primary Wires
Owner: PUBLIC SERVICE
of INDIANA & INDIANAPOLIS
POWER & LIGHT CO.
1-11 Cable
INDIANA BELL
TELEPHONE CO.

CURVE DATA
Δ = 22° 40' 30" LI.
D = 4° 00'
T = 287.2
E = 28.5
L = 566.87

For "R" Alignment see Sheet N° 26
For "R" Drainage see Sheet N° 26
For Alignment of Temporary Runaround see Sheet N° 26

STRUCTURE N° 42
68' of 42" B.C.M. Pipe Req'd.
Remove Existing Hdwl.
Extend Existing Pipe 34'
on Each End. Anchors
Req'd. Construct Inlet
and Outlet Drives
= 106.77, 40' "R" AA
= 114.67, 115' "S-5-AA"

STRUCTURE N° 48
24' of 22" X 18" B.C.C.M.
Pipe Arch. Req'd.

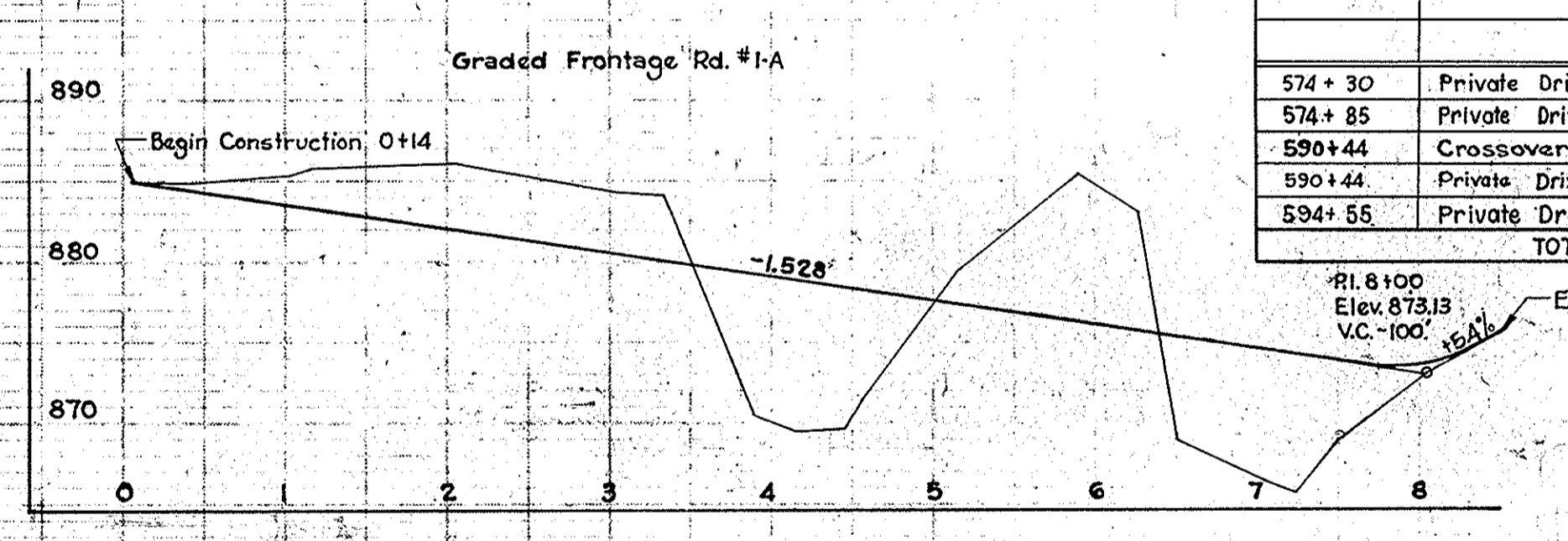
For Alignment of Temporary Runaround see Sheet N° 26

STRUCTURE N° 2
SEPARATE CONTRACT
STRUCTURE 136-69-4433
NO EXCEPTION
PROJECT N° 1-74-2(5) BRIDGE

EXCESS LAND PURCHASED
"ALL R/W ON THIS SHEET TO BE AS SHOWN"
LIMITED ACCESS PROVISIONS TO APPLY
WHERE INDICATED

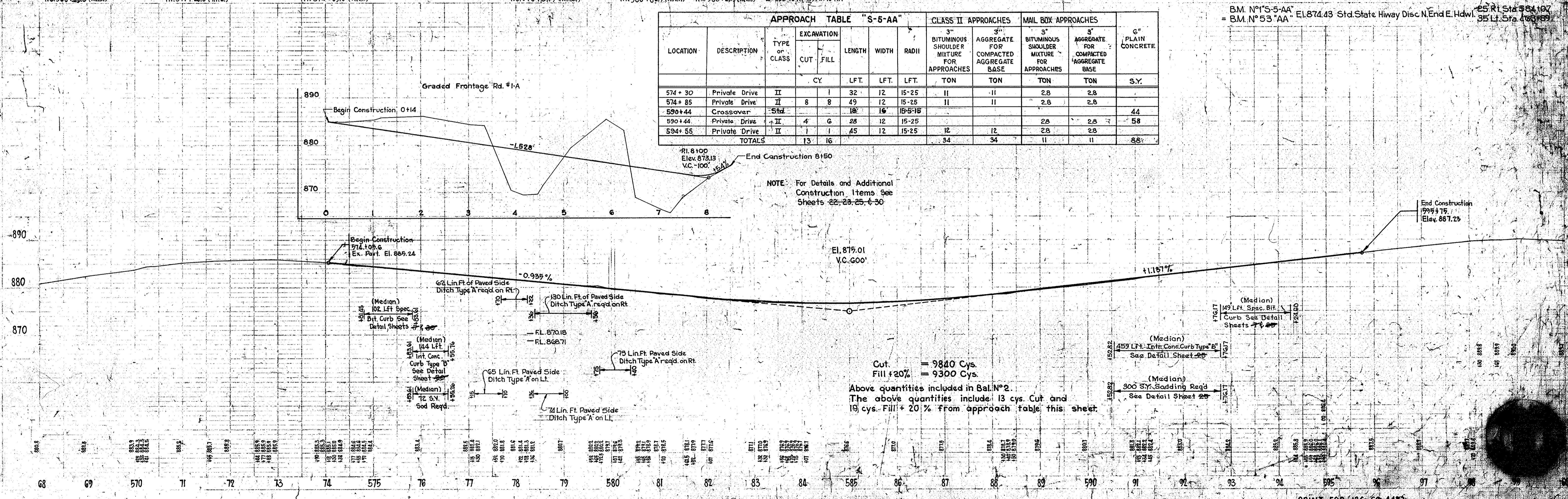
EQUATION:
P.O.T. 583+76.9 (H.L.) S-5-AA
= P.O.T. 463+71.7 "AA"
A = 103.10 R1 S-5-AA TO "AA"

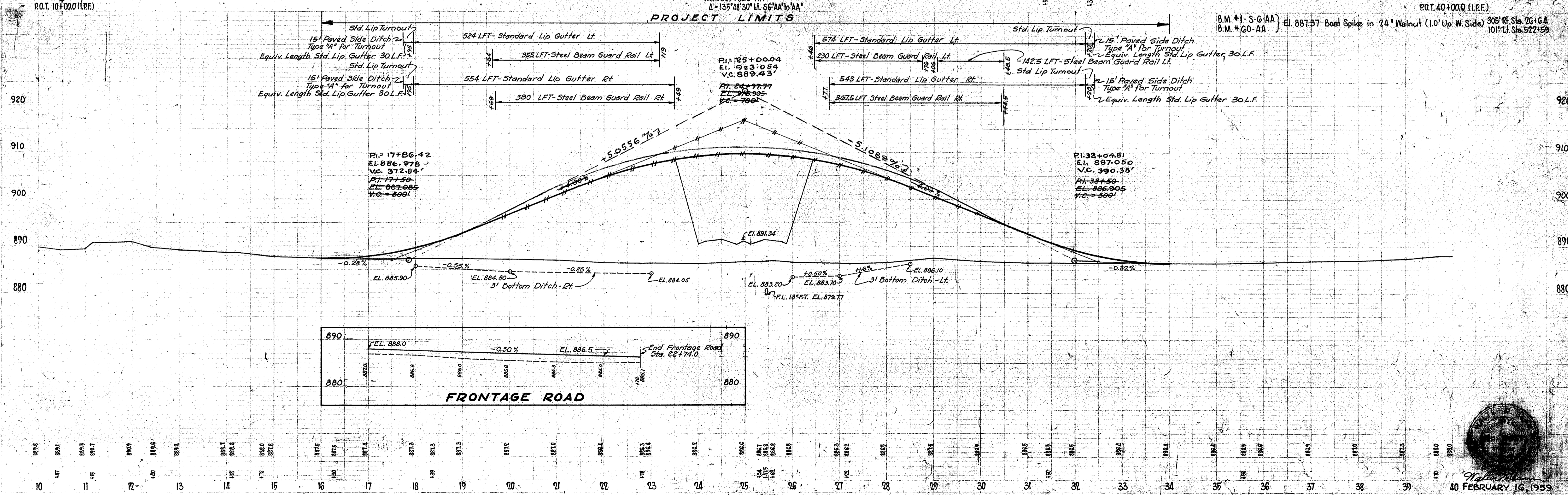
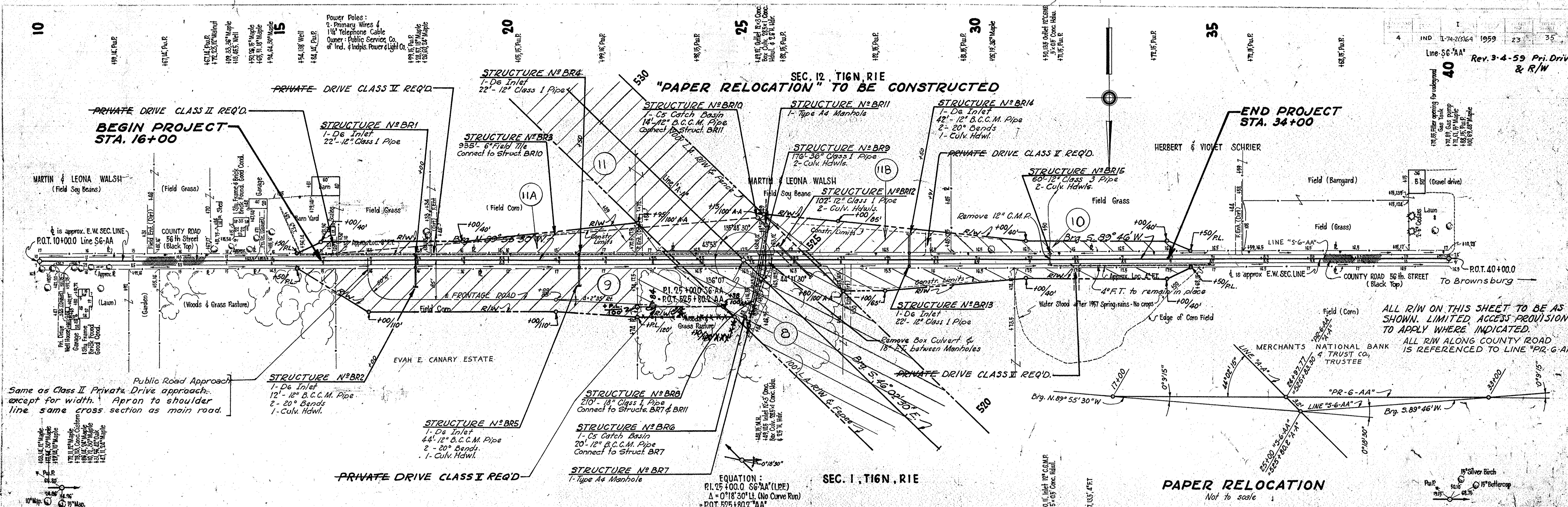
APPROACH TABLE "S-5-AA"		EXCAVATION		CLASS II APPROACHES		MAIL BOX APPROACHES		6" PLAIN CONCRETE		
LOCATION	DESCRIPTION	TYPE OF CLASS	CUT	FILL	3" BITUMINOUS SHOULDER MIXTURE FOR APPROACHES	3" AGGREGATE FOR COMPACTED AGGREGATE BASE	3" BITUMINOUS SHOULDER MIXTURE FOR APPROACHES		3" AGGREGATE FOR COMPACTED AGGREGATE BASE	
574+30	Private Drive	II	1	32	12	15-25	II	2.8	2.8	
574+85	Private Drive	II	8	8	49	12	II	2.8	2.8	
590+44	Crossover	Std.	16	16	15-5-15				44	
590+44	Private Drive	II	4	6	28	12	15-25	2.8	2.8	
594+55	Private Drive	II	1	45	12	15-25	12	2.8	2.8	
TOTALS			13	16			34	34	11	11

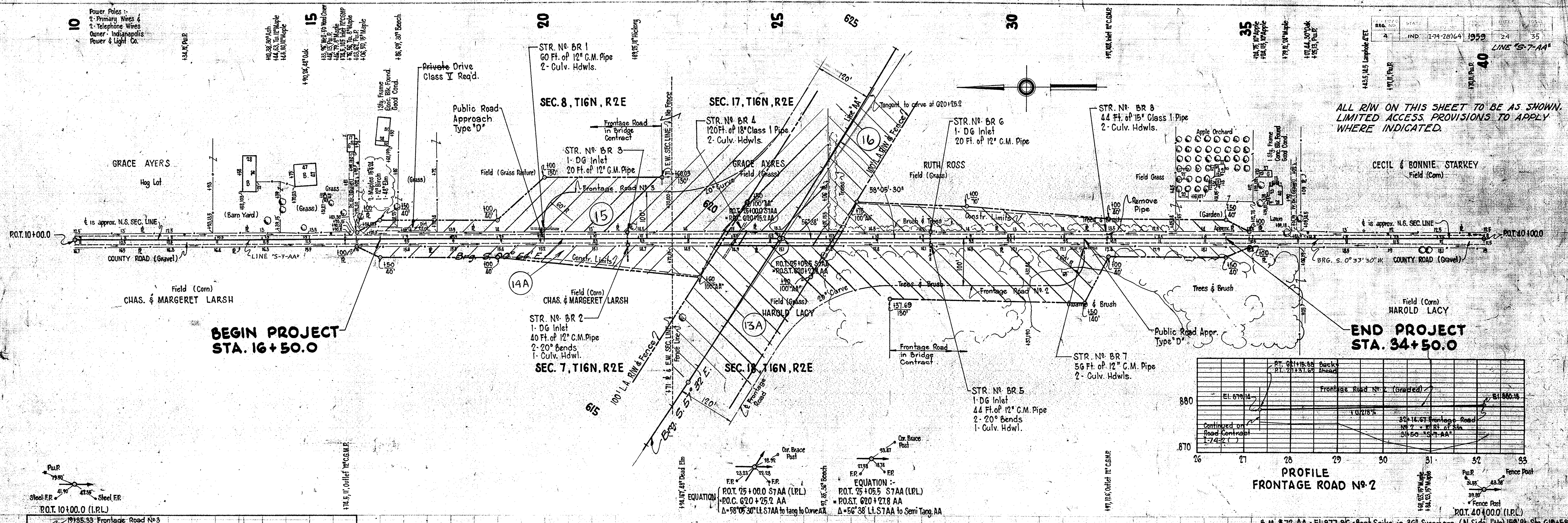


NOTE: For Details and Additional Construction Items See Sheets 22-23-25 & 30

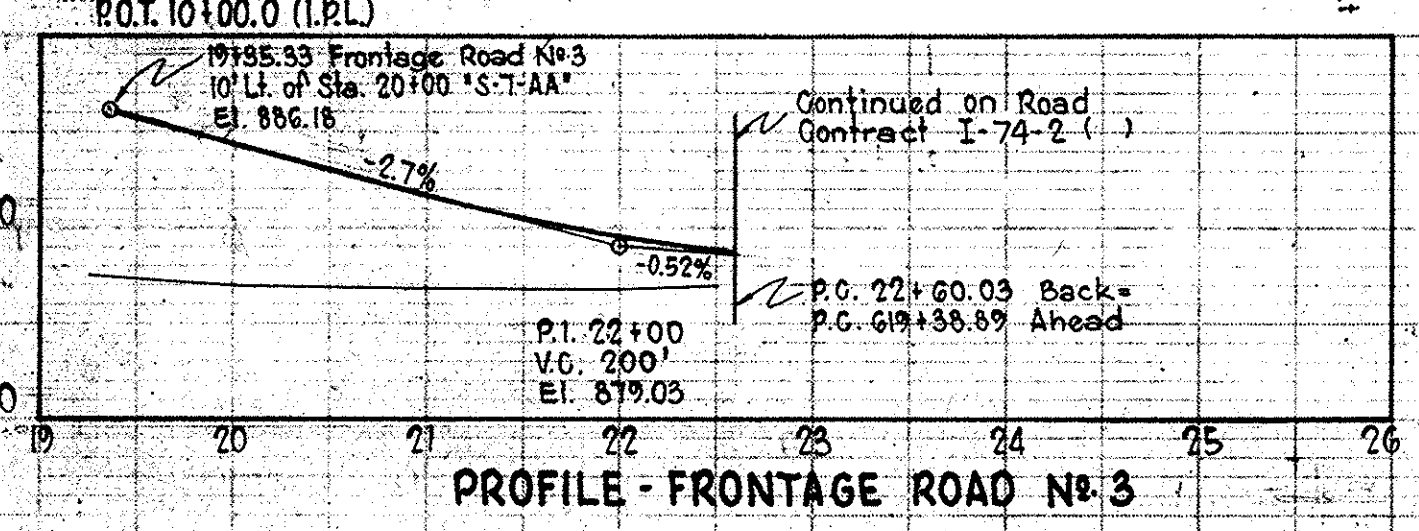
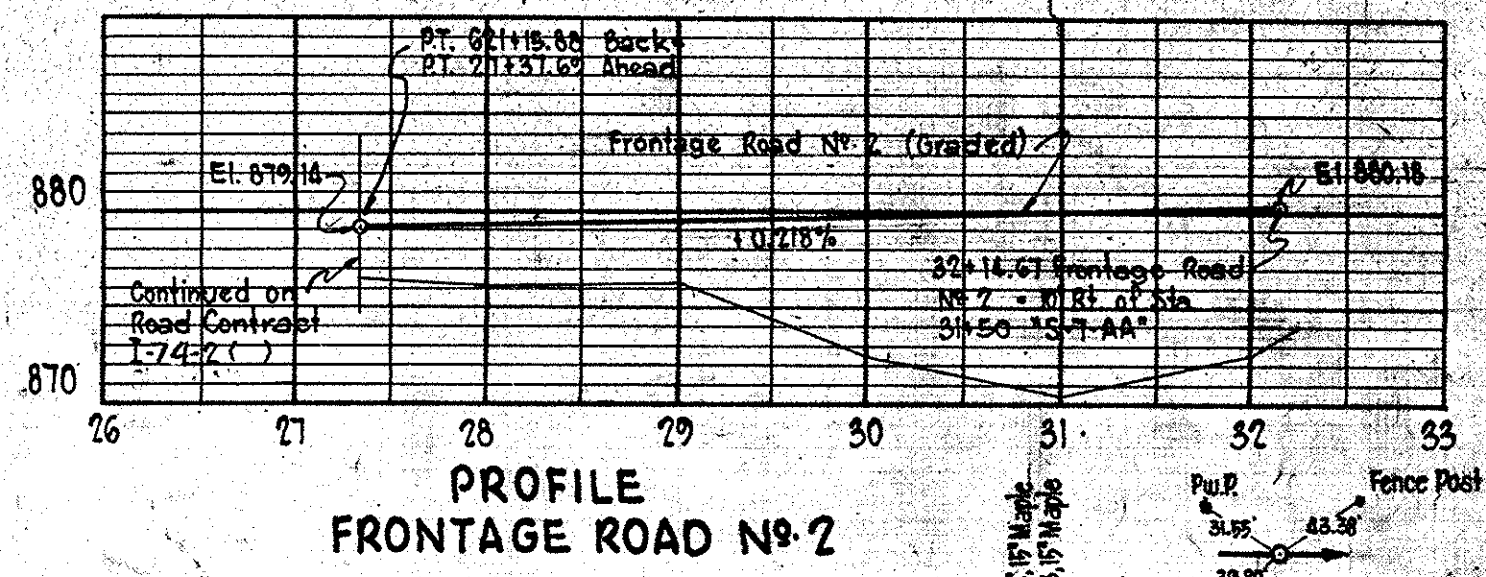
Cut = 9840 Cys.
Fill + 20% = 9300 Cys.
Above quantities included in Bal. N° 2.
The above quantities include 13 cys. Cut and 19 cys. Fill + 20% from approach table this sheet.



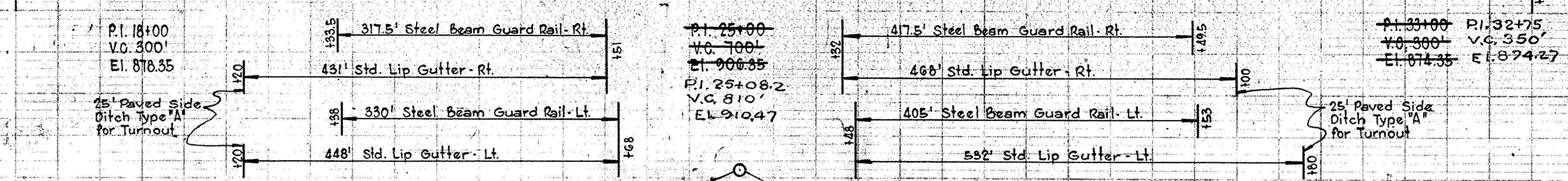




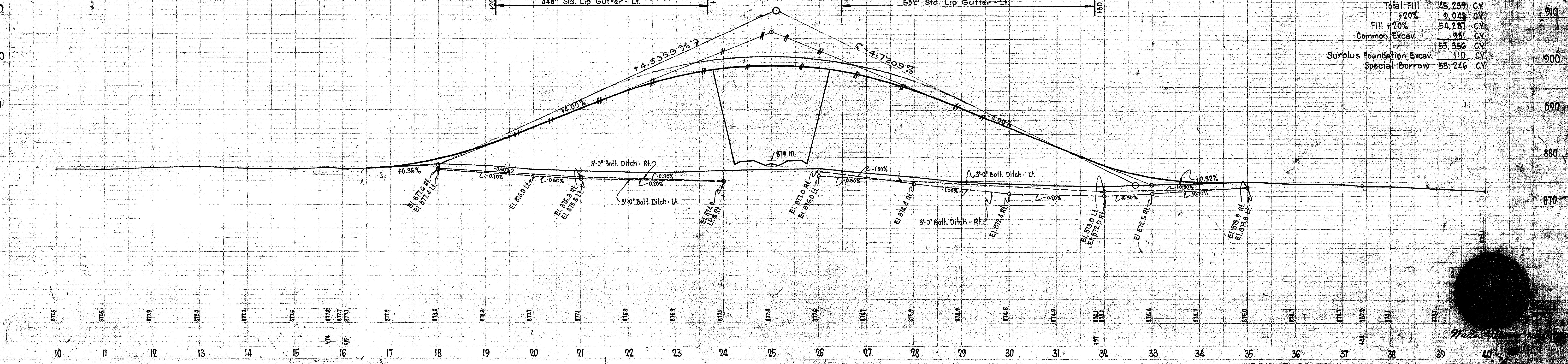
ALL R/W ON THIS SHEET TO BE AS SHOWN. LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.

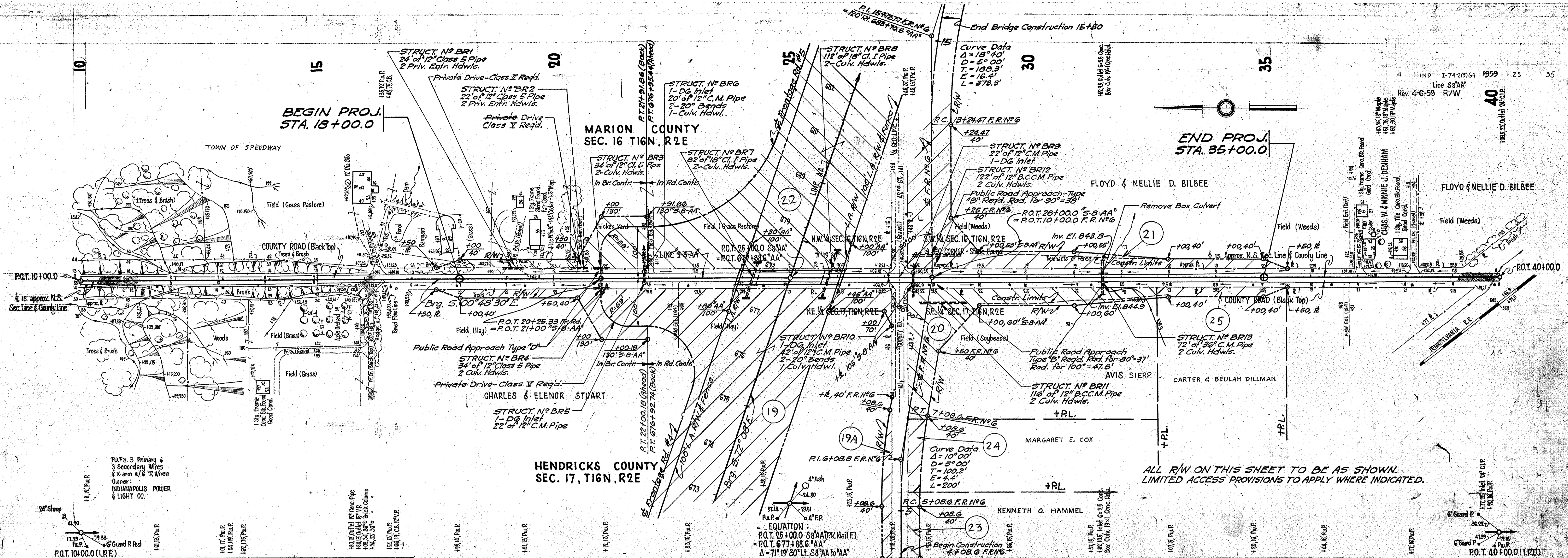


PROJECT LIMITS

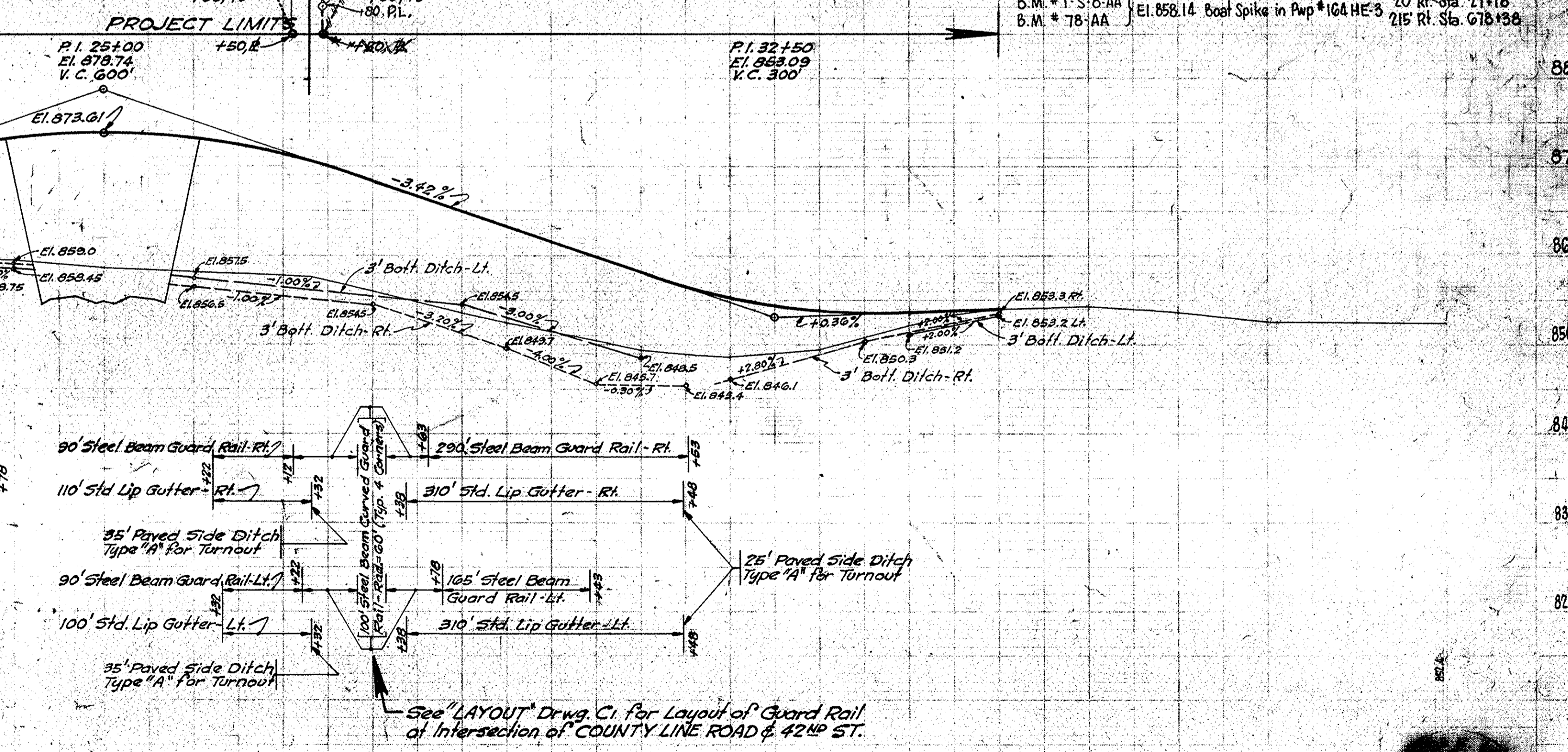
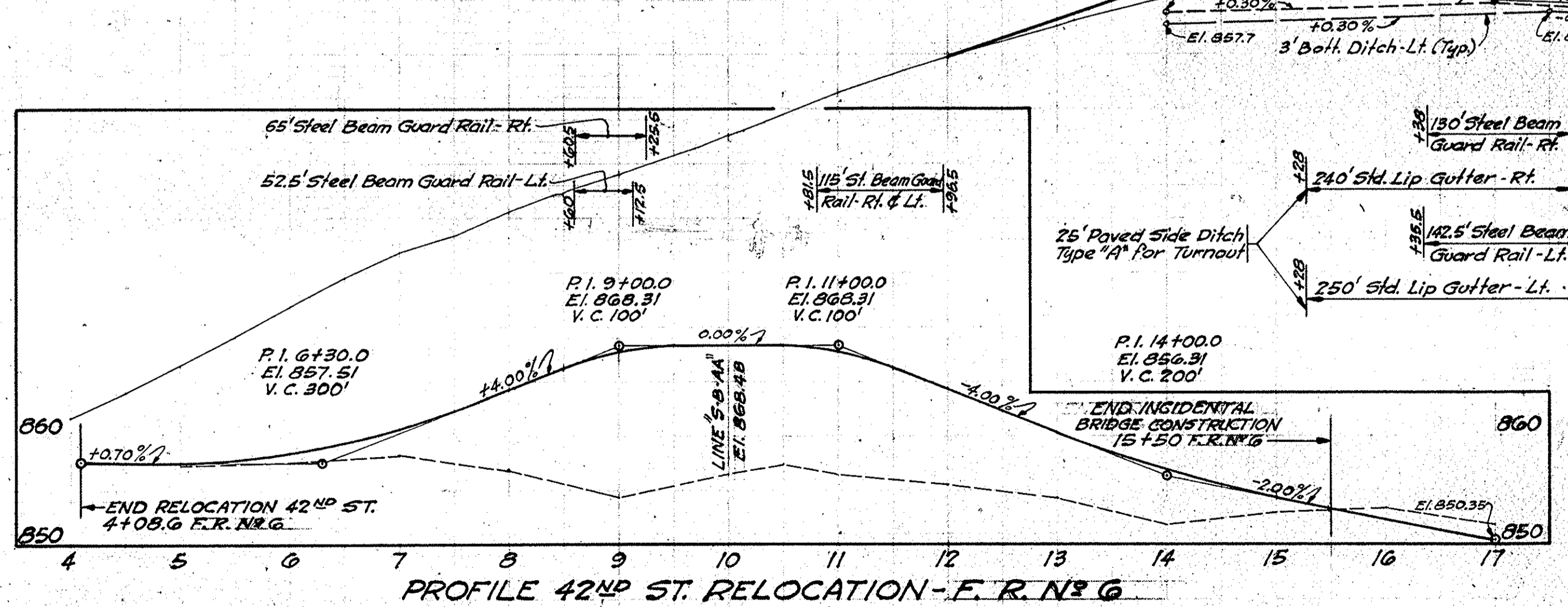
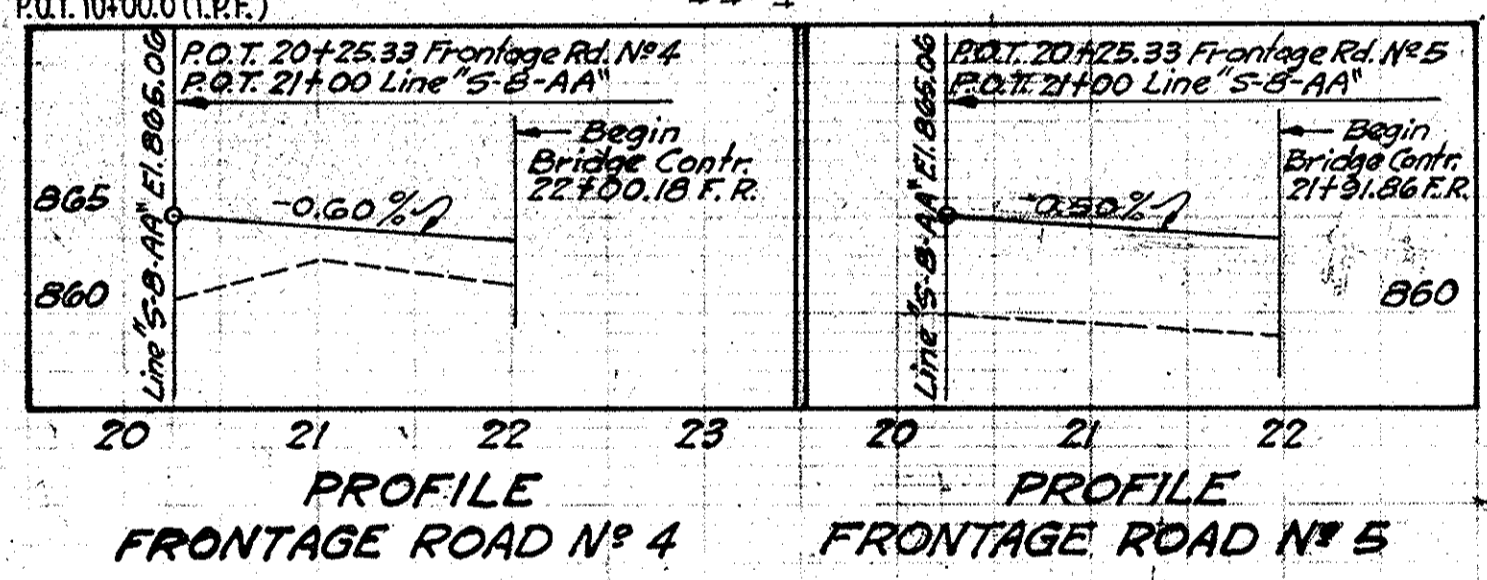


EARTHWORK SUMMARY	
Fill - Line "S-7-AA"	39,396 C.Y.
Fill - Frontage Road No. 2	4,245 C.Y.
Fill - Frontage Road No. 3	2,170 C.Y.
Total	45,813 C.Y.
Grade "B" Special Borrow	574 C.Y. + 25% = 9 C.Y. = 727 C.Y.
Total Fill	45,239 C.Y.
Fill + 20%	9,048 C.Y.
Common Excav.	54,287 C.Y.
	981 C.Y.
Surplus Foundation Excav.	53,356 C.Y.
Special borrow	53,246 C.Y.





ALL R/W ON THIS SHEET TO BE AS SHOWN.
 LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.

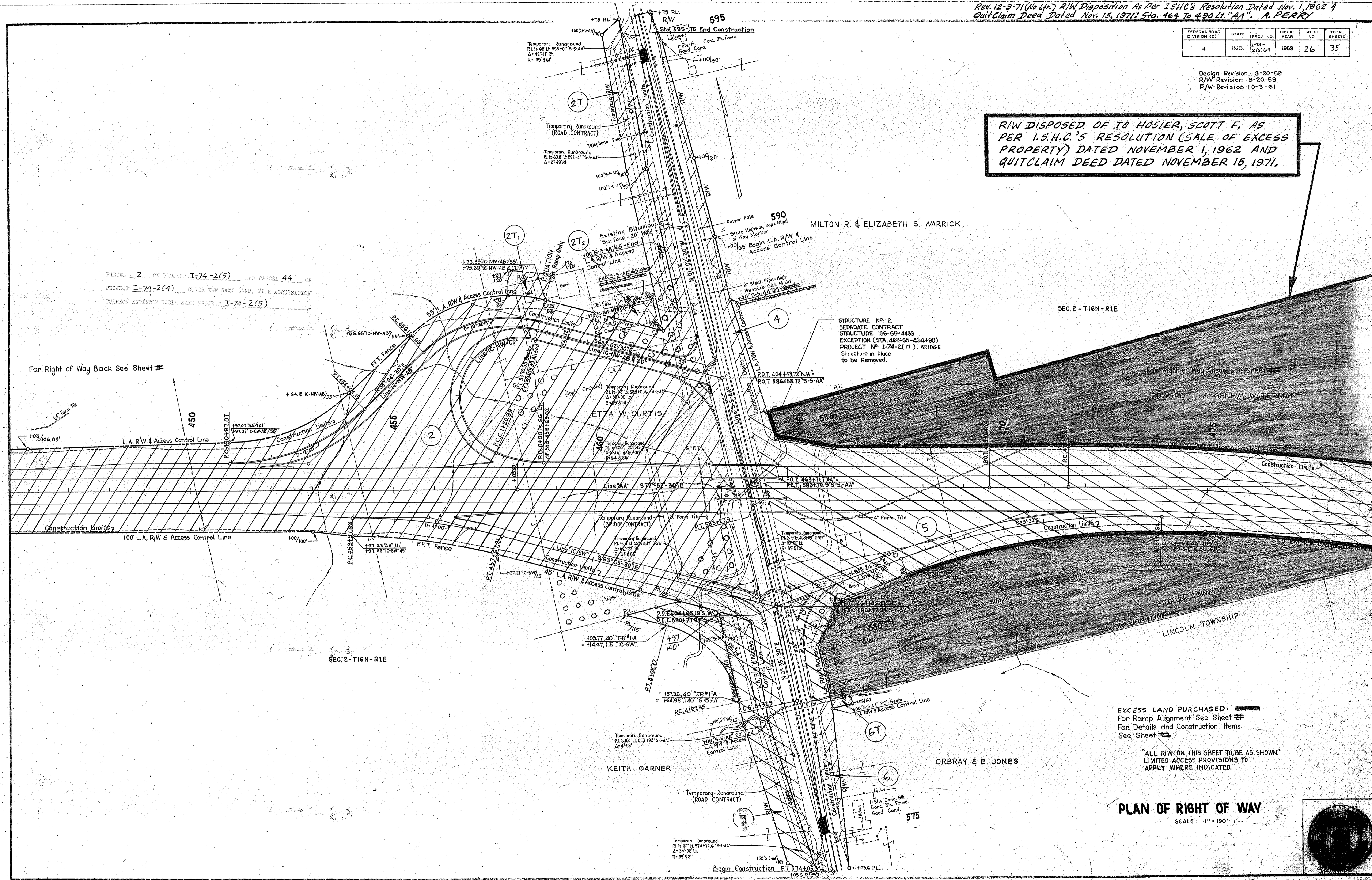


See LAYOUT Drwg. C1 for Layout of Guard Rail
 at Intersection of COUNTY LINE ROAD & 42ND ST.

FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(5)64	1959	26	35

Design Revision 3-20-59
 R/W Revision 3-20-59
 R/W Revision 10-3-61

RIW DISPOSED OF TO HOSIER, SCOTT F. AS PER I.S.H.C.'S RESOLUTION (SALE OF EXCESS PROPERTY) DATED NOVEMBER 1, 1962 AND QUITCLAIM DEED DATED NOVEMBER 15, 1971.



PARCELS 2 OF RECORD I-74-2(5) AND PARCEL 44 ON PROJECT I-74-2(4) COVER THE SAME LAND, WITH ACQUISITION THEREOF ENTIRELY UNDER SAID PROJECT I-74-2(5)

For Right of Way Back See Sheet #

EXCESS LAND PURCHASED:
 For Ramp Alignment See Sheet #
 For Details and Construction Items See Sheet #

ALL R/W ON THIS SHEET TO BE AS SHOWN LIMITED ACCESS PROVISIONS TO APPLY WHERE INDICATED.

PLAN OF RIGHT OF WAY
 SCALE: 1" = 100'

FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(5)A	1959	27	35

ADT 1955 2,080
ADT 1975 5,541
DHW 831
Dir. Factor 07
% Comm. 5

Design Revision 3-20-59

Point "B"

Approach	W.	Cap.	Est.
E-B	24'	700	582
A-B	24'	750	575
D-B	24'	750	340
A-B(Lt. Turn L)	14'	220	242
D-B(Lt. Turn L)	12'	175	183

ADT 1955 5,289
ADT 1975 15,708
DHW 1,728
Dir. Factor 06
% Comm. 7

ADT 1955 2,942
ADT 1975 7,767
DHW 893
Dir. Factor 06
% Comm. 10

Point "O"

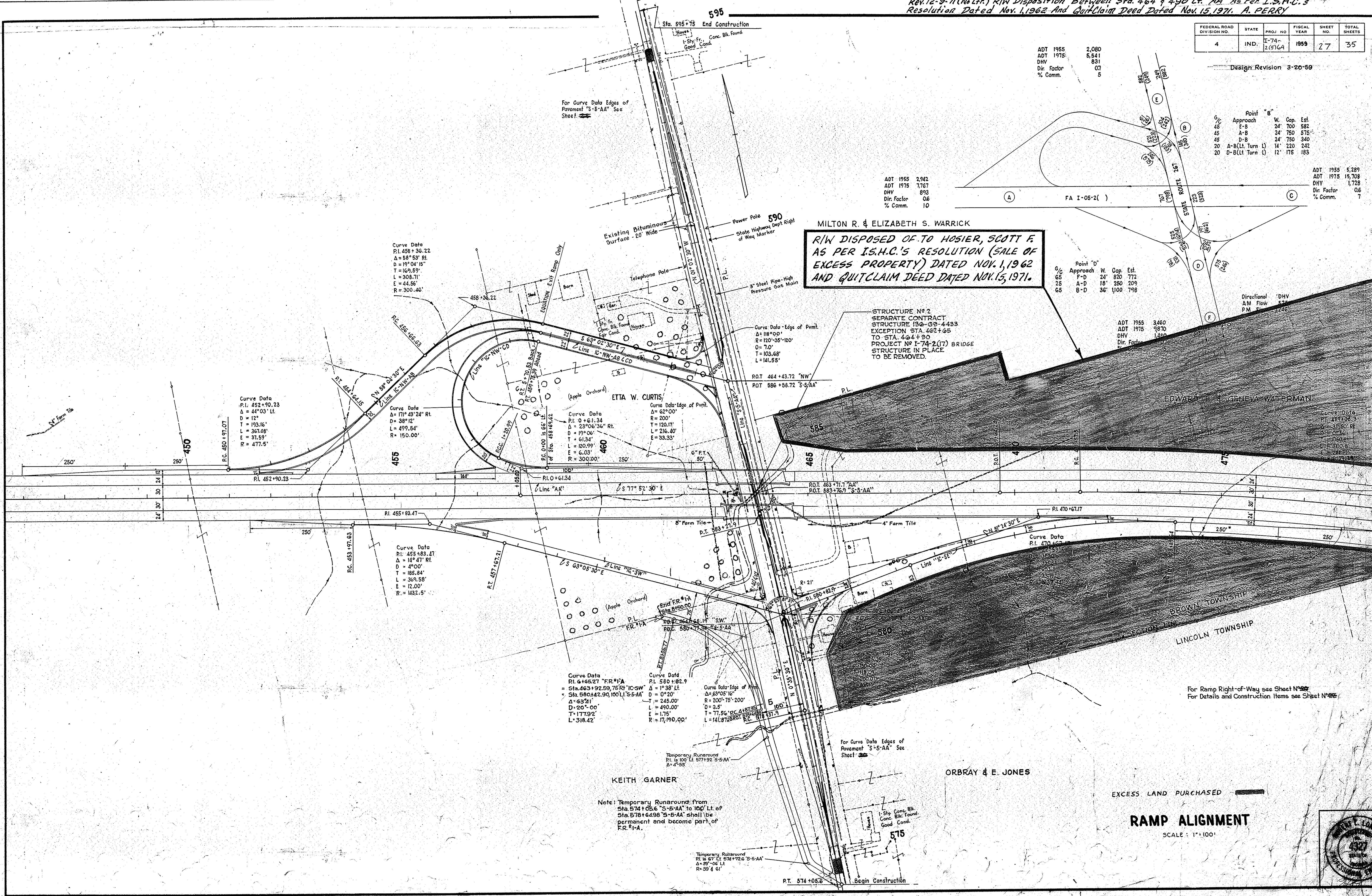
Approach	W.	Cap.	Est.
F-D	24'	820	772
A-D	18'	250	209
B-D	36'	1100	798

ADT 1955 3,460
ADT 1975 9,870
DHW 1,480
Dir. Factor 05
% Comm. 5

Directional AM Flow PM Flow
DHW 577 (246)

MILTON R. & ELIZABETH S. WARRICK
R/W DISPOSED OF TO HOSIER, SCOTT F. AS PER I.S.H.C.'S RESOLUTION (SALE OF EXCESS PROPERTY) DATED NOV. 1, 1962 AND QUITCLAIM DEED DATED NOV. 15, 1971.

STRUCTURE NO. 2 SEPARATE CONTRACT STRUCTURE 136-03-4433 EXCEPTION STA 462+65 TO STA 464+30 PROJECT NO I-74-2(17) BRIDGE STRUCTURE IN PLACE TO BE REMOVED.

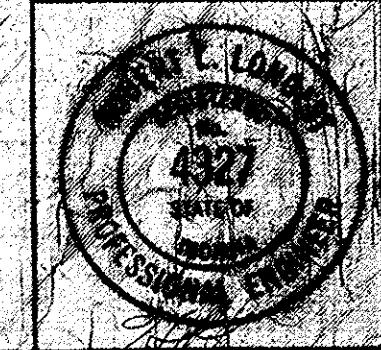


For Ramp Right-of-Way see Sheet No. 28
For Details and Construction Items see Sheet No. 29

EXCESS LAND PURCHASED

RAMP ALIGNMENT

SCALE: 1"=100'



FEDERAL ROAD DIVISION NO.	STATE	PROJ NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	E-74-2(5)64	1959	28	35

Design Revision 3-20-59
~~2-15-59~~ ~~Conduit Added~~

- STRUCTURE No 17 (451+80 'AA') Spl. Inlet, Type D6 & 36" of 12" Class I Pipe Req'd. 1-Hdwl. Req'd. Use Std. D-6 except A=4'-0"
- STRUCTURE No 18 (453+00 'AA') Std. Inlet, Type 'N' & 76" of 12" Class I Pipe Req'd. 1-Hdwl. Req'd.
- STRUCTURE No 19 (456+57 'AA') Spl. Inlet, Type D6, 46" of 12" Class I Pipe & 1-Hdwl. Req'd. Use Std. D-6 except A=4'-0"
- STRUCTURE No 20 (458+70 'AA') Std. D6 Inlet, 22" of 12" B.C.C.M. Pipe, 2-22'30" Bends & 1-Hdwl. Req'd.
- STRUCTURE No 21 (459+00 'AA') Std. D6 Inlet, 22" of 12" B.C.C.M. Pipe, 2-22'30" Bends & 1-Hdwl. Req'd.
- STRUCTURE No 22 (461+10 'AA') Std. D6 Inlet, 22" of 12" B.C.C.M. Pipe, 2-22'30" Bends & 1-Hdwl. Req'd.
- STRUCTURE No 23 (461+35 'AA') Std. D6 Inlet, 48" of 12" B.C.C.M. Pipe, 2-22'30" Bends & 1-Hdwl. Req'd.
- * STRUCTURE No 24 (463+05 'AA') 222' of 36" Class I Pipe on 13'10" Skew, and 1-L Hdwl. Req'd.
- * STRUCTURE No 26 (463+71.7 'AA') 209' of 8'x6' Box Culvert Req'd.

* BRIDGE CONTRACT

- STRUCTURE No 33 (457+33 'IC-NW-AB') 78' of 24" Class I Pipe on 25° Skew Req'd. Construct Inlet & Outlet Ditches.
- STRUCTURE No 34 (3400 'IC-NW-CD') Std. A3 Inlet, 26" of 12" B.C.C.M. Pipe, 2-22'30" Bends, & 1-Hdwl. Req'd.
- STRUCTURE No 35 (3+36 'IC-NW-CD') 82' of 24" Class I Pipe on 45° Skew Req'd. Construct Inlet & Outlet Ditches
- STRUCTURE No 36 (463+00 'IC-NW-AB & CD') 74' of 18" Class I Pipe Req'd.
- * STRUCTURE No 25 (462+95 'IC-SW') 88' of 114"x77" Structural Plate Pipe Arch and 2-Anchors Req'd. Skew 15° Rt.
- STRUCTURE No 37 (463+25 'IC-SW') Std. D6 Inlet, 34" of 12" B.C.C.M. Pipe, 2-22'30" Bends, & 1-Hdwl. Req'd. Construct Outlet Ditch
- STRUCTURE No 38 (463+50 'IC-SW') Std. D6 Inlet, 44" of 12" B.C.C.M. Pipe, 2-22'30" Bends, & 1-Hdwl. Req'd. Construct Outlet Ditch
- STRUCTURE No 39 (465+00 'IC-SE') 84' of 18" Class I Pipe & 1-A3 Inlet Req'd.

STANDARD PAVED GUTTER

Sta. 453+80 'IC-NW-AB' (Lt.)	11 Lft.
Sta. 459+75 'IC-NW-AB & CD' (Lt.)	11 Lft.
Sta. 459+75 'IC-NW-AB & CD' (Rt.)	11 Lft.
Sta. 463+00 'IC-NW-AB & CD' (Lt.)	14 Lft.
Sta. 463+00 'IC-NW-AB & CD' (Rt.)	11 Lft.
Sta. 463+22 'IC-NW-AB & CD' (Rt.)	11 Lft.
Sta. 463+71 'AA' (Lt.)	11 Lft.
Total	69 Lft.

STANDARD PAVED SIDE DITCH TYPE 'A'

Line 'AA' Sta. 461+04 to Sta. 462+78 (Rt.)	172 Lft.
Line 'IC-SW' Sta. 460+97 to Sta. 461+82 (Lt.)	185 Lft.
Line 'IC-SW' Sta. 461+40 to Sta. 462+79 (Rt.)	142 Lft.
Line 'IC-SE' Sta. 467+02 to Sta. 468+65 (Lt.)	184 Lft.
Sta. 579+58 (Rt.) 'S-5-AA' to Sta. 465+00 (Rt.) 'IC-SE'	82 Lft.
Sta. 453+80 'IC-NW-AB' (Lt.)	10 Lft.
Sta. 459+75 'IC-NW-AB & CD' (Lt.)	4 Lft.
Sta. 459+75 'IC-NW-AB & CD' (Rt.)	4 Lft.
Sta. 463+00 'IC-NW-AB & CD' (Lt.)	5 Lft.
Sta. 463+00 'IC-NW-AB & CD' (Rt.)	5 Lft.
Sta. 463+22 'IC-NW-AB & CD' (Rt.)	21 Lft.
Sta. 463+71 'AA' (Lt.)	4 Lft.
Total	819 Lft.

SPECIAL CONCRETE CENTER CURB

Sta. 459+75.39 to Sta. 463+61.5 Line 'IC-NW-AB & CD'	386 Lft. (Total)
--	------------------

BITUMINOUS CURS

Sta. 464+46 'AA' (Lt.) to Sta. 468+01 'AA' (Lt.)	355 Lft. (Total)
--	------------------

SPECIAL LIP GUTTER

Sta. 469+50 'AA' (Rt.)	36 Lft. (Total)
------------------------	-----------------

STEEL BEAM GUARD RAIL

Sta. 459+275 'AA' (Lt.) to Sta. 462+10 'AA' (Lt.)	342.5 Lft.
Sta. 459+295 'AA' (Rt.) to Sta. 462+10 'AA' (Rt.)	367.5 Lft.
Sta. 464+46 'AA' (Lt.) to Sta. 468+01 'AA' (Lt.)	355 Lft.
Sta. 464+73 'AA' (Rt.) to Sta. 467+655 'AA' (Rt.)	292.5 Lft.
Sta. 463+52 'IC-SW' (Rt.) to Sta. 463+92 'IC-SW' (Rt.)	40 Lft.
Sta. 1450 'IC-NW-CD' (Lt.) to Sta. 3400 'IC-NW-CD' (Lt.)	180 Lft.
Sta. 463+935 'IC-SW' (Lt.) to Sta. 581+405 'S-5-AA' (Lt.)	12.5 Lft.
Total	1590 Lft.

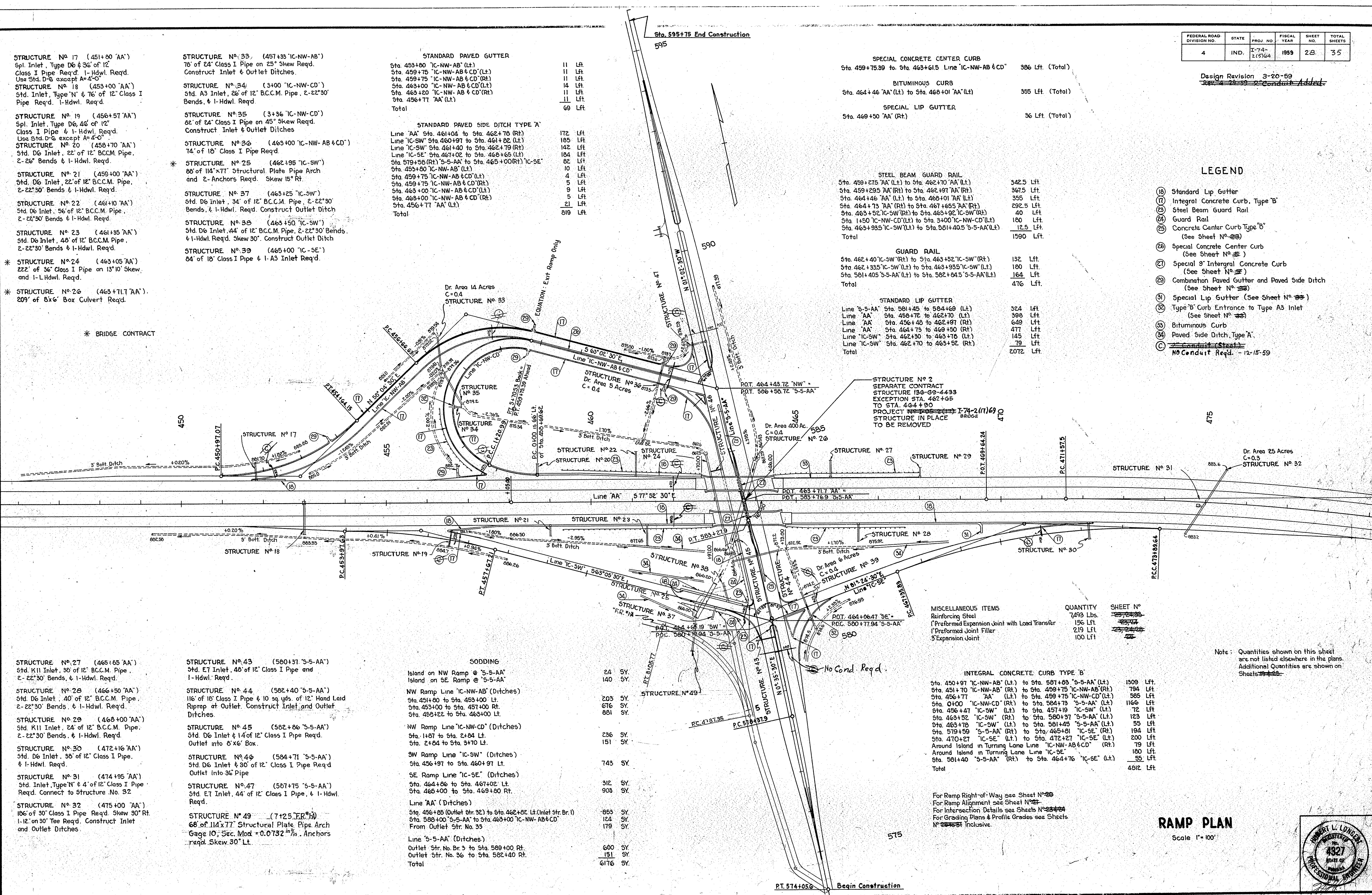
GUARD RAIL

Sta. 462+40 'IC-SW' (Rt.) to Sta. 463+52 'IC-SW' (Rt.)	132 Lft.
Sta. 462+335 'IC-SW' (Lt.) to Sta. 463+935 'IC-SW' (Lt.)	180 Lft.
Sta. 581+405 'S-5-AA' (Lt.) to Sta. 582+845 'S-5-AA' (Lt.)	164 Lft.
Total	476 Lft.

STANDARD LIP GUTTER

Line 'S-5-AA' Sta. 581+45 to 584+69 (Lt.)	324 Lft.
Line 'AA' Sta. 458+72 to 462+70 (Lt.)	398 Lft.
Line 'AA' Sta. 456+48 to 462+97 (Rt.)	649 Lft.
Line 'AA' Sta. 464+73 to 469+50 (Rt.)	477 Lft.
Line 'IC-SW' Sta. 462+30 to 463+70 (Lt.)	145 Lft.
Line 'IC-SW' Sta. 462+10 to 463+52 (Rt.)	79 Lft.
Total	2072 Lft.

- LEGEND
- (16) Standard Lip Gutter
 - (17) Integral Concrete Curb, Type 'B'
 - (23) Steel Beam Guard Rail
 - (24) Guard Rail
 - (25) Concrete Center Curb Type 'B' (See Sheet No. 28)
 - (26) Special Concrete Center Curb (See Sheet No. 28)
 - (27) Special 9" Integral Concrete Curb (See Sheet No. 28)
 - (28) Combination Paved Gutter and Paved Side Ditch (See Sheet No. 28)
 - (31) Special Lip Gutter (See Sheet No. 28)
 - (32) Type 'B' Curb Entrance to Type A3 Inlet (See Sheet No. 28)
 - (33) Bituminous Curb
 - (34) Paved Side Ditch, Type 'A'
 - (C) ~~Conduit (Street)~~
No Conduit Req'd. - 12-15-59



MISCELLANEOUS ITEMS

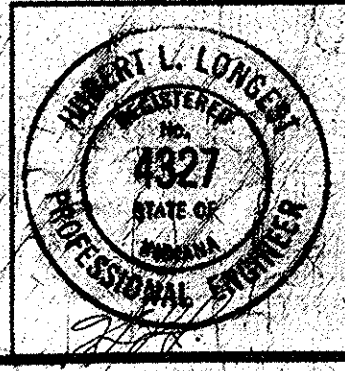
ITEM	QUANTITY	SHEET NO.
Reinforcing Steel	7493 Lbs.	28, 29, 30, 31, 32, 33, 34, 35
1" Preformed Expansion Joint with Load Transfer	156 Lft.	28, 29, 30, 31, 32, 33, 34, 35
1" Preformed Joint Filler	219 Lft.	28, 29, 30, 31, 32, 33, 34, 35
3" Expansion Joint	100 Lft.	28, 29, 30, 31, 32, 33, 34, 35

INTEGRAL CONCRETE CURB TYPE 'B'

Sta. 450+97 'IC-NW-AB' (Lt.) to Sta. 501+03 'S-5-AA' (Lt.)	1509 Lft.
Sta. 451+70 'IC-NW-AB' (Rt.) to Sta. 459+75 'IC-NW-AB' (Rt.)	794 Lft.
Sta. 456+77 'AA' (Lt.) to Sta. 459+75 'IC-NW-CD' (Lt.)	585 Lft.
Sta. 0100 'IC-NW-CD' (Rt.) to Sta. 584+73 'S-5-AA' (Lt.)	1166 Lft.
Sta. 456+47 'IC-SW' (Lt.) to Sta. 457+19 'IC-SW' (Lt.)	72 Lft.
Sta. 463+52 'IC-SW' (Rt.) to Sta. 580+37 'S-5-AA' (Lt.)	123 Lft.
Sta. 463+70 'IC-SW' (Lt.) to Sta. 581+45 'S-5-AA' (Lt.)	55 Lft.
Sta. 579+59 'S-5-AA' (Rt.) to Sta. 465+81 'IC-SE' (Rt.)	194 Lft.
Sta. 470+27 'IC-SE' (Lt.) to Sta. 472+27 'IC-SE' (Lt.)	200 Lft.
Around Island in Turning Lane Line 'IC-NW-AB & CD' (Rt.)	79 Lft.
Around Island in Turning Lane Line 'IC-SE'	180 Lft.
Sta. 581+40 'S-5-AA' (Rt.) to Sta. 464+76 'IC-SE' (Lt.)	55 Lft.
Total	4812 Lft.

For Ramp Right-of-Way see Sheet No. 28
 For Ramp Alignment see Sheet No. 28
 For Intersection Details see Sheets No. 28 & 29
 For Grading Plans & Profile Grades see Sheets No. 28 & 29 Inclusive.

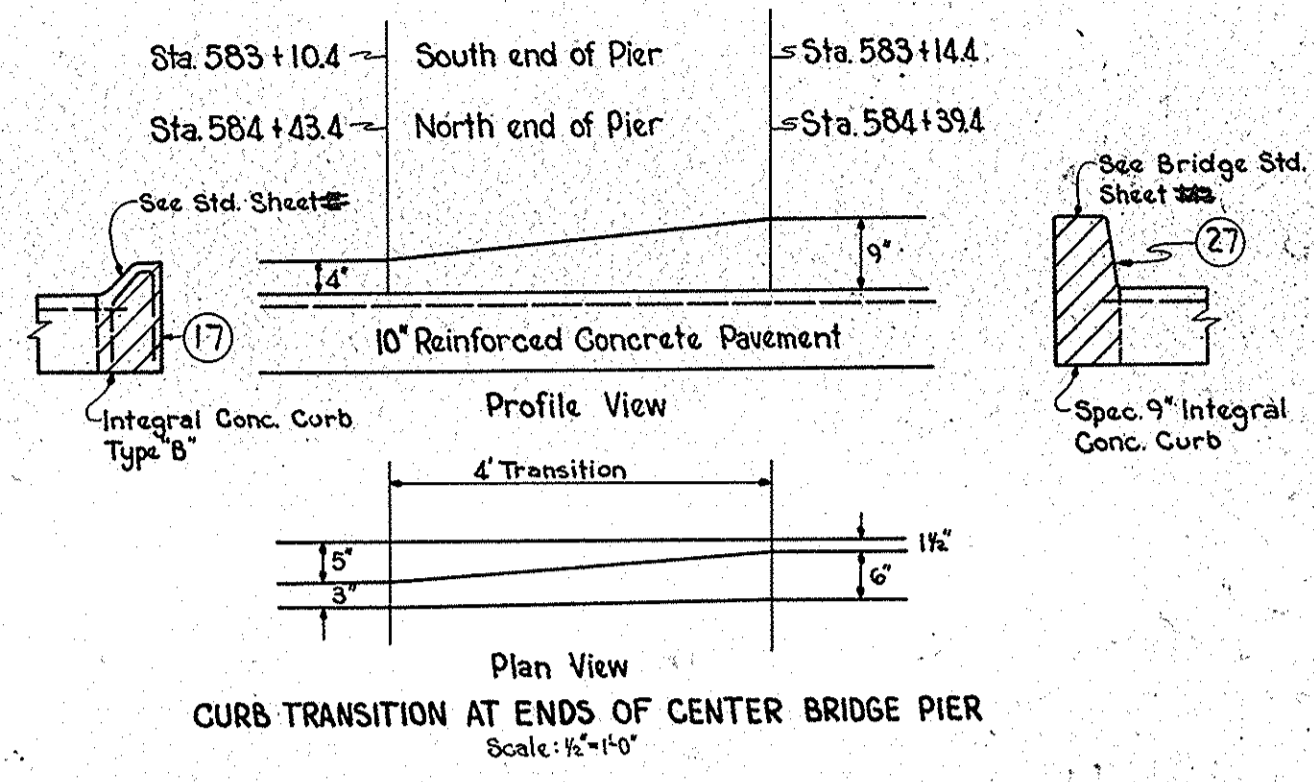
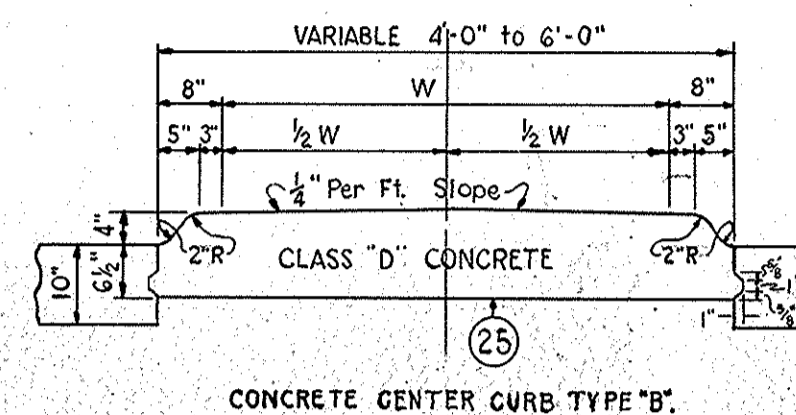
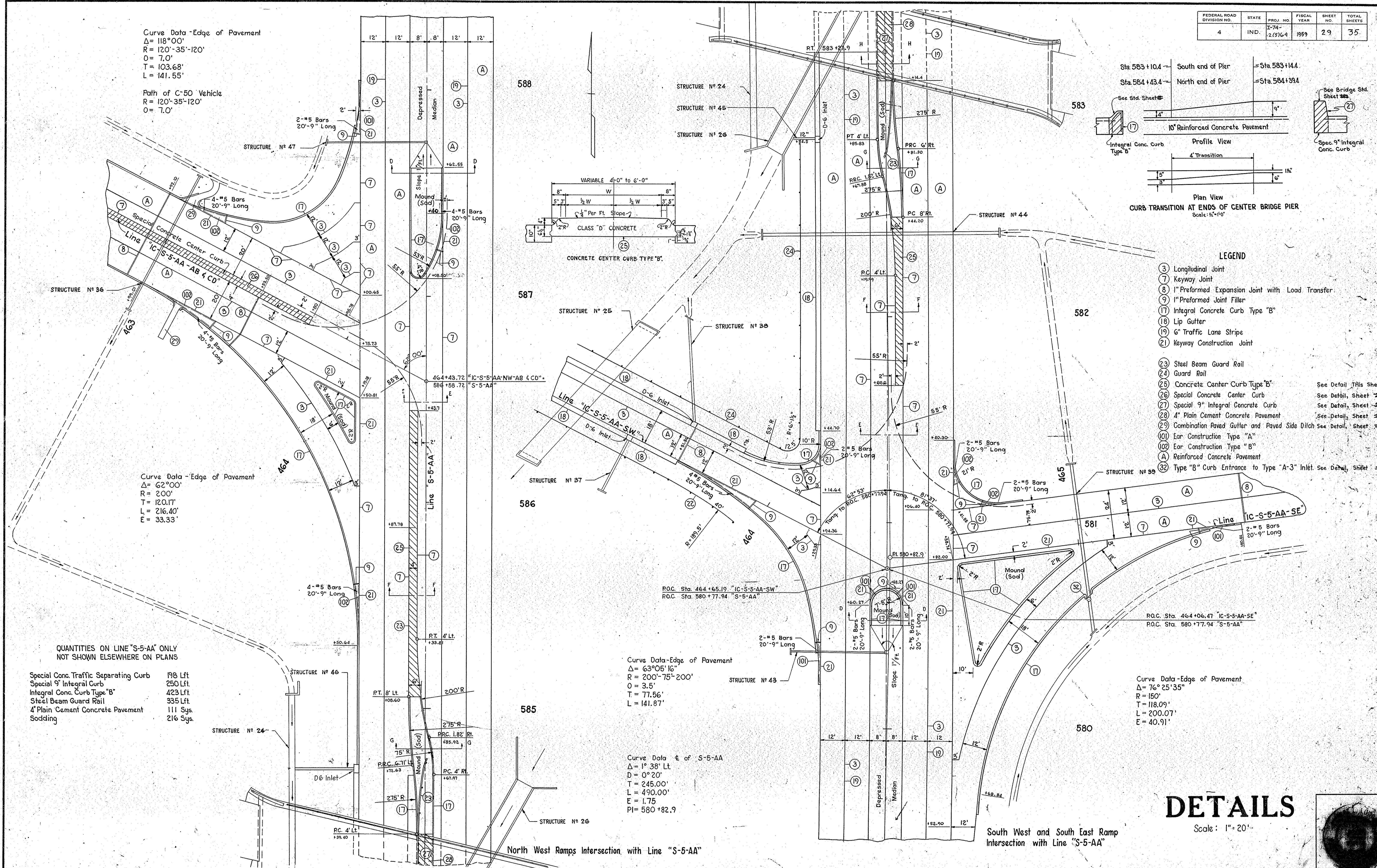
RAMP PLAN
 Scale 1"=100'



FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	E-74-2.19/64	1969	29	35

Curve Data -Edge of Pavement
 $\Delta = 118^{\circ}00'$
 $R = 120'-35"-120'$
 $O = 7.0'$
 $T = 103.68'$
 $L = 141.55'$

Path of C-50 Vehicle
 $R = 120'-35"-120'$
 $O = 7.0'$



- LEGEND**
- (3) Longitudinal Joint
 - (7) Keyway Joint
 - (8) 1" Preformed Expansion Joint with Load Transfer
 - (9) 1" Preformed Joint Filler
 - (17) Integral Concrete Curb Type "B"
 - (18) Lip Gutter
 - (19) 6" Traffic Lane Stripes
 - (21) Keyway Construction Joint
 - (23) Steel Beam Guard Rail
 - (24) Guard Rail
 - (25) Concrete Center Curb Type "B"
 - (26) Special Concrete Center Curb
 - (27) Special 9" Integral Concrete Curb
 - (28) 4" Plain Cement Concrete Pavement
 - (29) Combination Paved Gutter and Paved Side Ditch
 - (101) Ear Construction Type "A"
 - (102) Ear Construction Type "B"
 - (A) Reinforced Concrete Pavement
 - (B) Type "B" Curb Entrance to Type "A-3" Inlet

QUANTITIES ON LINE "S-5-AA" ONLY
 NOT SHOWN ELSEWHERE ON PLANS

- Special Conc. Traffic Separating Curb 198 Lft
- Special 9" Integral Curb 250 Lft
- Integral Conc. Curb Type "B" 423 Lft
- Steel Beam Guard Rail 335 Lft
- 4" Plain Cement Concrete Pavement 111 Sys.
- Sodding 216 Sys.

Curve Data -Edge of Pavement
 $\Delta = 62^{\circ}00'$
 $R = 200'$
 $T = 120.17'$
 $L = 216.40'$
 $E = 33.33'$

Curve Data -Edge of Pavement
 $\Delta = 63^{\circ}05'16''$
 $R = 200'-75"-200'$
 $O = 3.5'$
 $T = 77.56'$
 $L = 141.87'$

Curve Data & of S-5-AA
 $\Delta = 1^{\circ}38' Lt.$
 $D = 0^{\circ}20'$
 $T = 245.00'$
 $L = 490.00'$
 $E = 1.75'$
 $PI = 580+82.9$

Curve Data -Edge of Pavement
 $\Delta = 76^{\circ}25'35''$
 $R = 150'$
 $T = 118.09'$
 $L = 200.07'$
 $E = 40.91'$

DETAILS

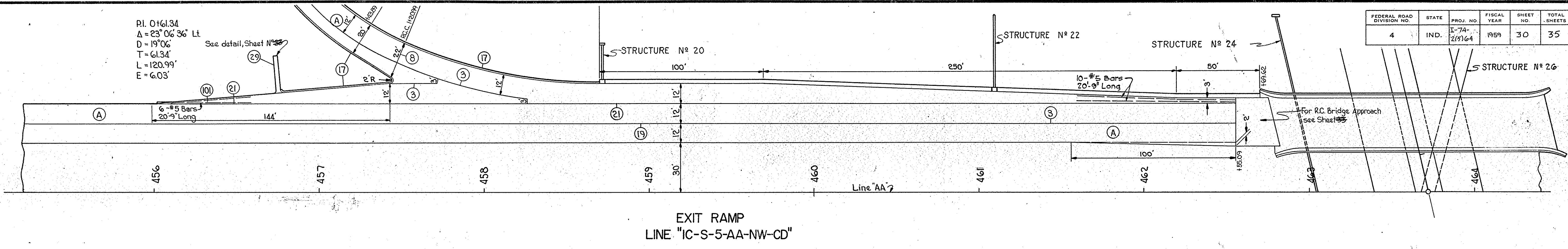
Scale: 1" = 20'

South West and South East Ramp
 Intersection with Line "S-5-AA"

North West Ramps Intersection with Line "S-5-AA"

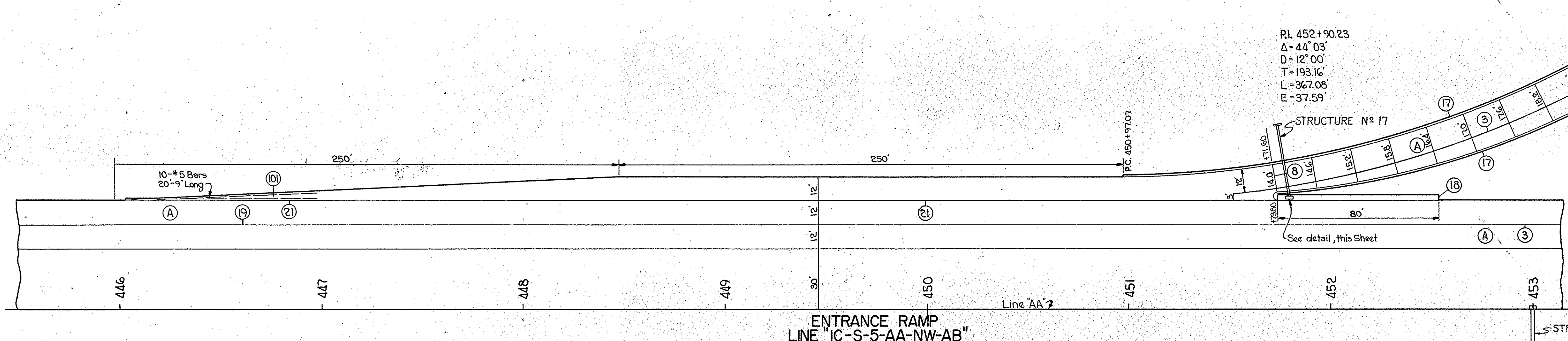
FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(1)64	1959	30	35

P.I. 0+61.34
 $\Delta = 23^{\circ}06'36''$ Lt
 $D = 197.06'$
 $T = 61.34'$
 $L = 120.99'$
 $E = 6.03'$

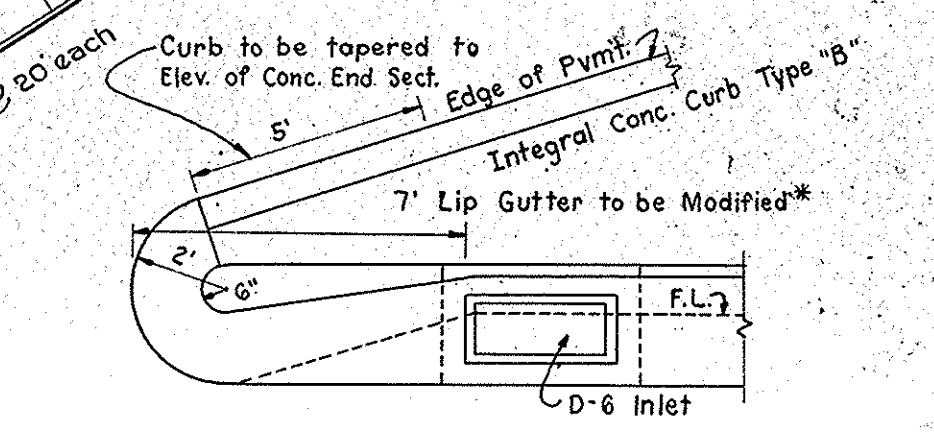


EXIT RAMP
 LINE "IC-S-5-AA-NW-CD"

P.I. 452+90.23
 $\Delta = 44^{\circ}03'$
 $D = 12^{\circ}00'$
 $T = 193.16'$
 $L = 367.05'$
 $E = 37.59'$

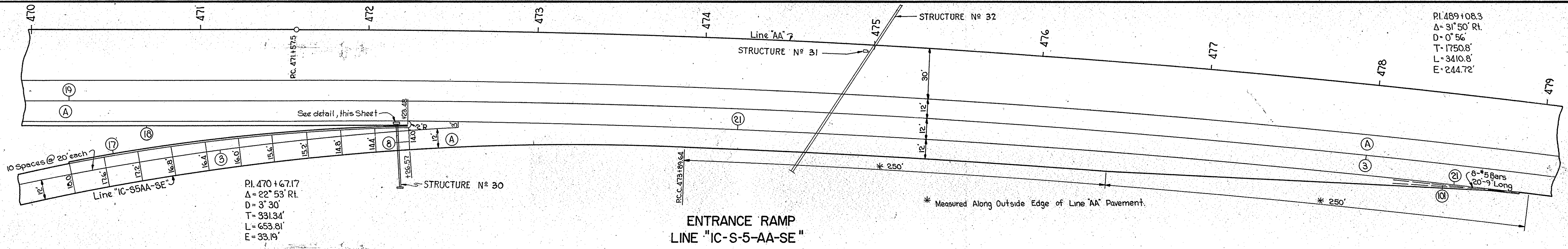


ENTRANCE RAMP
 LINE "IC-S-5-AA-NW-AB"



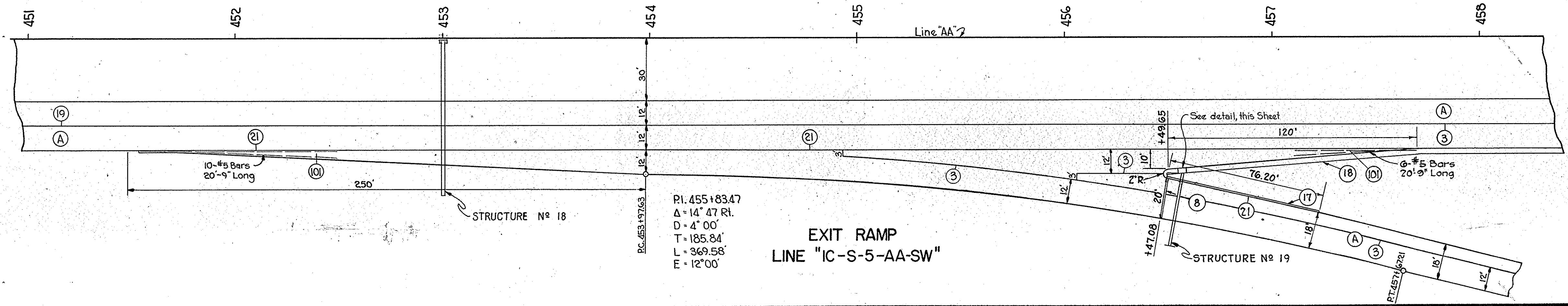
* Modified Lip Gutter to be paid for as 10 Lft. of Std. Lip Gutter.
 Scale 1/4" = 1'-0"

P.I. 489+08.3
 $\Delta = 31^{\circ}50'$ Rt
 $D = 0^{\circ}56'$
 $T = 1750.8'$
 $L = 3410.8'$
 $E = 244.72'$



ENTRANCE RAMP
 LINE "IC-S-5-AA-SE"

* Measured Along Outside Edge of Line "AA" Pavement.



EXIT RAMP
 LINE "IC-S-5-AA-SW"

- (3) Longitudinal Joint
- (8) Preformed Expansion Joint with Load Transfer
- (9) Preformed Joint Filler
- (17) Integral Concrete Curb Type "B"
- (18) Lip Gutter
- (19) Traffic Lane Stripe
- (21) Keyway Construction Joint
- (A) Reinforced Concrete Pavement
- (B) Ear Construction Type "A"

DETAILS
 LINE "AA"
 SCALE: 1/2" = 30'



CURVE DATA — PAVEMENT EDGES — LINE "S-5-AA"

Outside Edges (VII) (IX) (X) (XI)
 $\Delta = 3^{\circ}33'03''$
 $D = 1^{\circ}00'$
 $T = 177.61'$
 $L = 355.07'$
 $E = 2.75'$

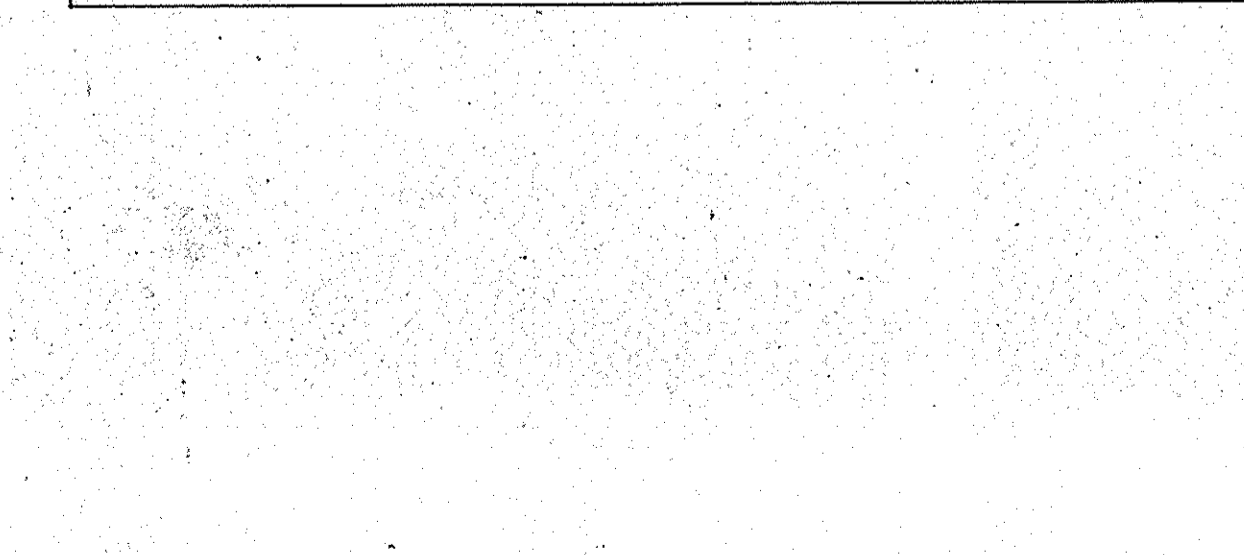
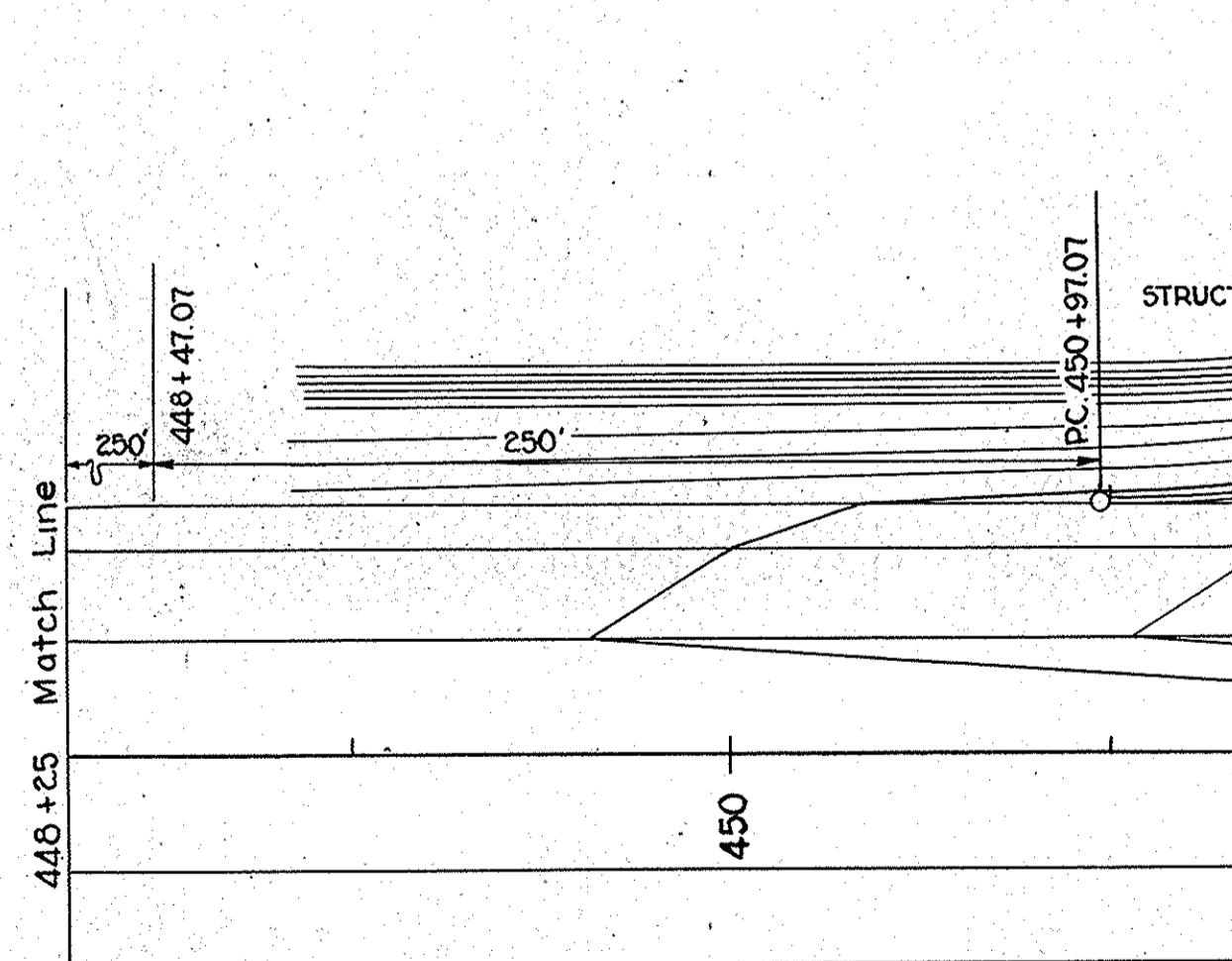
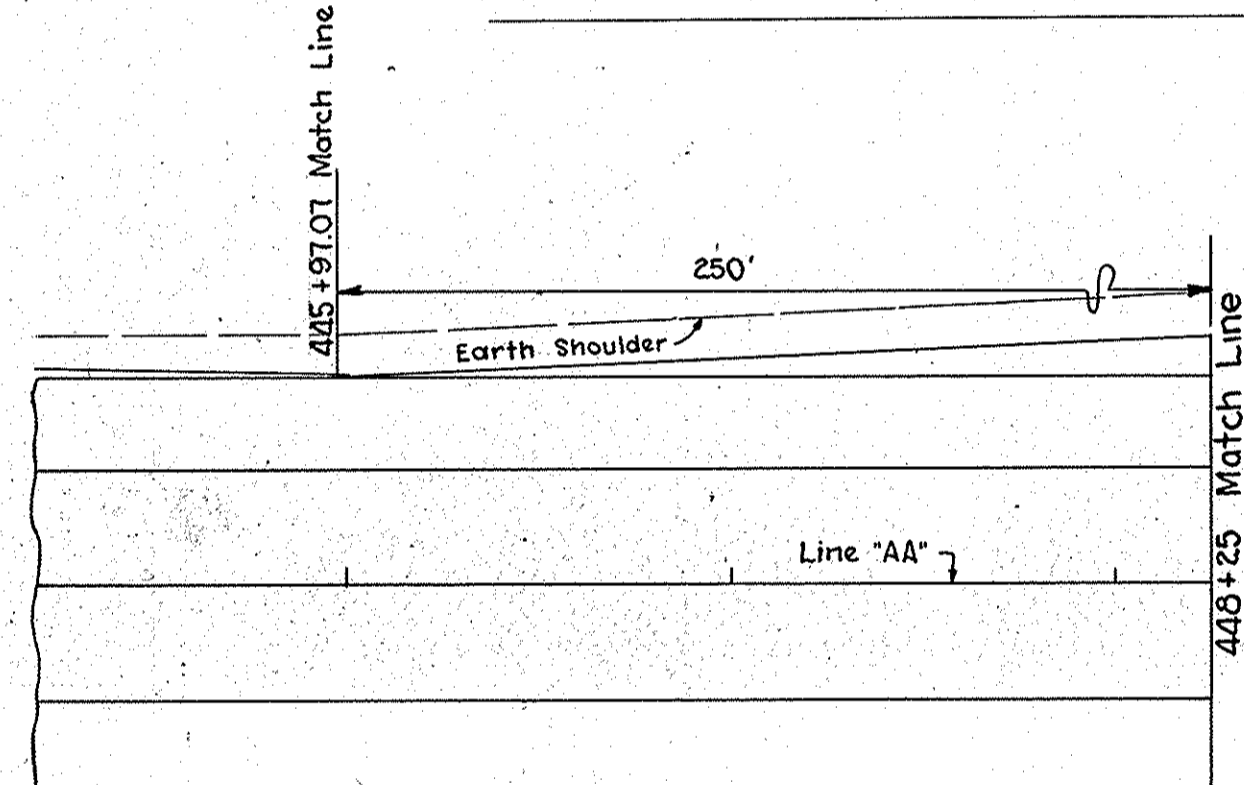
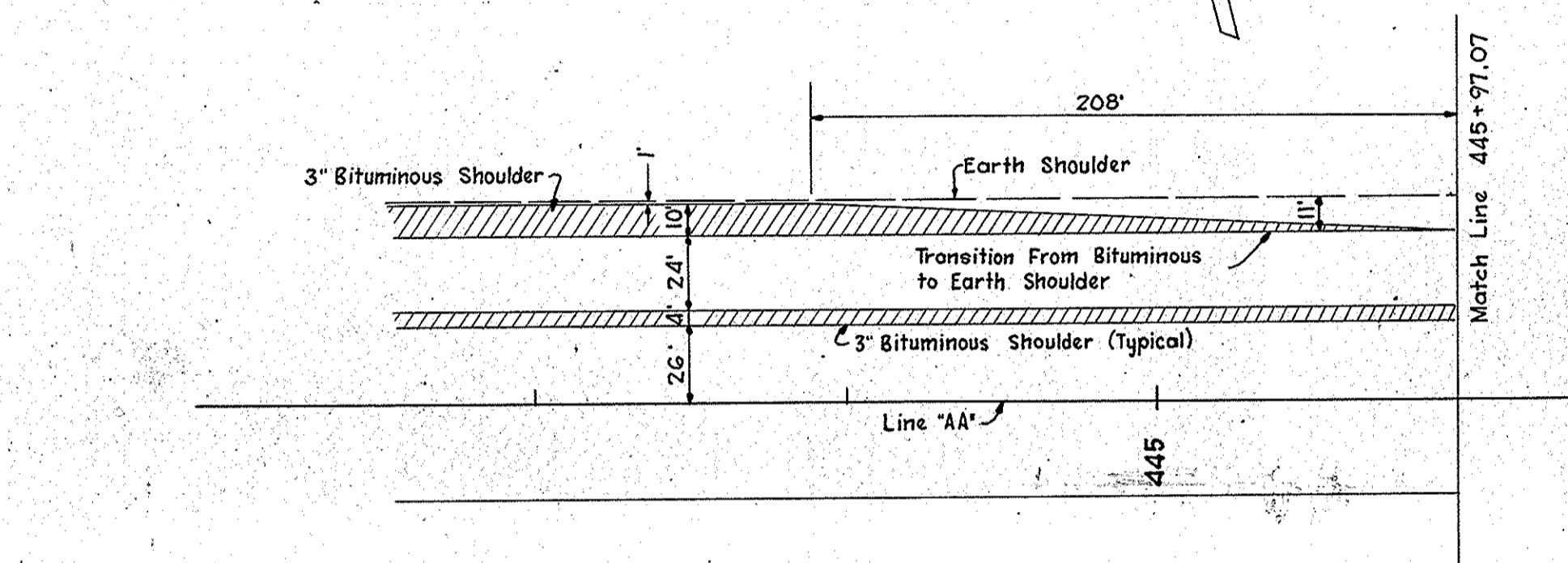
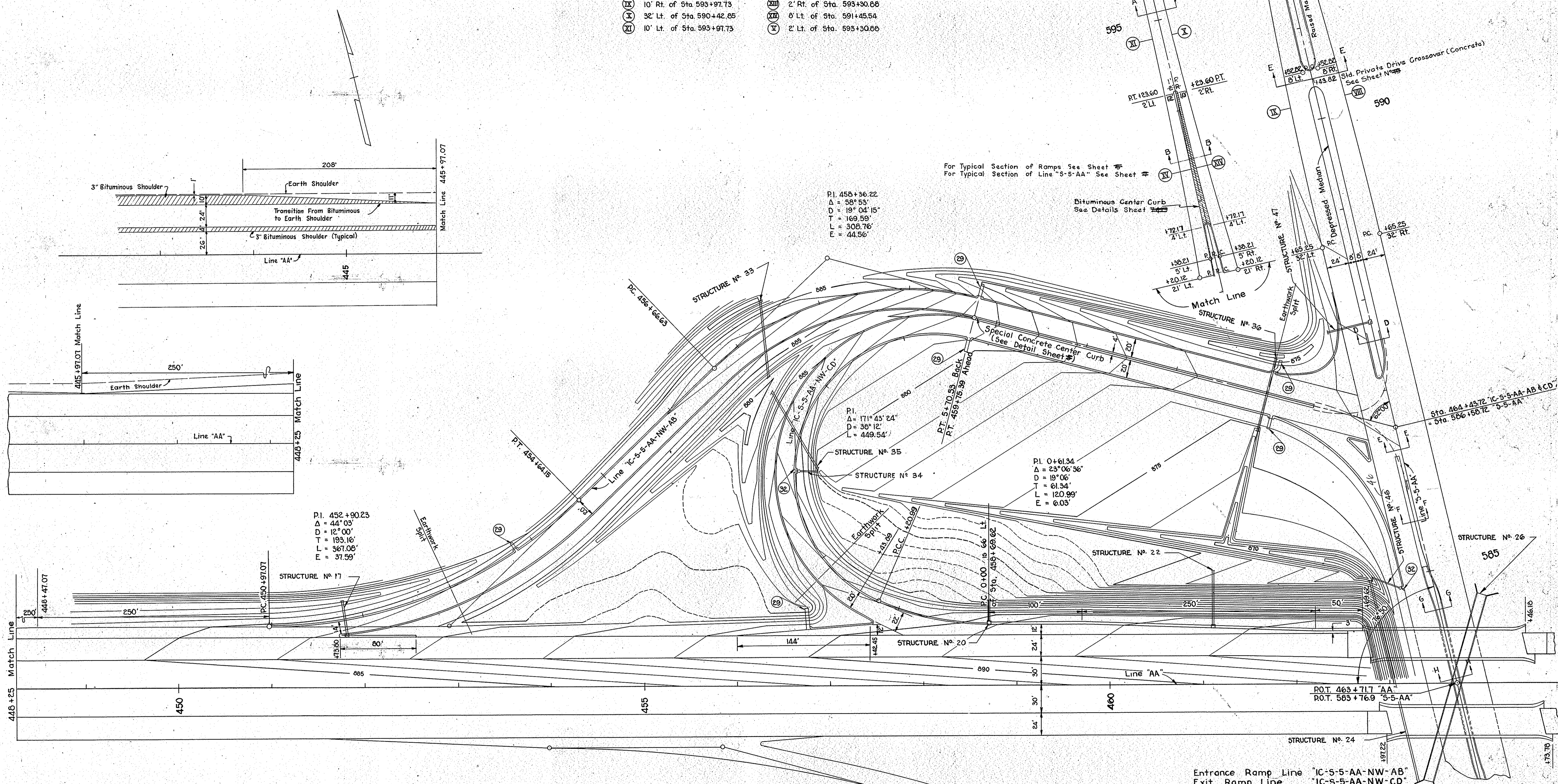
Inside Edges (XII) (XIII) (XIV) (XV)
 $\Delta = 1^{\circ}51'15''$
 $D = 1^{\circ}00'$
 $T = 92.72'$
 $L = 165.42'$
 $E = 0.75'$

P.I. LOCATIONS

- (VII) 32' Rt. of Sta. 590+42.85
- (IX) 10' Rt. of Sta. 593+97.73
- (X) 32' Lt. of Sta. 590+42.85
- (XI) 10' Lt. of Sta. 593+97.73
- (XII) 8' Rt. of Sta. 591+45.54
- (XIII) 2' Rt. of Sta. 593+30.88
- (XIV) 8' Lt. of Sta. 591+45.54
- (XV) 2' Lt. of Sta. 593+30.88

FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(1)64	1959	31	35

Design Revision
 March 20, 1959



For Typical Section of Ramps See Sheet #
 For Typical Section of Line "S-5-AA" See Sheet #

P.I. 456+36.22
 $\Delta = 58^{\circ}53'$
 $D = 19^{\circ}04'15''$
 $T = 169.59'$
 $L = 306.76'$
 $E = 44.56'$

P.I. 5170+33.24
 $\Delta = 171^{\circ}43'24''$
 $D = 38^{\circ}12'$
 $L = 449.54'$

P.I. 0+61.34
 $\Delta = 23^{\circ}06'36''$
 $D = 19^{\circ}06'$
 $L = 61.34'$
 $T = 120.99'$
 $E = 0.03'$

Entrance Ramp Line "IC-S-5-AA-NW-AB"
 Exit Ramp Line "IC-S-5-AA-NW-CD"

For Ramp Profile See Sheet #

DETAILS

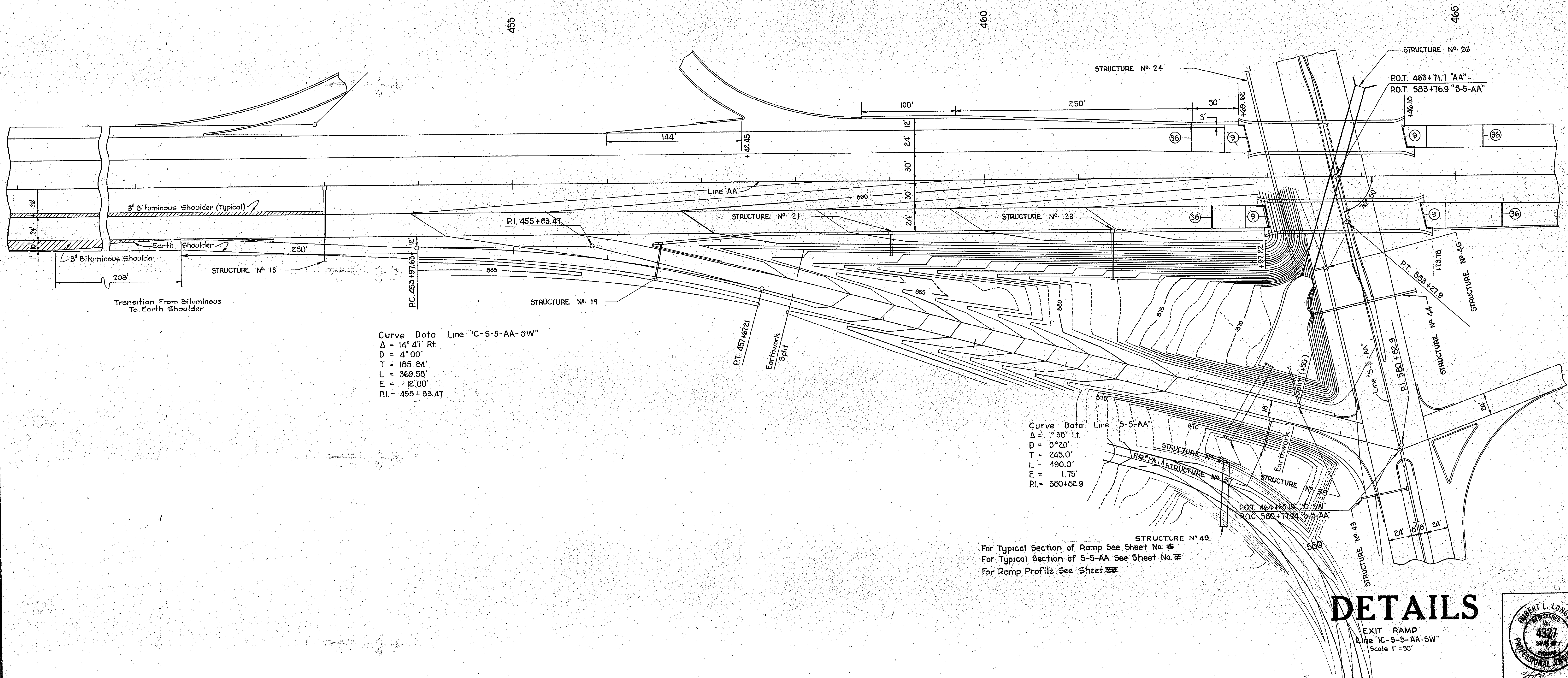
GRADING PLAN
 Scale 1" = 50'



FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-74-2(1)64	1959	32	35

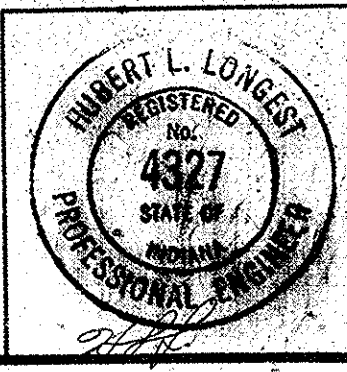
Design Revision 3/25/59

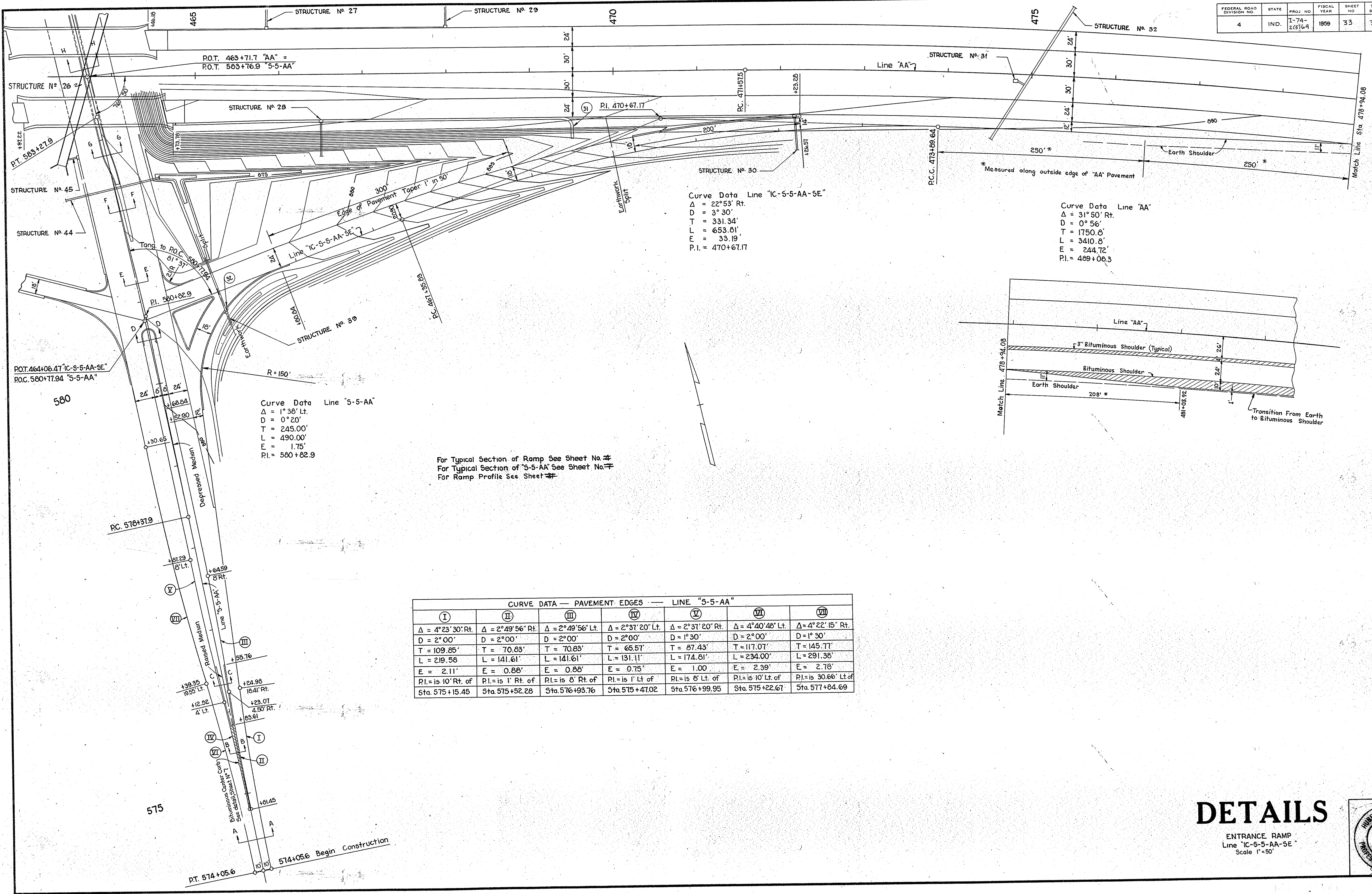
LEGEND
 (9) 1" Preformed Joint Filler
 (36) 3" Expansion Joint
 (Quantities on Sheet 33)



For Typical Section of Ramp See Sheet No. 34
 For Typical Section of S-5-AA See Sheet No. 35
 For Ramp Profile See Sheet 33

DETAILS
 EXIT RAMP
 Line "IC-5-5-AA-SW"
 Scale 1" = 50'





Curve Data Line "IC-5-5-AA-SE"

Δ = 22°53' Rt.
D = 3° 30'
T = 331.34'
L = 653.81'
E = 33.19'
P.I. = 470+67.17

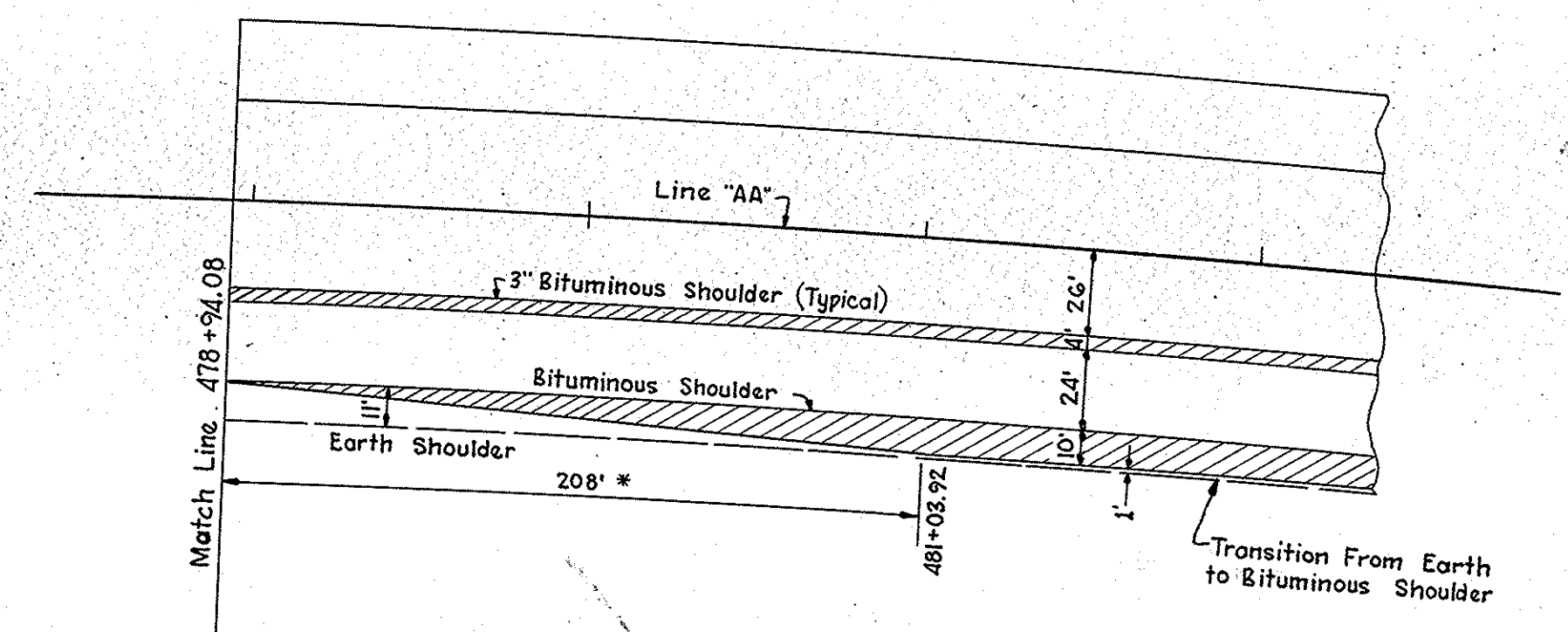
Curve Data Line "AA"

Δ = 31°50' Rt.
D = 0° 56'
T = 1750.8'
L = 3410.8'
E = 244.72'
P.I. = 489+06.3

Curve Data Line "5-5-AA"

Δ = 1° 38' Lt.
D = 0° 20'
T = 245.00'
L = 490.00'
E = 1.75'
P.I. = 560+82.9

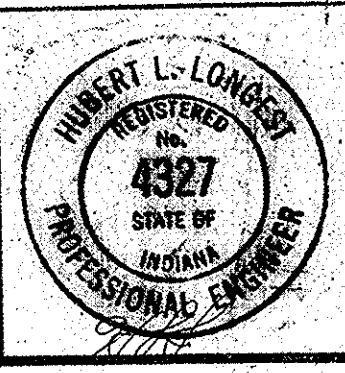
For Typical Section of Ramp See Sheet No. 34
For Typical Section of "5-5-AA" See Sheet No. 37
For Ramp Profile See Sheet 38



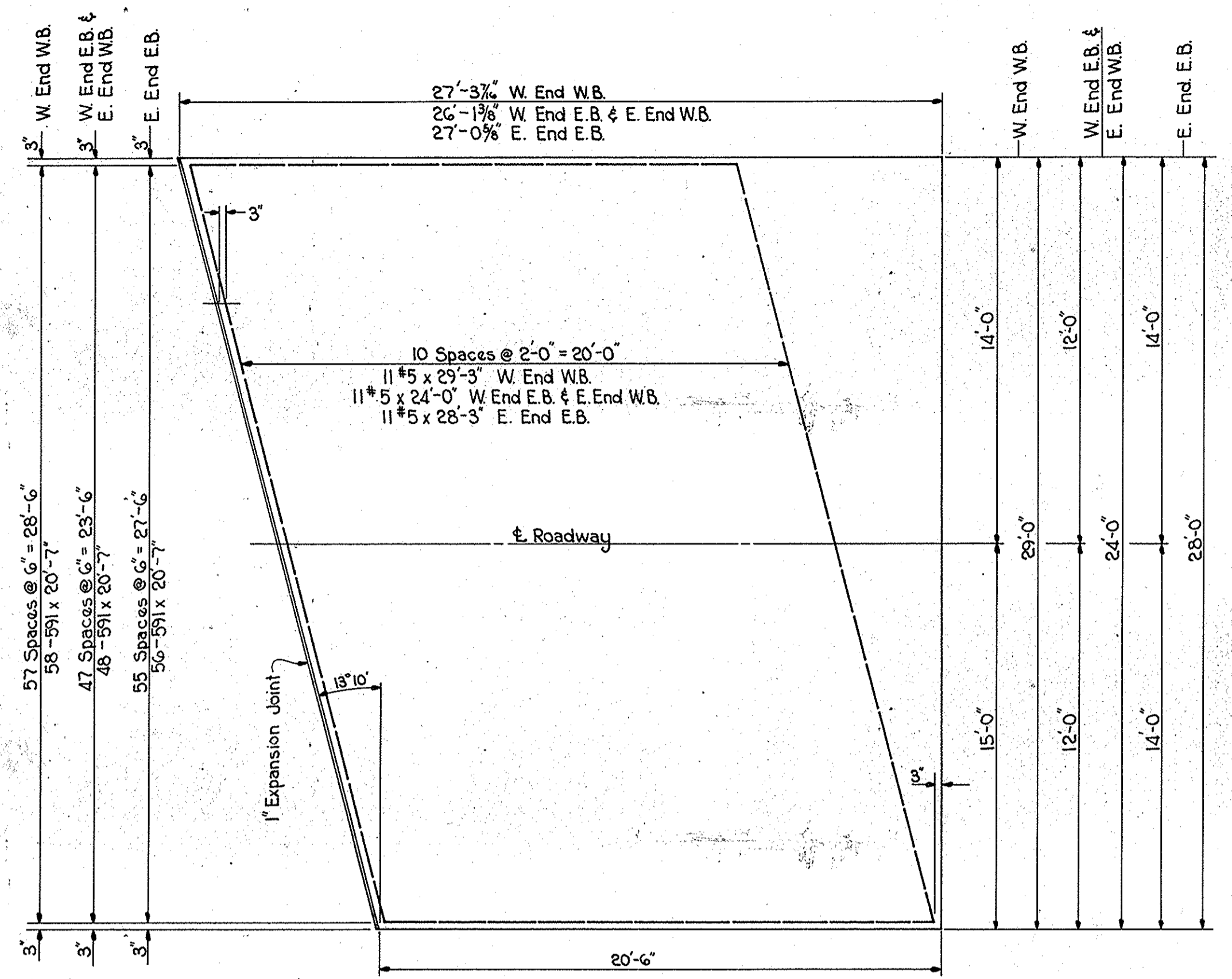
CURVE DATA — PAVEMENT EDGES — LINE "5-5-AA"						
I	II	III	IV	V	VI	VII
Δ = 4°23' 30" Rt.	Δ = 2°49' 56" Rt.	Δ = 2°49' 56" Lt.	Δ = 2°31' 20" Lt.	Δ = 2°31' 20" Rt.	Δ = 4°40' 46" Lt.	Δ = 4°22' 15" Rt.
D = 2° 00'	D = 2° 00'	D = 2° 00'	D = 2° 00'	D = 1° 30'	D = 2° 00'	D = 1° 30'
T = 109.85'	T = 70.83'	T = 70.83'	T = 65.57'	T = 87.43'	T = 117.07'	T = 145.77'
L = 219.58'	L = 141.61'	L = 141.61'	L = 131.11'	L = 174.81'	L = 234.00'	L = 291.38'
E = 2.11'	E = 0.88'	E = 0.88'	E = 0.75'	E = 1.00'	E = 2.39'	E = 2.78'
P.I. = is 10' Rt. of Sta. 575+15.45	P.I. = is 1' Rt. of Sta. 575+52.28	P.I. = is 8' Rt. of Sta. 576+93.76	P.I. = is 1' Lt. of Sta. 575+47.02	P.I. = is 6' Lt. of Sta. 576+99.95	P.I. = is 10' Lt. of Sta. 575+22.67	P.I. = is 30.66' Lt. of Sta. 577+84.69

DETAILS

ENTRANCE RAMP
Line "IC-5-5-AA-SE"
Scale 1"=50'

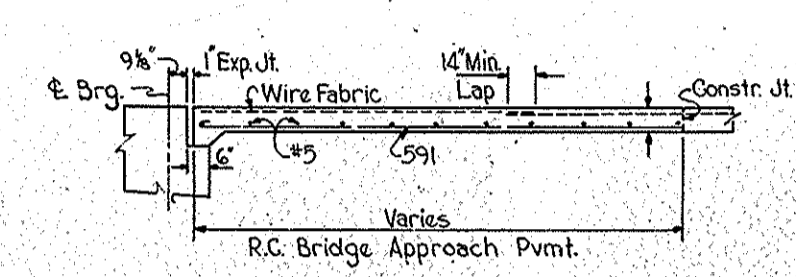


FEDERAL ROAD DIVISION NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	2-74-2(5)64	1959	34	35

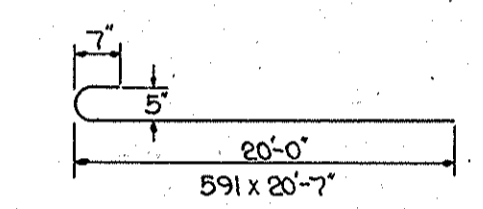


STRUCTURE N°2
R.C. BRIDGE APPROACH
Scale: 1/4" = 1'-0"

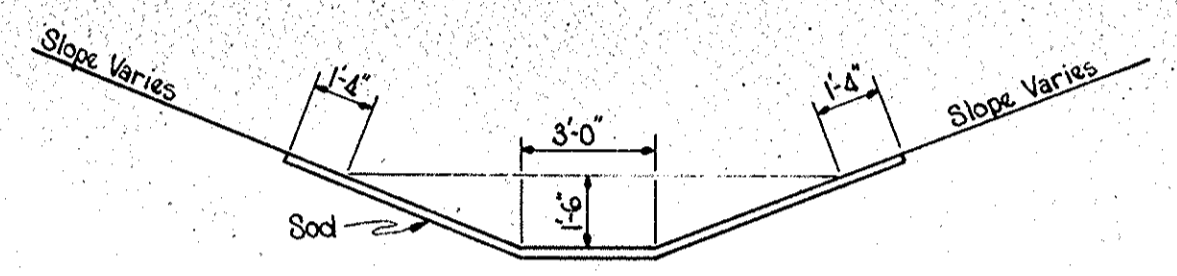
REINFORCING STEEL				
Size & Mark	N° of Bars	Length	Weight	Lane
#5	104	20'-7"		Eastbound
#5	11	28'-3"		"
#5	11	24'-0"		"
#5	106	20'-7"		Westbound
#5	11	29'-3"		"
#5	11	24'-0"		"
TOTAL	STEEL		5719 #	
R.C. Pavement				275.2 Sq. Yds.



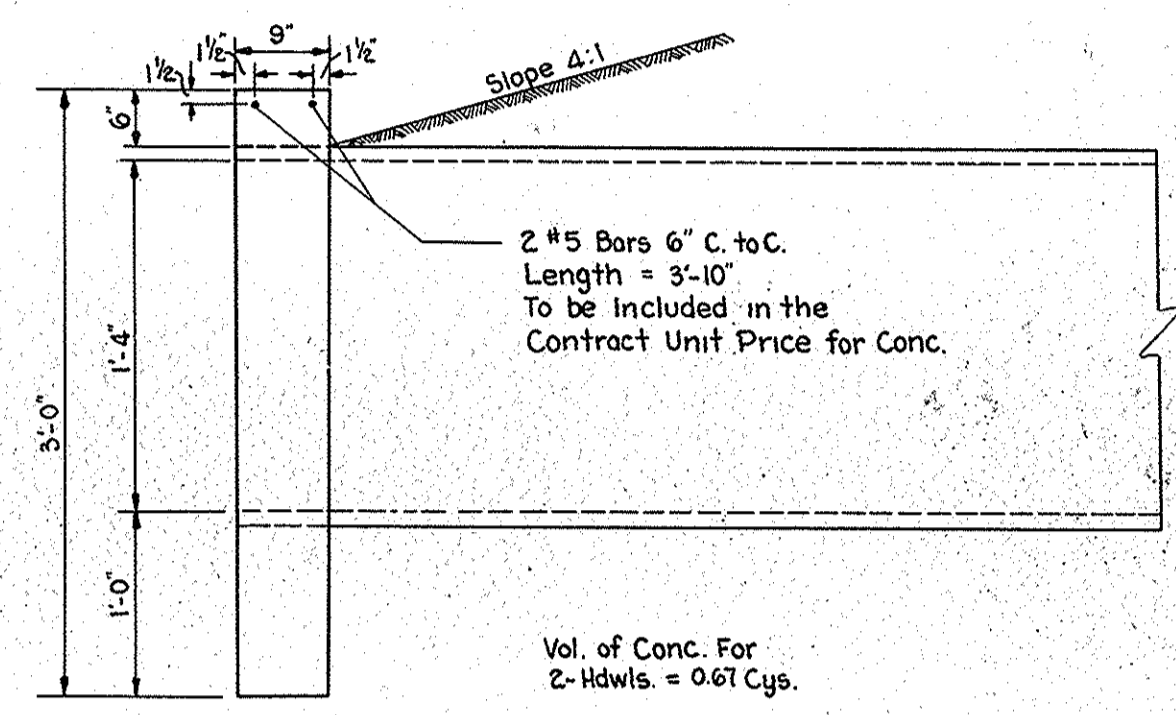
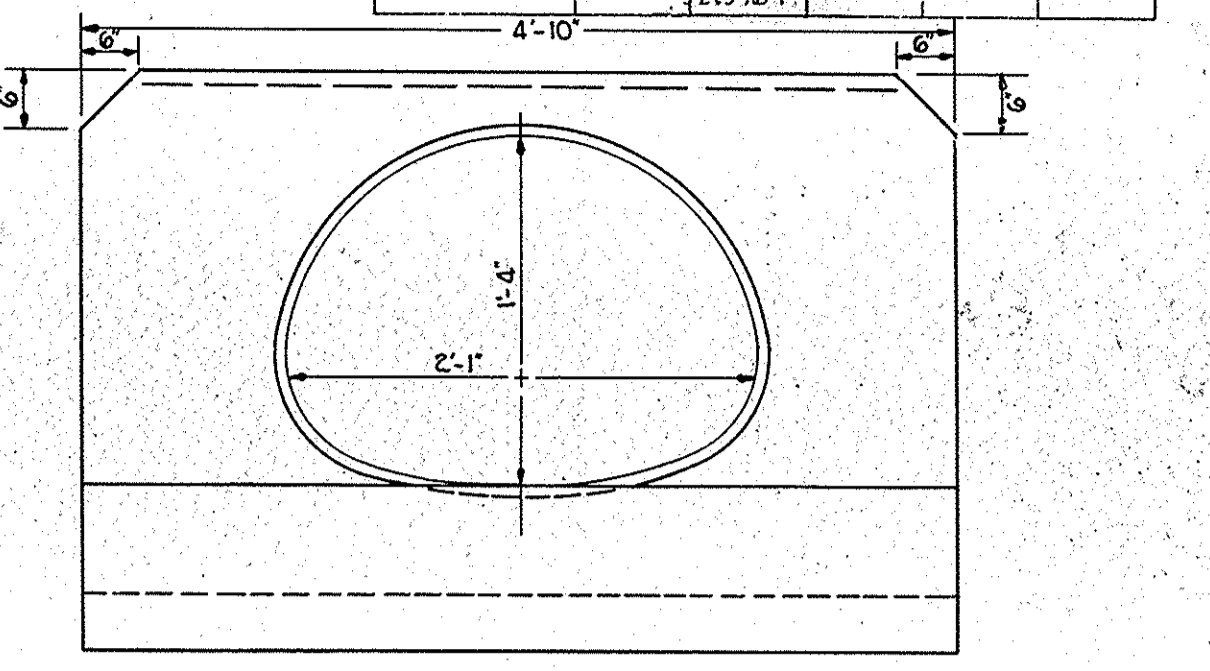
LONGITUDINAL SECTION THRU
R.C. BRIDGE APPROACH PVMT.
Scale: 1/8" = 1'-0"



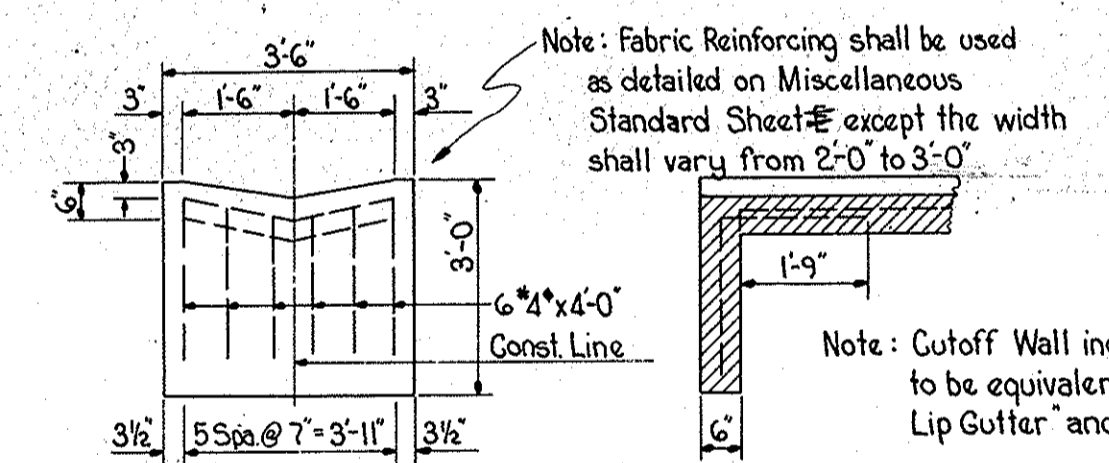
BENDING DIAGRAM
Not to Scale



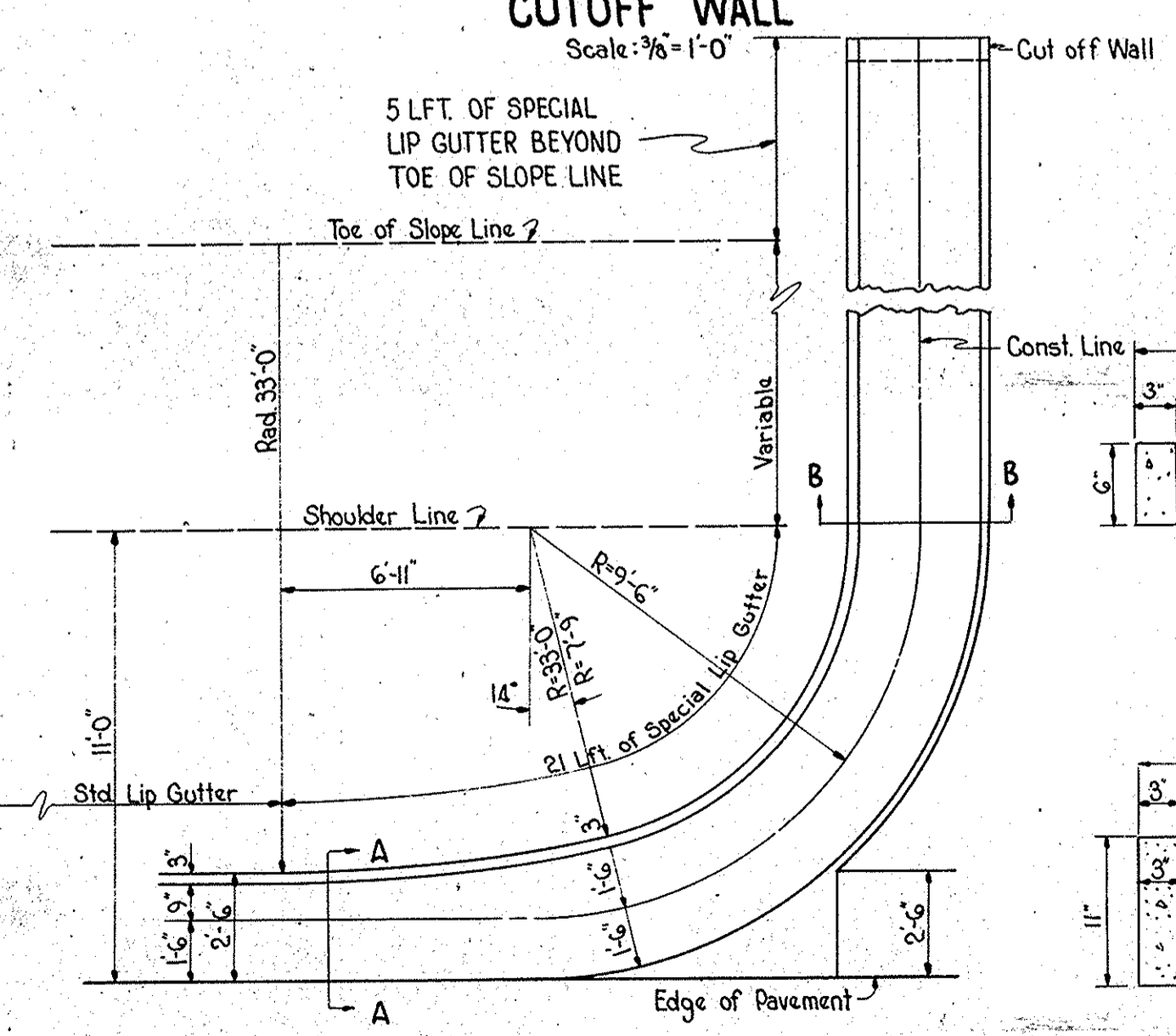
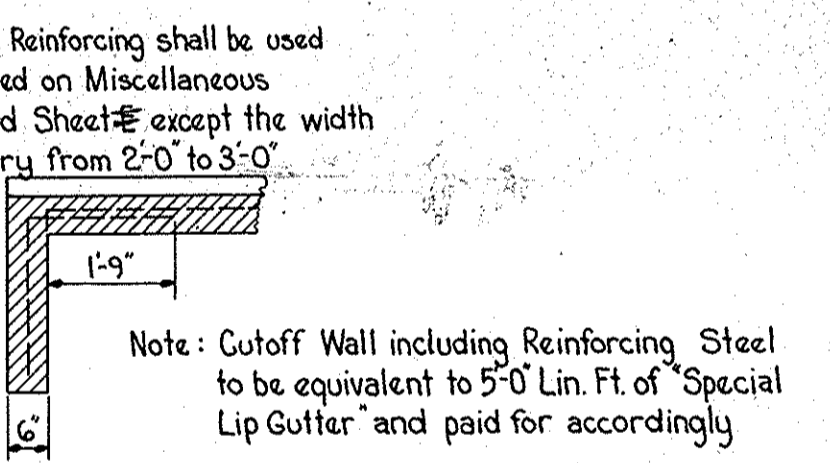
SODDED DITCH DETAIL
Scale: 1/4" = 1'-0"



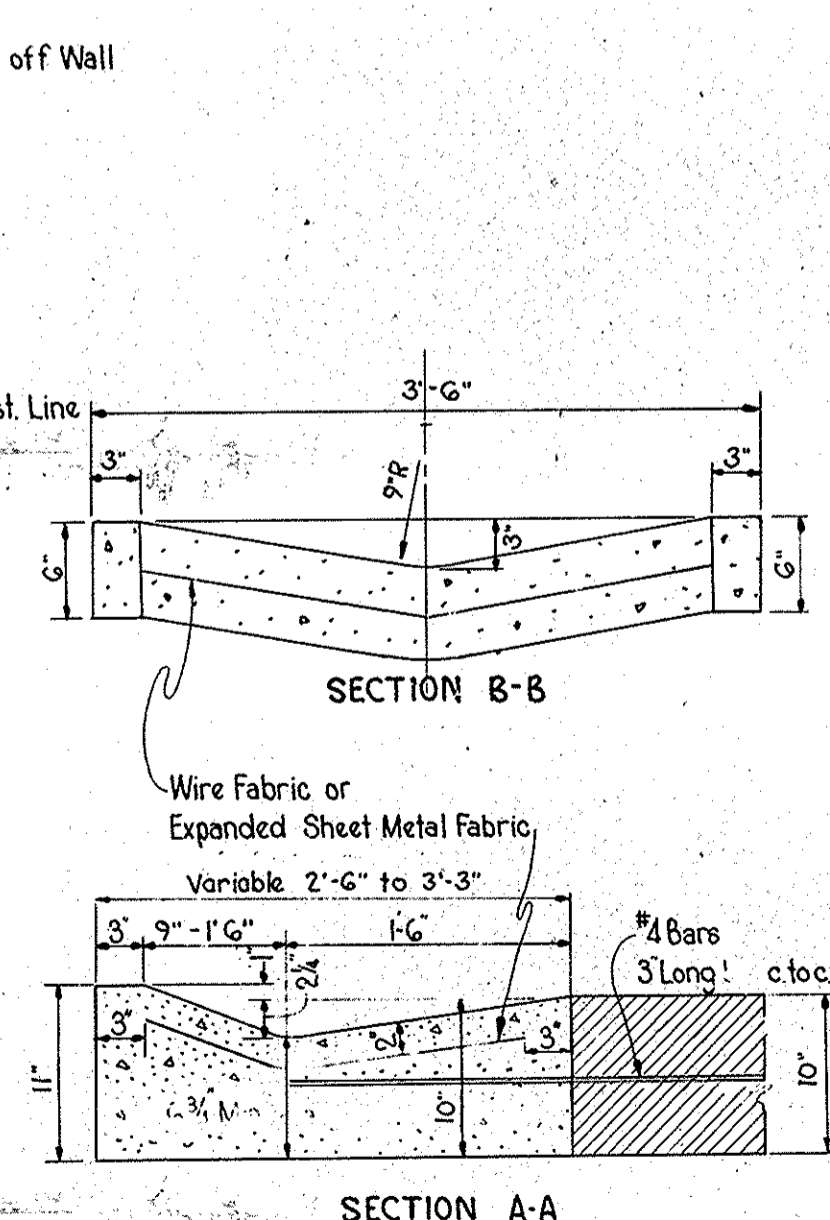
DETAIL OF HEADWALL FOR
25x16" C.M. PIPE ARCH
Not to Scale
STRUCTURE N° 55
Sheet #



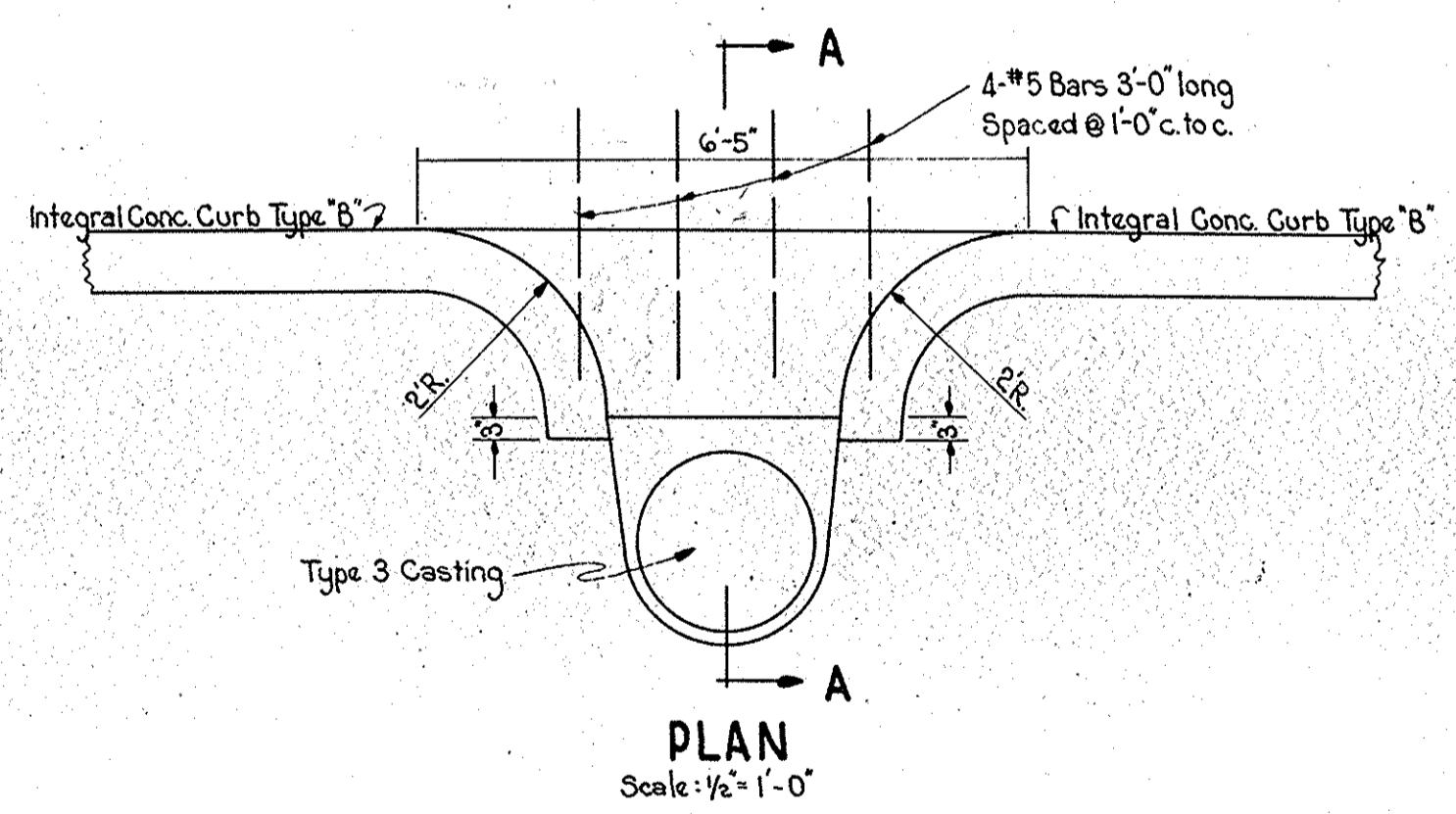
CUTOFF WALL
Scale: 3/8" = 1'-0"



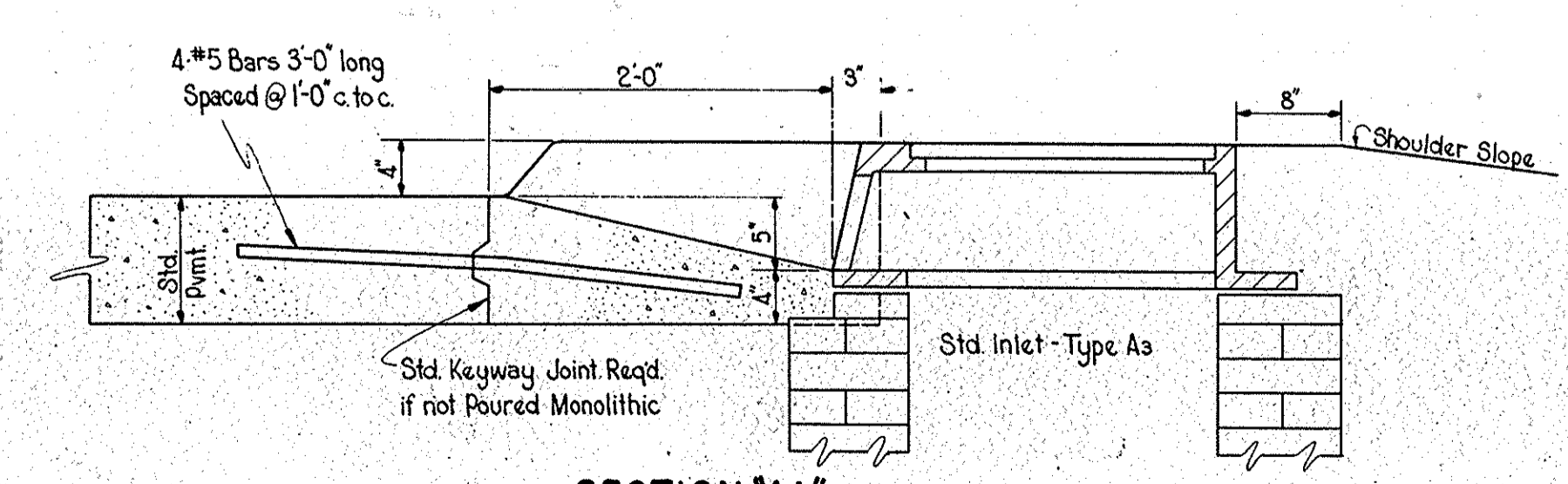
DETAIL OF SPECIAL LIP GUTTER (31)
Scale: 1/4" = 1'-0"
See Sheet 22



SECTION A-A
SCALE: 1" = 1'-0"

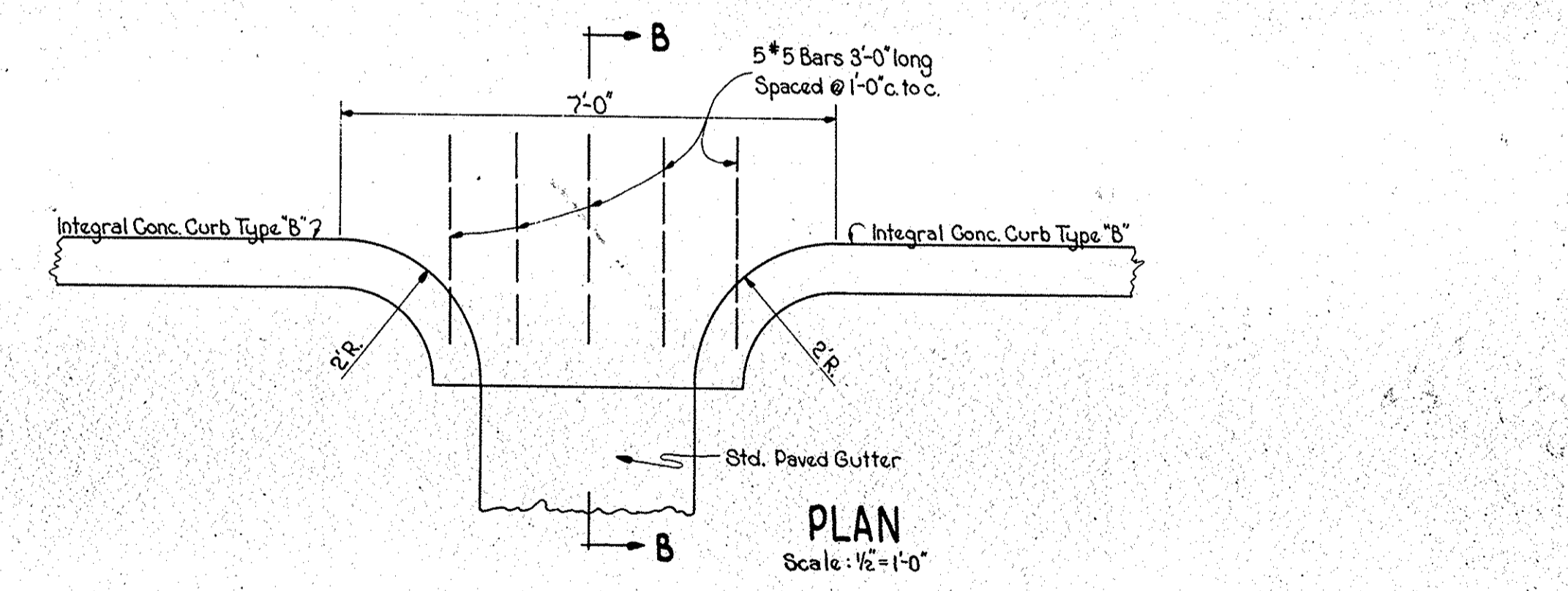


PLAN
Scale: 1/2" = 1'-0"

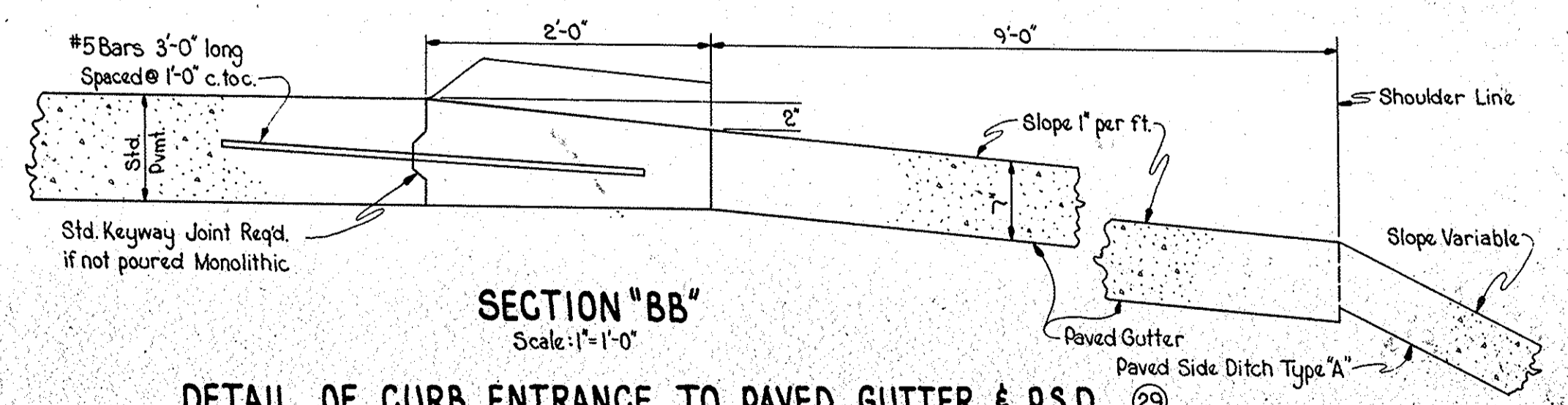


SECTION "AA"
Scale: 1" = 1'-0"

DETAIL OF ENTRANCE TO TYPE A3 INLET (32)
See Sheets 22, 23, 25



PLAN
Scale: 1/2" = 1'-0"



SECTION "BB"
Scale: 1" = 1'-0"

DETAIL OF CURB ENTRANCE TO PAVED GUTTER & P.S.D. (29)
See Sheets 22, 23, 25

DETAILS

Scales as Noted

